



HILLINGDON  
LONDON

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## **VIRTUAL** North Planning Committee

### To Councillors on the Committee

Councillor Henry Higgins (Chairman)  
Councillor John Morgan (Vice-Chairman)  
Councillor Jas Dhot  
Councillor Becky Haggar  
Councillor Allan Kauffman  
Councillor Carol Melvin BSc (Hons)  
Councillor John Oswell (Opposition Lead)  
Councillor Jagjit Singh  
Councillor David Yarrow

**Date:** THURSDAY, 11 MARCH  
2021

**Time:** 7.30 PM

**Location:** VIRTUAL MEETING

**Watch Live** Watch a live broadcast of this meeting on the Council's YouTube channel: [Hillingdon London](#)

**Important Meeting Advice:** Following UK Government advice to avoid all but essential travel and to practice social distancing, the usual physical public speaking rights at Planning Committee will be suspended temporarily during the coronavirus pandemic. Instead, written representations will be considered as part of Hillingdon's established Petitions Scheme.

**Published:** Wednesday, 3 March 2021

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Lloyd White  
Head of Democratic Services  
London Borough of Hillingdon,  
Phase II, Civic Centre, High Street, Uxbridge, UB8 1UW

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# Agenda

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## Chairman's Announcements

- 1 Apologies for Absence
- 2 Declarations of Interest in matters coming before this meeting
- 3 To sign and receive the minutes of the previous meeting 1 - 10
- 4 Matters that have been notified in advance or urgent
- 5 To confirm that the items of business marked Part 1 will be considered in public and that the items marked Part 2 will be considered in private

## PART I - Members, Public and Press

Items are normally marked in the order that they will be considered, though the Chairman may vary this. The name of the local ward area is also given in addition to the address of the premises or land concerned.

### Applications with a Petition

	Address	Ward	Description & Recommendation	Page
6	50 Newnham Avenue, Ruislip –  70191/APP/2021/174	Cavendish	Single storey side/rear extension, canopy structure above existing side entrance door and external alterations to the property, following demolition of existing outbuilding (Revised Description)  <b>Recommendation: Approval</b>	11 – 24  138-151
7	169 Joel Street, Eastcote –  22642/APP/2020/3181	Northwood Hills	Demolition of the existing dwelling and erection of a part one, part two and part three-storey building comprising 9 flats (3 x 1-bed, 5 x 2-bed and 1 x 3-bed) with 4 parking spaces.  <b>Recommendation: Refusal</b>	25 – 42  152-168

8	33 Wieland Road, Northwood –  14787/APP/2020/3531	Northwood Hills	Part two, part three storey rear extension, subterranean garage/services and storage room, structural underpinning, external remodelling including new roof construction  <b>Recommendation: Refusal</b>	43 – 58  169-178
9	40 Station Approach, South Ruislip –  27354/APP/2020/4256	South Ruislip	Erection of a three storey building comprising a dental surgery and 7 residential units (6 no. 1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery.  <b>Recommendation: Refusal</b>	59 – 86  179-213

### Applications without a Petition

	Address	Ward	Description & Recommendation	Page
10	11 Sandy Lodge Way, Northwood -  16948/APP/2020/3623	Northwood	Erection of a two storey building with habitable roofspace to create 3 x 2 -bed self-contained flats and 1 x 3-bed self-contained flat with forecourt car parking, cycle parking, new vehicular access and amenity space following demolition of existing dwelling  <b>Recommendation: Approval</b>	87 – 116  214-221

### PART II - Members Only

*That the reports in Part 2 of this agenda be declared not for publication because they involve the disclosure of information in accordance with Section 100(A) and Part 1 of Schedule 12 (A) to the Local Government Act 1972 (as amended), in that they contain exempt information and that the public interest in withholding the information outweighs the public interest in disclosing it.*

11	ENFORCEMENT REPORT	117-124
12	ENFORCEMENT REPORT	125-136

### PART I - Plans for North Planning Committee – pages 137-222

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## Minutes



**NORTH** Planning Committee

**17 February 2021**

**Meeting held at VIRTUAL - Live on the Council's YouTube channel: Hillingdon London**

	<p><b>Committee Members Present:</b> Councillors Henry Higgins (Chairman), John Morgan (Vice-Chairman), Jas Dhot, Becky Haggart, Allan Kauffman, Carol Melvin, John Oswell (Opposition Lead), Jagjit Singh and David Yarrow</p> <p><b>LBH Officers Present:</b> Desmond Adumekwe (Enforcement Manager), Glen Egan (Office Managing Partner - Legal Services), Liz Penny (Democratic Services Officer), James Rodger (Head of Planning, Transportation and Regeneration), Alan Tilly (Transport Planning and Development Manager), James Wells (Planning Team Leader)</p>
127.	<p><b>APOLOGIES FOR ABSENCE</b> (<i>Agenda Item 1</i>)</p> <p>There were no apologies for absence.</p>
128.	<p><b>DECLARATIONS OF INTEREST IN MATTERS COMING BEFORE THIS MEETING</b> (<i>Agenda Item 2</i>)</p> <p>Councillor Carol Melvin declared an interest in agenda item 6 as she had been involved with different planning applications at the property in the past. Councillor Melvin left the meeting and did not participate in the discussion or voting for this item.</p>
129.	<p><b>TO SIGN AND RECEIVE THE MINUTES OF THE PREVIOUS MEETING DATED 28 JANUARY 2021</b> (<i>Agenda Item 3</i>)</p> <p><b>RESOLVED: That the minutes of the meeting dated 28 January 2021 be approved as an accurate record.</b></p>
130.	<p><b>MATTERS THAT HAVE BEEN NOTIFIED IN ADVANCE OR URGENT</b> (<i>Agenda Item 4</i>)</p> <p>None.</p>
131.	<p><b>TO CONFIRM THAT THE ITEMS OF BUSINESS MARKED PART 1 WILL BE CONSIDERED IN PUBLIC AND THAT THE ITEMS MARKED PART 2 WILL BE CONSIDERED IN PRIVATE</b> (<i>Agenda Item 5</i>)</p> <p>It was confirmed that items 1 – 13 were marked Part I and would be considered in public and items 14 – 16 were marked Part II and would be considered in private.</p>

132. **16 MURRAY ROAD, NORTHWOOD - 4626/APP/2020/3048** (*Agenda Item 6*)

**Two storey rear extension and enlargement of habitable roofspace to include 2 rear dormers, 3 side rooflights, central roof lantern and alterations to elevations.**

Officers introduced the report and advised Members that 7 responses to public consultations and a petition in objection to the application had been received. The Committee heard that the original property had previously been extended to the rear to include a 4.25m deep single storey extension and a 5.5m deep two storey flat roofed extension. The proposal would incorporate these and further extend to the rear, giving a two-storey rear extension of between 2.3m and 9.85m in depth. A further flat roofed ground floor extension of 4.5m in depth and 3.35m in height was proposed.

Members were informed that the Conservation Officer had raised strong objections to the proposal indicating that the proposed rear additions would be disproportionately large in relation to the original house. The development would fail to enhance the appearance of Northwood Town Centre Conservation Area.

A written submission was read to the Committee on behalf of petitioners objecting to the application. Key points highlighted included:

- The proposal would create an excessively large extension with a massive 70% increase in internal floor area;
- The design was simplistic, overwhelming, out-of-place, insensitive to its setting and an over-development with an excessively large extension;
- The development would result in a 3-storey house that lent itself to subdivision into flats or an HMO;
- The excessive bulk, depth and height would impact negatively on the gap views between numbers 16, 18 and 18A;
- The proposed 3-storey development would have an adverse impact on adjacent properties and would contravene Hillingdon Policies DMH1 and DMHB11;
- The proposed development would have a detrimental impact on the residential amenity of 14, 18 and 18A in terms of overdominance, loss of light and outlook;
- The application failed to meet the standards set out in Hillingdon Council's Conservation Areas policy;
- Extensive habitable accommodation was proposed at 3<sup>rd</sup> floor roof level – to comply with structural and thermal insulation requirements, the existing roof would need to be demolished or substantially re-configured which would change the character of the original building;
- There was no Design and Access Statement which was unacceptable;
- Trees or hedges would need to be removed or pruned which would be detrimental to established wildlife;
- The proposal did not respect the property's original plan of the house therefore did not accord with Hillingdon's planning policies;
- The proposed 3-storey extension would be overly large and incongruous and would be an intrusive addition to the property;
- The design failed to respect and harmonise with the architectural character of the original dwelling, the group of 3 houses of which it formed part, the street scene and the wider Northwood Town Conservation Area.

Members agreed that the proposal constituted an unacceptable over-development of the site. The officer's recommendation was moved, seconded, and when put to a vote, agreed with 6 votes in favour of refusing the application. Councillor Melvin had

declared an interest therefore did not vote on this item and Councillor Yarrow had been unable to access the meeting at this point.

**RESOLVED: That the application be refused.**

133. **4 WOODSIDE ROAD, NORTHWOOD - 73105/APP/2020/3521** (*Agenda Item 7*)

**Erection of a garden shed to rear.**

Officers presented the application which sought planning permission for the erection of a garden shed.

A written submission was read to the Committee on behalf of petitioners objecting to the application. Key points included:

- Planning conditions were not being complied with - petitioners enquired why the applicant was being allowed to ignore conditions at will and had not followed the process to appeal any conditions considered to be unfair;
- The Inspector had specified conditions that, in order to protect the character and appearance of the Area of Special Local Character, trees and bushes marked on the application were to be retained or replaced and there were to be no garages, sheds or other outbuildings. All these conditions were being ignored by the applicant who had felled a healthy mature apple tree in order to replace it with a shed;
- The plastic shed did not enhance existing landscaping, trees, biodiversity or other natural feature of merit as was required by the Council's Trees and Landscaping policy;
- The application site was within an Area of Special Local Character (the Gatehill Estate). Cabinet had adopted a document in which Council officers had outlined the characteristics and features of the Gatehill Estate. One of those defining characteristics was "the gaps between houses allowing views of mature trees in the rear gardens from the street." The Inspector had wished to protect the character of the area but the Committee was being asked to believe that the view of a plastic shed was the same as the view of a mature apple tree;
- Some previous work had been carried out without consent and the applicants now sought to overturn conditions set out by the Planning Inspectorate.

Members sought clarification regarding the points raised by the lead petitioner. It was explained that, under normal circumstances, planning permission would not be required in order to erect a shed. In this case it was needed as permitted development rights had been removed at the property. Members heard that it was important to be proportionate when making decisions – the application was for a small garden shed. It was regrettable that a small tree had been removed to make room for the shed but a degree of proportionality was essential in terms of what the Council would pursue.

Members enquired whether it would be possible to add a condition in relation to the replacement of the tree which had been felled. It was confirmed that the garden was small therefore it was not deemed reasonable to request this. In response to further questions from the Committee, it was confirmed that, although the site lay within a critical drainage area, the shed would have no impact on drainage.

Councillors observed that it was unfortunate that a tree had been lost and felt a wooden shed would have been more in keeping with the area; however, it was recognised that this was not a matter for the Committee. No further concerns were

raised.

The officer's recommendation was moved, seconded and, when put to a vote, agreed with 7 votes in favour. One Councillor abstained as he had been experiencing technical difficulties and had missed part of the discussion of the item.

**RESOLVED: That the application be approved.**

134. **THE SIX BELLS PUBLIC HOUSE, DUCKS HILL ROAD, RUISLIP - 14387/APP/2020/4126** (*Agenda Item 8*)

**Proposed barn extension to provide an extended dining area at ground floor and 8 no. guest rooms at first floor, adding a guest house use to the existing public house/restaurant to create a mixed use (Sui Generis), with associated works and landscaping.**

The Committee agreed to consider items 8 and 9 collectively as they related to the same application site. Officers presented the report noting that the proposed development was considered to pose 'less than substantial harm' to the setting of the Grade II Listed Building.

A written submission was read to the Committee on behalf of petitioners in support of the application (item 8). Key points highlighted included:

- The Six Bells Inn on Ducks Hill Road was one of the older buildings in the Borough, built around 1810, on the northern edge of Ruislip;
- It had served the community as a pub and inn for over 200 years but had closed in 2015. Three years later it had been re-opened by Richard Arens as a bar and restaurant;
- The site was on the edge of the Green Belt. Mr Arens had obtained special planning permission to demolish some of the outbuildings and replace them with a dining hall. The new hall was to be in the local barn style with walls of mixed wood, brick and render and a tall gabled roof;
- Before building work could commence, Covid-19 had brought everything to a halt. Mr Arens had recognised the need to diversify therefore had decided to restore the accommodation aspect of the Six Bells by adding bedrooms, including a specially adapted room for disabled guests. Ruislip was not well served for accommodation so the additional facility was welcome;
- The dining hall / inn was well set back from the road and from neighbouring properties. Very little additional land was used by the proposal;
- Since the previous application had been rejected, Mr Arens had worked with Hillingdon planning officers to address the concerns raised and had produced modified plans with reduced roof height and length of the new hall so it does not overtop the Grade II-listed part of the building;
- Many of the signatories of the petition were immediate neighbours of the Six Bells. The Ruislip Residents' Association had agreed to support the application as being in the best interests of the community and the Chairman of Ruislip Woods Management Advisory Group had raised no objections.

Members noted that a previous application had been refused and enquired why the current application was deemed to be acceptable and how it differed from the original. It was confirmed that very special circumstances applied in this cases and, on balance, the proposed development was considered to provide public benefits to outweigh the harm posed. The Committee heard that the previous application had been for a larger

development with 10 bedrooms proposed. In the new application, the length of the proposed structure had been reduced by approximately 4m and it was starting to become more subservient to the main pub building. Officers felt the proposed development was large but approvable.

Members welcomed the new proposal noting that it was smaller and an improvement on the previous one. Clarification was sought regarding alterations to the door as set out on page 61 of the agenda pack; it was noted that the door had been altered from a 3 panel door to a modern 4 panel door – apparently without authorisation. Members sought reassurance that no further amendments to the application would be allowed without prior consent being given. It was agreed that delegated authority be granted to the Head of Planning to expand Condition 6 (agenda item 9) to tighten the list of building conditions to be adhered to.

Members welcomed the application and observed that the development would be good for both the pub and the surrounding area. The Committee was pleased to note that the local Residents' Association was also in favour of the proposal. The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.

**RESOLVED:**

- 1) That delegated authority be granted to the Head of Planning to expand Condition 6 (agenda item 9) so as to tighten up the list of building conditions to be adhered to; and**
- 2) That the application be approved.**

135. **THE SIX BELLS PUBLIC HOUSE, DUCKS HILL ROAD, RUISLIP - 14387/APP/2020/4128** (*Agenda Item 9*)

**Proposed barn extension to provide an extended dining area at ground floor and 8 no. guest rooms at first floor (Application for Listed Building Consent).**

This agenda item was discussed in tandem with agenda item 8.

**RESOLVED:**

- 1) That delegated authority be granted to the Head of Planning to expand Condition 6 so as to tighten up the list of building conditions to be adhered to; and**
- 2) That the application be approved.**

136. **188 BURY STREET, RUISLIP - 8697/APP/2020/3622** (*Agenda Item 10*)

**Demolition of existing bungalow and erection of 4 x 4 bed dwelling houses with associated parking spaces and external works.**

Officers introduced the application noting that the proposed dwellings would be detached and would have a 2m separation gap between each dwelling. The first two dwellings would follow the established building line of 186 Bury Street. The remaining two dwellings would be set forward of this building line by approximately 4m.

Members welcomed the application stating that it was good to see a development of 4 bed family houses with gardens; this type of dwelling was sought after in the Borough.

The Committee sought clarification regarding drainage arrangements at the site noting that the area was prone to flooding. It was confirmed that adequate drainage had been provided for as set out in Condition 8. In response to further questions from the Committee, it was confirmed that each home would be furnished with one active charging point for electric vehicles.

Members welcomed the proposal and raised no further concerns.

The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.

**RESOLVED: That the application be approved.**

137. **PYLON FARM, NEW YEAR GREEN LANE, HAREFIELD - 12579/APP/2020/2742**  
(Agenda Item 11)

**Demolition of existing outbuildings and construction of single replacement outbuilding.**

Officers introduced the application informing Members that the proposal was for the demolition of a small group of existing farm buildings and the construction of a single replacement building. It was noted that a similar application had been refused planning permission under delegated powers on 22/5/20 as it was considered that the application lacked detail in terms of the intended use of the building in relation to Green Belt policy and other supporting information. The additional clarification and information had now been provided with the new application and it was considered that the reasons for refusal of the previous application had been overcome. The Committee heard that the new outbuilding would occupy a similar site to the demolished buildings which were in a poor state of repair. It was considered that the proposed development would not materially impact on the openness of the Green Belt. The application was recommended for approval.

In response to Members' requests for clarification, it was confirmed that the caravan currently on site would be removed. It was further confirmed that conditions relating to the removal of asbestos had been included on page 135 of the agenda pack.

The Committee raised no further concerns or queries. The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.

**RESOLVED: That the application be approved.**

138. **MOUNT VERNON HOSPITAL, RICKMANSWORTH ROAD, NORTHWOOD - 3807/APP/2020/4265**  
(Agenda Item 12)

**Construction of 1.5m wide gravel footpath around lawn perimeter and through the centre of the woodland belt to south of lawn, to include two access ramps with handrails.**

The Committee agreed that this was a straightforward application therefore no presentation was required. Members raised no queries or concerns and were happy to endorse the officer's recommendation for approval.

The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.

**RESOLVED: That the application be approved.**

139. **THE OLDE NORTHWOOD PH, 142 PINNER ROAD, NORTHWOOD - 73773/APP/2020/3002** (*Agenda Item 13*)

**Conversion of public house to residential use to provide 4 self-contained flats including habitable roofspace and roof terrace, demolition of existing garages and rear element and the erection of 2 x two storey dwellings with habitable roofspace including parking and amenity space and external works.**

Officers presented the application noting that planning approval for the demolition of the existing public house and erection of a part 3-storey, part 2-storey building to provide 9 flats had been approved in January 2020. Members were informed that the current application had the benefit of retaining the original pub exterior. It was felt that the proposed development would respect the character and appearance of the Area of Special Local Character. It was considered that the proposal would not significantly impact on the amenity of neighbouring occupiers and would provide adequate living accommodation, amenity space and parking provision. Officers highlighted a slight error on page 178 of the agenda pack – it was noted that Condition 4, point 2.d should read ‘Car Parking Layouts (including demonstration that **20%** of all parking spaces are served by electrical charging points)’. The application was recommended for approval.

Members were pleased to note that the attractive exterior of the building was to be maintained. The Committee sought clarification regarding the amenity space to be provided and enquired whether this would be adequate. It was confirmed that the gardens for the two new dwellings were slightly below the Council’s amenity standards; however, it was noted that a football ground, golf course and other amenity spaces were nearby and easily accessible. A nearby outdoor gym was also planned.

In response to questions from the Committee, it was confirmed that six car parking spaces would be provided – one for each flat and one for each dwelling. The Highways Officer confirmed that the developer would be required to provide a car parking allocation plan.

Members raised no further concerns and commented that this was a good proposal. The officer’s recommendation was moved, seconded and, when put to a vote, unanimously agreed.

**RESOLVED: That the application be approved.**

140. **ENFORCEMENT REPORT** (*Agenda Item 14*)

**RESOLVED:**

- 1. That the enforcement action, as recommended in the officer’s report, was agreed; and,**
- 2. That the Committee resolved to release their decision and the reasons for it outlined in the report, into the public domain, solely for the purposes of it issuing the formal beach of condition notice to the individual concerned.**

This item is declared as exempt from publication as it involves the disclosure of information in accordance with Section 100(A) and **paragraphs 1, 2 and 7** of Part 1 of

	<p>Schedule 12 (A) to the Local Government Act 1972 (as amended), in that the report contains <b>information relating to any individual, information likely to reveal the identity of an individual and information relating to any action taken or to be taken in connection with the prevention, investigation or prosecution of crime</b> and that the public interest in withholding the information outweighs the public interest in disclosing it.</p>
141.	<p><b>ENFORCEMENT REPORT</b> (<i>Agenda Item 15</i>)</p> <p><b>RESOLVED:</b></p> <ol style="list-style-type: none"> <li>1. That the enforcement action, as recommended in the officer's report, was agreed; and,</li> <li>2. That the Committee resolved to release their decision and the reasons for it outlined in the report, into the public domain, solely for the purposes of it issuing the formal beach of condition notice to the individual concerned.</li> </ol> <p>This item is declared as exempt from publication as it involves the disclosure of information in accordance with Section 100(A) and <b>paragraphs 1, 2 and 7</b> of Part 1 of Schedule 12 (A) to the Local Government Act 1972 (as amended), in that the report contains <b>information relating to any individual, information likely to reveal the identity of an individual and information relating to any action taken or to be taken in connection with the prevention, investigation or prosecution of crime</b> and that the public interest in withholding the information outweighs the public interest in disclosing it.</p>
142.	<p><b>ENFORCEMENT REPORT</b> (<i>Agenda Item 16</i>)</p> <p><b>RESOLVED:</b></p> <ol style="list-style-type: none"> <li>1. That the enforcement action, as recommended in the officer's report, was agreed; and,</li> <li>2. That the Committee resolved to release their decision and the reasons for it outlined in the report, into the public domain, solely for the purposes of it issuing the formal beach of condition notice to the individual concerned.</li> </ol> <p>This item is declared as exempt from publication as it involves the disclosure of information in accordance with Section 100(A) and <b>paragraphs 1, 2 and 7</b> of Part 1 of Schedule 12 (A) to the Local Government Act 1972 (as amended), in that the report contains <b>information relating to any individual, information likely to reveal the identity of an individual and information relating to any action taken or to be taken in connection with the prevention, investigation or prosecution of crime</b> and that the public interest in withholding the information outweighs the public interest in disclosing it.</p>
	<p>The meeting, which commenced at 6.00 pm, closed at 7.30 pm.</p>

These are the minutes of the above meeting. For more information on any of the resolutions please contact Liz Penny on Telephone 01895 250636 or email: [democratic@hillington.gov.uk](mailto:democratic@hillington.gov.uk). Circulation of these minutes is to Councillors, Officers,

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the Press and Members of the Public.

**The public part of this meeting was filmed live on the Council's YouTube Channel to increase transparency in decision-making; however, these minutes remain the official and definitive record of proceedings.**

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## Report of the Head of Planning, Transportation and Regeneration

**Address** 50 NEWNHAM AVENUE RUISLIP MIDDLESEX

**Development:** Single storey side/rear extension, canopy structure above existing side entrance door and external alterations to the property, following demolition of existing outbuilding (Revised description)

**LBH Ref Nos:** 70191/APP/2021/174

**Drawing Nos:** Location Plan  
69112 - 1100 A  
69112 - 1101 A  
69112 - 1200 A  
69112 - 1201 A  
69112 - 1300 A  
69112 - 1500 B  
69112 - 3300 B  
69112 - 3100 B  
69112 - 3101 B  
69112 - 3200 B  
69112 - 3201 B

**Date Plans Received:** 14/01/2021

**Date(s) of Amendment(s):** 18/01/2021

**Date Application Valid:** 14/01/2021

### 1. CONSIDERATIONS

#### 1.1 Site and Locality

The application site is located on the western side of Newnham Avenue and comprises a two storey semi-detached dwelling. The property is characterised by its front bay windows and hipped roof profile, and is externally finished in a mixture of brickwork and pebble dash render painted white. There is a driveway to the front of the site which has the capacity to accommodate two vehicles.

The surrounding area is predominately residential in nature and is defined by semi-detached and terraced properties. However, it is noted that Newnham School is located to the North-West of the site, approximately 140 metres away. Number 48 Newnham Avenue is an adjacent two storey property located immediately to the West of the site. Number 52 Newnham Avenue is located immediately to the East of the site, and forms the adjoining semi-detached property to the application property.

The application site is not designated within a Conservation Area, nor an Area of Special Local Character. The site does not contain any Listed Buildings. There are no trees that are subject to a Tree Preservation Order within the site or on adjoining land. The site lies in Flood Zone 1 and is designated as being within a Critical Drainage Area.

#### 1.2 Proposed Scheme

Planning permission is sought for a single storey side/rear extension, a canopy structure above the existing side entrance door and external alterations to the property, following the demolition of an existing outbuilding.

Following negotiations, revised drawings were submitted showing a reduction to the size of the proposed single storey side/rear extension. Please refer to the section of the report titled 'Main Planning Issues' for further details on the amendments that have made to the proposal.

### **1.3 Relevant Planning History**

#### **Comment on Planning History**

There is no relevant planning history attached to this site.

### **2. Advertisement and Site Notice**

**2.1** Advertisement Expiry Date:- Not applicable

**2.2** Site Notice Expiry Date:- Not applicable

### **3. Comments on Public Consultations**

Four neighbouring properties and Eastcote Residents Association were consulted on 19th January 2021. Two additional neighbouring properties were consulted on 20th January 2021. Following changes to the wording of the proposal description and amendments to the drawings, the same six neighbouring properties and Eastcote Residents Association were re-consulted on 19th February 2021. The consultation period expires on 5th March 2021. If there are any further representations not covered in this report they will be included in the Committee addendum.

A petition against the application with 20 signatures was received by the Council, with the lead petitioner residing at number 52 Newnham Avenue. The objection raised is as follows

"My objection to a utility room and kitchen built near my rear dining room, next to my adjoining party wall. The issues of noise, flooding, social disturbance and dampness occurring to my property. Also fire, ventilation, blockage of raw sewage."

Officer's Response: Issues relating to party wall matters are not a planning consideration and are covered by the Party Wall etc Act 1996. The concerns raised in regards to potential dampness, fire safety, ventilation problems and the blockage of drains are not planning considerations and are covered by separate legislation. The concerns relating to the impact on neighbouring residential amenity and flooding will be covered in the section below.

### **4. Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

Part 2 Policies:

DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 18	Private Outdoor Amenity Space
DMHD 1	Alterations and Extensions to Residential Dwellings
DMT 6	Vehicle Parking
LPP 3.5	(2016) Quality and design of housing developments

## 5. MAIN PLANNING ISSUES

### MAIN PLANING ISSUES:

The main considerations are the design and impact on the character of the existing property, the impact upon the streetscene and locality, the impact upon the amenities of adjoining occupiers, the reduction in size of the rear garden and car parking provision.

### CHARACTER AND APPEARANCE:

Policy BE1 of the Hillingdon Local Plan: Part One Strategic Policies (2012) seeks a quality of design in all new development that enhances and contributes to the area in terms of form, scale and materials; is appropriate to the identity and context of the townscape; and would improve the quality of the public realm and respect local character.

Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that new development will be required to be designed to the highest standards and incorporate principles of good design including: harmonising with the local context by taking into account the surrounding scale of development, considering the height, mass and bulk of adjacent structures.

Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that alterations and extensions to dwellings should not have an adverse cumulative impact on the character and appearance of the street scene, and should appear subordinate to the main dwelling.

With regard to single storey side extensions, Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that:

- i) side extensions should not exceed half the width of the original property;
- ii) extensions to corner plots should ensure that the openness of the area is maintained and the return building line is not exceeded; and
- iii) garages should reflect the size guidelines set out in Appendix C Parking standards.

With regards to single storey rear extensions, Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that:

- i) single storey rear extensions on terraced or semi-detached houses with a plot width of 5 metres or less should not exceed 3.3 metres in depth or 3.6 metres where the plot width is 5 metres or more;
- ii) single storey rear extensions to detached houses with a plot width of 5 metres or more should not exceed 4.0 metres in depth;
- iii) flat roofed single storey extensions should not exceed 3.0 metres in height and any pitched or sloping roofs should not exceed 3.4 metres in height, measured from ground level; and
- iv) in Conservation Areas and Areas of Special Local Character, flat roofed single storey

extensions will be expected to be finished with a parapet.

Following negotiations, revised drawings were submitted showing a reduction to the size of the proposed single storey side/rear extension. The original set of drawings showed that the proposed extension would measure between 4.5 metres to 4.8 metres in depth. The revised proposed extension has now been reduced to a depth ranging between 3.6 metres and 3.9 metres. It would extend beyond the side (western) wall of the host dwelling by 1.4 metres, and then wrap-around the full width of the rear elevation of the building. The proposed single storey side/rear extension would have a pitched roof profile with four roof lights. Windows are being proposed in the front and side (western) elevations of the proposed extension, and open glazed doors would be fitted in its rear elevation.

Due to its modest size and scale, it is considered that the proposed single storey side/rear extension would appear as a subordinate addition when viewed against the two storey scale of the original host dwelling. When measured from the original rear wall of the host dwelling, the depth of the revised proposed extension would be compliant with the prescribed depth set out in Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020). Views of the single storey side element of the proposed extension would be visible from the street scene. However, given its 8 metre set-back from the front wall of the property, it is considered that the proposed single storey side/rear extension would appear visually subservient to the host dwelling when viewed from the public domain on Newnham Avenue.

It is acknowledged that the pitched roof profile on the proposed single storey side/rear extension would not match the roof form on the host dwelling. The maximum ridge height would also marginally exceed the prescribed height set out in Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020). However, the exceedance over the prescribed height would only be 10 centimetres, with the rear element of the proposed extension being set-down from the first floor rear window sills. It is therefore considered that the roof profile on the rear element of the proposed extension would not detract from the architectural composition of the host dwelling. Furthermore, when viewed against the varied roof profiles on the properties on Newnham Avenue, it is considered that the pitched roof on the side element of the proposed extension would not appear out of character with the surrounding context.

The proposal also seeks to make external alterations to the property. These alterations includes replacing the front entrance door and first floor side window with ones of a different style; the introduction of glass panelling on both sides of the new front entrance door; and the removal of the rear chimney stack.

It is considered that the new front door (including the glass panelling) and first floor side window would respect the architectural style of the host dwelling. The application property is not designated within an Conservation Area, nor is it a Listed Building. Given its limited historic merit, there is no objection to the removal of the existing rear chimney stack. It is also worth highlighting that chimney stacks can potentially be removed without requiring planning permission under permitted development rights.

Due to its siting, design and modest size, it is considered that the proposed canopy structure above the existing side entrance door would appear as a sympathetic feature to the application property. The proposed canopy structure would provide an element of visual interest to the side elevation of the original host dwelling, whilst ensuring that it would not appear overly prominent from the street scene.

The proposed single storey side/rear extension would be externally finished in white render. It is considered that this choice of material would complement the white painted pebble dash rendering found on the upper front elevation, side and rear elevations of the host dwelling.

In light of the above, it is considered that the proposal would not cause harm to the character and appearance of the host dwelling and the surrounding area, in accordance with the provisions set out in the NPPF (2019), Policy BE1 of the Hillingdon Local Plan: Part One Strategic (2012) and Policies DMHB 11, DMHB 12 and DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

#### NEIGHBOURING RESIDENTIAL AMENITY:

Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that planning applications relating to alterations and extensions of dwellings will be required to ensure that: ii) a satisfactory relationship with adjacent dwellings is achieved; and v) there is no unacceptable loss of outlook to neighbouring occupiers. Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) seeks to ensure that development proposals do not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

Numbers 48 and 52 Newnham Avenue are the principal properties that must be considered in regard to neighbouring residential amenity, given that they are the nearest dwellings to the application site.

Number 48 Newnham Avenue is an adjacent neighbouring property located to the North-West of the application site. This neighbouring property contains a garage which runs along the mutual boundary shared with the application site, and a single storey rear extension measuring approximately 3.5 metres deep. The new side (western) wall of the proposed single storey side/rear extension would be set-away from the mutual boundary shared with number 48 by approximately by 1 metre. Given this relationship, it is considered that the proposed single storey side/rear extension would not adversely impact the residential amenities enjoyed by the occupiers at number 48, in terms of loss of daylight/sunlight, outlook, sense of enclosure and overbearing impact.

It is noted that the proposed single storey side/rear extension would contain a window fitted in its western elevation. However, given that the proposed window would directly face the existing boundary treatment, it is considered that the proposal would result in an unacceptable loss of privacy or overlooking for the neighbouring occupiers at number 48 Newnham Avenue. The proposed window fitted in the front elevation of the proposed single storey side/rear extension would be in excess of 30 metres from the properties to the East of the site, on the opposite side of Newnham Avenue.

Number 52 Newnham Avenue is an adjoining neighbouring property located to the South-East of the application site. This neighbouring property has not been extended to its rear. Following negotiations, the depth of the proposed single storey side/rear extension has been reduced to a depth of 3.6 metres along the mutual boundary shared with number 52. The revised depth of the proposed single storey side/rear extension would now be compliant to prescribed depth set out in Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020). Taking this into account, coupled with its modest eaves height of 2.6 metres (measured from the proposed parapet wall), it is considered that the proposed single storey side/rear extension would not cause undue

harm to the living conditions of the neighbouring occupiers at number 52, in terms of loss of daylight/sunlight, outlook, sense of enclosure and overbearing impact.

Concerns have been raised by neighbouring residents regarding noise and disturbance. The submitted floor plans show that the proposed extension would serve as additional ancillary floor space in the form of an open plan kitchen, lounge and dining area. The existing dining room would partially be converted into a utility room. Given that the property would remain in use as a single family dwelling, it is considered that the proposed layout would not result in an unacceptable level of noise and disturbance over and beyond what is typically associated with a single family household. As such, it is considered that a refusal on this basis would be unjustified, and difficult to sustain in the event of an appeal.

Due to the nature of the proposed external alterations and the limited size of the proposed side canopy, it is considered that these elements of the proposal would not cause harm to the living conditions of neighbouring occupiers. In the event of an approval, a condition is recommended requiring the proposed first floor replacement window (serving the stairway) to be obscure glazed and non-opening up to 1.8 metres of the finished floor level.

The petition objecting to the application has been duly noted. However, having regard to the above, it is considered that the proposed development, subject to conditions, would not have an adverse impact on the residential amenities of neighbouring occupiers, in accordance with the objectives of Policies DMHB 11 and DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

#### EXTERNAL AMENITY SPACE PROVISION:

Policy DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that planning applications relating to alterations and extensions of dwellings will be required to ensure that adequate garden space is retained. Policy DMHB 18 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that all new residential development and conversions will be required to provide good quality and usable private outdoor amenity space. Amenity space should be provided in accordance with the standards set out in Table 5.3, which requires three bedroom dwellings to have at least 60 square metres of private amenity space.

The proposed development would retain approximately 120 square metres of rear garden space, which would far exceed the minimum requirements set out in Table 5.3 of the Hillingdon Local Plan Part 2: Development Management Policies (2020). As such, it is considered that the proposal would not undermine the provision of private external amenity space for the existing occupiers at the site, in compliance with Policies DMHD 1 and DMHB 18 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

#### PARKING AND HIGHWAY SAFETY:

Policy DMHD 1 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) states that planning applications relating to alterations and extensions of dwellings will be required to ensure that adequate off-street parking should be retained, as set out in Table 1: Parking Standards in Appendix C. This is reinforced by Policy DMT 6 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) which states that "Development proposals must comply with the parking standards outlined in Appendix C Table 1 in order to facilitate sustainable development and address issues relating to

congestion and amenity."

The proposed development would not increase the number of bedrooms within the application property, albeit the overall footprint of the building would be increased. There is an existing area of hardstanding to the front of the property which is of a sufficient size to accommodate two off-street car parking spaces, in line with the Council's parking standards as set out in Appendix C Table 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020). On this basis, it is considered that the proposal would not significantly exacerbate the demand for street parking or prejudice highway safety, in accordance with DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

#### FLOOD RISK AND DRAINAGE:

Policy DMEI 9 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused.

The site is located in Flood Zone 1 where the risk of flooding from rivers or seas is low. As such, all forms of development, including extensions to residential properties, are acceptable in this location, in terms of fluvial flood risk. According to the Council's GIP map, the site lies within a critical drainage area. However, given the limited footprint of the proposed extension, it is unlikely that the proposal would significantly exacerbate the surface water flood risk at the site over and beyond the existing circumstances. The proposal would therefore comply with Policies DMEI 9 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

#### CONCLUSION:

The main body of the report demonstrates that the revised proposal complies with the relevant policy objectives set out in the Hillingdon Local Plan: Part One Strategic Policies (2012) and Part Two - Development Management Policies (2020). It is therefore considered, taking all matters into account including all representations received, that planning permission should be granted subject to conditions.

## 6. RECOMMENDATION

### **APPROVAL subject to the following:**

#### **1** HO1 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

#### REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

#### **2** HO2 Accordance with approved

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted drawings, titled Location Plan and numbered 69112 - 1500 B, 69112 - 3100 B, 69112 - 3101 B, 69112 - 3200 B, 69112 -

3201 B and 69112 - 3300 B.

**REASON**

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020), and the London Plan (2016).

**3 HO4 Materials**

The development hereby approved shall be carried out in accordance with the materials specified on the approved plans and on the application form unless otherwise agreed in writing by the Local Planning Authority.

**REASON**

To safeguard the visual amenities of the area and to ensure that the proposed development does not have an adverse effect upon the appearance of the existing building in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020)

**4 HO6 Obscure Glazing**

The first floor side window serving the stairway as detailed on drawing number 69112 - 3100 B shall be glazed with permanently obscured glass to at least scale 4 on the Pilkington scale and be non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**INFORMATIVES**

- 1** On this decision notice policies from the Councils Local Plan: Part 1 - Strategic Policies appear first, then relevant Local Plan Part 2 (2020), then London Plan Policies (2016). Hillingdon's Full Council adopted the Hillingdon Local Plan: Part 1 - Strategic Policies on 8 November 2012 and the Hillingdon Local Plan Part 2 on 16 January 2020.
- 2** The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

For Private Roads: Care should be taken during the building works hereby approved to ensure no damage occurs to the verge of footpaths on private roads during construction. Vehicles delivering materials to this development shall not override or cause damage to a private road and where possible alternative routes should be taken to avoid private roads. The applicant may be required to make good any damage caused.

- 3 Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit ([www.hillingdon.gov.uk/noise](http://www.hillingdon.gov.uk/noise) Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

### Standard Informatives

- 1 The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).
- 2 The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

#### Part 1 Policies:

PT1.BE1 (2012) Built Environment

#### Part 2 Policies:

DMHB 11 Design of New Development  
DMHB 12 Streets and Public Realm  
DMHB 18 Private Outdoor Amenity Space  
DMHD 1 Alterations and Extensions to Residential Dwellings  
DMT 6 Vehicle Parking

LPP 3.5 (2016) Quality and design of housing developments

- 3 You are advised this permission is based on the dimensions provided on the approved drawings as numbered above. The development hereby approved must be constructed precisely in accordance with the approved drawings. Any deviation from these drawings requires the written consent of the Local Planning Authority.
- 4 You are advised that if any part of the development hereby permitted encroaches by either its roof, walls, eaves, gutters, or foundations, then a new planning application may have to be submitted. The validity of this planning permission may be challengeable by third parties if the development results in any form of encroachment onto land outside the applicant's control that is considered to cause harm to local amenity.
- 5 Your attention is drawn to the need to comply with the relevant provisions of the Building Regulations, the Building Acts and other related legislation. These cover such works as - the demolition of existing buildings, the erection of a new building or structure, the extension or alteration to a building, change of use of buildings, installation of services, underpinning works, and fire safety/means of escape works. Notice of intention to demolish existing buildings must be given to the Council's Building Control Service at least 6 weeks before work starts. A completed application form together with detailed plans must be submitted for approval before any building work is commenced. For further information and advice, contact - Planning, Environment and Community Services, Building Control, 3N/01 Civic Centre, Uxbridge (Telephone 01895 250804 / 805 / 808).
- 6 You have been granted planning permission to build a residential extension. When undertaking demolition and/or building work, please be considerate to your neighbours and do not undertake work in the early morning or late at night or at any time on Sundays or Bank Holidays. Furthermore, please ensure that all vehicles associated with the construction of the development hereby approved are properly washed and cleaned to prevent the passage of mud and dirt onto the adjoining highway. You are advised that the Council does have formal powers to control noise and nuisance under The Control of Pollution Act 1974, the Clean Air Acts and other relevant legislation. For further information and advice, please contact - Environmental Protection Unit, 4W/04, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 250190).
- 7 The Party Wall Act 1996 requires a building owner to notify, and obtain formal agreement from, any adjoining owner, where the building owner proposes to:
  - carry out work to an existing party wall;
  - build on the boundary with a neighbouring property;

- in some circumstances, carry out groundworks within 6 metres of an adjoining building.

Notification and agreements under this Act are the responsibility of the building owner and are quite separate from Building Regulations, or Planning Controls. The Building Control Service will assume that an applicant has obtained any necessary agreements with the adjoining owner, and nothing said or implied by the Council should be taken as removing the necessity for the building owner to comply fully with the Party Wall Act. Further information and advice is to be found in "the Party Walls etc. Act 1996 - explanatory booklet" published by the ODPM, available free of charge from the Planning, Environment and Community Services Reception, Civic Centre, Uxbridge, UB8 1UW.

8 Your attention is drawn to the fact that the planning permission does not override property rights and any ancient rights of light that may exist. This permission does not empower you to enter onto land not in your ownership without the specific consent of the owner. If you require further information or advice, you should consult a solicitor.

9 Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with: -

A) Demolition and construction works should only be carried out between the hours of 08.00 hours and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays Bank and Public Holidays.

B) All noise generated during such works should be controlled in compliance with British Standard Code of Practice BS 5228: 1984.

C) The elimination of the release of dust or odours that could create a public health nuisance.

D) No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit, 3S/02, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel.01895 277401) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

10 You are advised that care should be taken during the building works hereby approved to avoid spillage of mud, soil or related building materials onto the pavement or public highway. You are further advised that failure to take appropriate steps to avoid spillage or adequately clear it away could result in action being taken under the Highways Act.

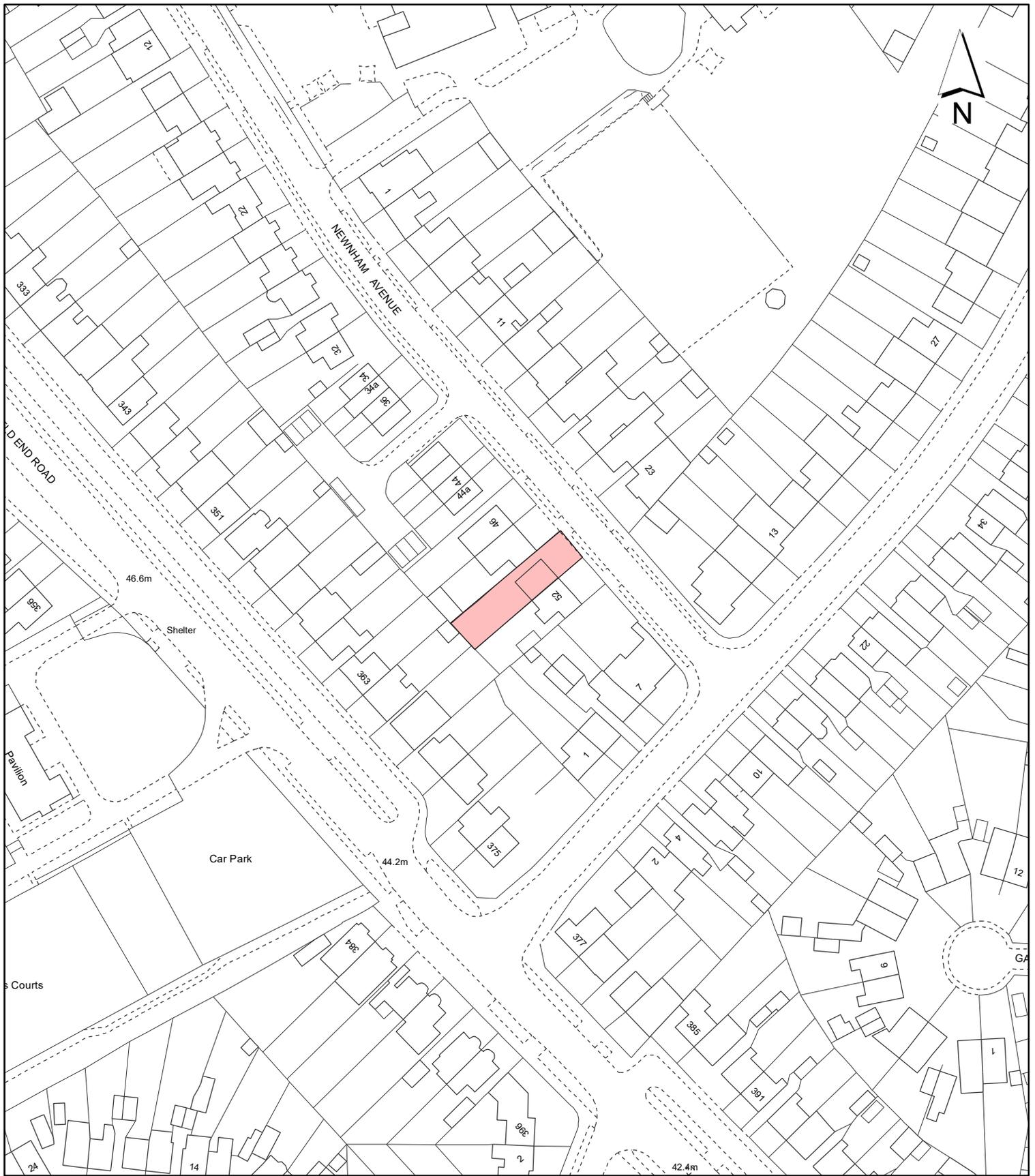
11 To promote the development of sustainable building design and construction methods, you are encouraged to investigate the use of renewable energy resources which do not produce any extra carbon dioxide (CO2) emissions,

including solar, geothermal and fuel cell systems, and use of high quality insulation.

- 12 You are advised that care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense. For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

**Contact Officer:** Nesha Burnham

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**50 Newnham Avenue**

**LONDON BOROUGH OF HILLINGDON**  
**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**70191/APP/2021/174**

Scale:

**1:1,250**

Planning Committee:

**North Page 23**

Date:

**March 2021**



**HILLINGDON**  
 LONDON

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## Report of the Head of Planning, Transportation and Regeneration

**Address** 169 JOEL STREET EASTCOTE PINNER

**Development:** Demolition of the existing dwelling and erection of a part one, part two and part three-storey building comprising 9 flats (3 x 1-bed, 5 x 2-bed and 1 x 3-bed) with 4 parking spaces.

**LBH Ref Nos:** 22642/APP/2020/3181

**Drawing Nos:** JSP P20 03  
JSP P20 04  
Photograph of Front View  
JSP P20 05  
JSP P20 20  
JSP P20 21  
JSP P20 06  
JSP P20 07  
JSP P20 08  
JSP P20 10  
JSP P20 11  
JSP P20 13  
JSP P20 12  
JSP P20 09  
JSP P20 02  
JSP P20 01  
Planning Design and Access Statement.  
Daylight Sunlight and Overshadowing Assessment(1)  
Transport Statement - September 2020

**Date Plans Received:** 05/10/2020      **Date(s) of Amendment(s):**

**Date Application Valid:** 05/10/2020

### 1. SUMMARY

The application proposes the demolition of the existing dwelling and the erection of a part one, part two and part three-storey building comprising 9 flats (3 x 1-bed, 5 x 2-bed and 1 x 3-bed) with 4 parking spaces.

Overall the development would constitute an overdevelopment of the site which would fail to provide an adequate level of off-street parking and would result in the provision of substandard accommodation. Furthermore the forward position of the development coupled with its significant width close to the site boundaries as well as its detailed design would be harmful to the character and appearance of the site, the street scene and its wider setting. The development would also result in a perceived loss of privacy to both the neighbouring properties at 167 and 171 Joel Street.

### 2. RECOMMENDATION

**REFUSAL for the following reasons:**

#### 1 NON2 Non Standard reason for refusal

The proposed development, by reason of the glazing at roof level and its width, proximity

to neighbouring boundaries and forward position, would result in a poor, incongruous and overly dominant form of development that would be detrimental to the character, appearance and visual amenities of the site, the street scene and the wider area. The proposal is therefore contrary to Policies BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (November 2012), Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan Part Two - Development Management Policies (January 2020), Policy D4 of the London Plan (2021) and the NPPF (2019).

**2 NON2 Non Standard reason for refusal**

The proposal would result in the provision of windows to habitable rooms that would be obscure glazed and there would be no outlook from these bedroom windows resulting in harm to the amenity of future occupants. The development would therefore be contrary to Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and Policy D6 of the London Plan (2021) and the NPPF (2019).

**3 NON2 Non Standard reason for refusal**

The proposed development, by virtue of its poor external layout would fail to provide an adequate level of external amenity for future residents in accordance with the Council's standards to the detriment of the amenity of future occupants. The development is therefore contrary to Policies DMHB 11 and DMHB 18 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policy D6 of the London Plan (2021) and the NPPF (2019).

**4 NON2 Non Standard reason for refusal**

The proposal would result in the provision of several habitable rooms facing towards the side elevations of both of the neighbouring properties resulting in loss of or perceived loss of privacy to the occupants of those dwellings harmful to their residential amenity. The development would therefore be contrary to Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

**5 NON2 Non Standard reason for refusal**

The lack of an internal lift to provide adequate access to all users of the development would constitute a substandard form of development, harming the residential amenity of future occupants. The development would therefore be contrary to DMHB 11 and DMHB 16 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and Policy D5 of the London Plan (2021).

**6 NON2 Non Standard reason for refusal**

The proposed development fails to provide sufficient off-street parking, and therefore the development is considered to result in substandard car parking provision, potentially leading to undue on-street parking to the detriment of public and highway safety contrary to Policies DMT 1, DMT 2 & DMT 6 of the Local Plan: Part 2 Development Plan (2020) and Policies T6 and T6.1 of the London Plan (2021).

**INFORMATIVES**

**1 I52 Compulsory Informative (1)**

The decision to REFUSE planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

## 2 I53 Compulsory Informative (2)

The decision to REFUSE planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan - The Spatial Development Strategy for London consolidated with alterations since 2011 (2016) and national guidance.

DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 15	Planning for Safer Places
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP D8	(2021) Public realm
LPP GG1	(2021) Building strong and inclusive communities
LPP GG2	(2021) Making the best use of land
LPP H1	(2021) Increasing housing supply
LPP H10	(2021) Housing size mix
LPP H9	(2021) Ensuring the best use of stock
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF- 2	NPPF-2 2018 - Achieving sustainable development
NPPF- 5	NPPF-5 2018 - Delivering a sufficient supply of homes
NPPF- 11	NPPF-11 2018 - Making effective use of land
NPPF- 12	NPPF-12 2018 - Achieving well-designed places

### 3. CONSIDERATIONS

#### 3.1 Site and Locality

The application site comprises a large, detached two storey house located on the eastern side of Joel Street, Pinner. The property is of brick construction partly finished in painted white render. It has a double gable and a hipped dormer to it's front elevation. The property is situated on a substantial plot which backs onto the rear garden of No.3 Middleton Drive. To the site frontage is a large area of hardstanding used for parking.

The neighbouring property to the north (No. 167) is a semi-detached house situated on a narrow plot which has been extended at the rear. The neighbouring property to the south at No. 171 is also a semi-detached house located on a narrow plot. Within the rear garden of this property is a large, detached outbuilding located against the boundary with the application site. Both of these properties sit forward of the front elevation of the application property and form part of the primary street pattern on Joel Street.

The area is residential in character where there is a mix of houses styles. The site is located within a designated Critical Drainage Area.

### 3.2 Proposed Scheme

The application proposes the demolition of the existing dwelling and the erection of a part one, part two and part three-storey building comprising 9 flats (3 x 1-bed, 5 x 2-bed and 1 x 3-bed) with 4 parking spaces. In terms of it's detailed design the development would incorporate a double gable and a dormer to it's front elevation and would occupy almost the full width of the plot. It would have a crown roof design with large glazed openings at roof level fronting the street. A small area with 4 parking spaces would be provided to the site frontage with a large amenity area located to the rear.

### 3.3 Relevant Planning History

22642/A/78/2013            169 Joel Street Eastcote Pinner

Alterations to elevation (P)

**Decision:** 06-03-1979    Approved

22642/APP/2011/1472    169 Joel Street Eastcote Pinner

Part two storey, part single storey side/ rear extension and two storey front extension with 4 rooflights to rear and 5 rooflights to side

**Decision:** 22-12-2011    Withdrawn

22642/APP/2011/3098    169 Joel Street Eastcote Pinner

Part two storey part single storey front/side extension and single storey rear extension with 3 x roof lanterns

**Decision:** 13-02-2012    Approved

22642/APP/2013/712    169 Joel Street Eastcote Pinner

Part two storey, part single storey side/rear extension with 3 roof lanterns and single storey front extension

**Decision:** 21-05-2013    Approved

22642/APP/2014/2278    169 Joel Street Eastcote Pinner

Boundary wall with iron railings to front, including electronic iron gates and pedestrian gate and involving soft landscaping (Part Retrospective)

**Decision:** 10-10-2014    Refused                    **Appeal:** 28-01-2015    Allowed

22642/APP/2014/94        169 Joel Street Eastcote Pinner

Boundary wall with iron railings to front and side to include electric iron gates and pedestrian gates involving removal of hedge

**Decision:** 31-03-2014 Refused

22642/C/85/1961 169 Joel Street Eastcote Pinner  
Householder development - residential extension(P)

**Decision:** 29-01-1986 Approved

22642/F/87/0379 169 Joel Street Eastcote Pinner  
Detached double garage

**Decision:** 23-04-1987 Approved

#### **Comment on Relevant Planning History**

The site has an extensive planning history relating to extensions and alterations to the site. There are no planning history records particularly relevant to this submission. Extensions to 167 Joel Street were granted planning permission in 1994.

#### **4. Planning Policies and Standards**

Development Plan

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)  
The Local Plan: Part 2 - Development Management Policies (2020)  
The Local Plan: Part 2 - Site Allocations and Designations (2020)  
The West London Waste Plan (2015)  
The London Plan (2021)

Material Considerations

The National Planning Policy Framework (NPPF) (2019) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

#### **UDP / LDF Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

PT1.H1 (2012) Housing Growth

Part 2 Policies:

DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 15	Planning for Safer Places
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP D8	(2021) Public realm
LPP GG1	(2021) Building strong and inclusive communities
LPP GG2	(2021) Making the best use of land
LPP H1	(2021) Increasing housing supply
LPP H10	(2021) Housing size mix
LPP H9	(2021) Ensuring the best use of stock
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF- 2	NPPF-2 2018 - Achieving sustainable development
NPPF- 5	NPPF-5 2018 - Delivering a sufficient supply of homes
NPPF- 11	NPPF-11 2018 - Making effective use of land
NPPF- 12	NPPF-12 2018 - Achieving well-designed places

## **5. Advertisement and Site Notice**

**5.1** Advertisement Expiry Date:- Not applicable

**5.2** Site Notice Expiry Date:- Not applicable

## **6. Consultations**

### **External Consultees**

A total of 36 adjoining and nearby neighbouring properties as well as the Northwood Hills Residents Association were consulted via letter dated 12/10/20 giving 21 days for comments. A total of 29 objections and a petition with 54 signatures were received, commenting as follows:-

-The development would result in noise pollution.

-Increased traffic and congestion as a result of lack of parking.

- Middleton Drive already has insufficient parking for its own residents and their visitors, which causes untold congestion and compromises safety for the residents and their children.

- Joel Street already has insufficient parking along that part of the street
- Loss of outlook and privacy.
- The development will be out of character with the area in terms of density and appearance.
- Lack of space for refuse bins.
- Poor visibility for pedestrians
- Lack of space for emergency vehicles.
- The development will detract from the locally listed Joel Street Farm House

Officer Comment: It should be noted that a significant number of the 29 individual objections not only involved objections to the low parking ratio, but residents highlighting what they considered to be severe local parking stress.

Councillor Flynn requested that this application be determined at committee level due to concerns of overdevelopment and impact on car parking in the area.

### **Internal Consultees**

#### **ACCESS OFFICER:**

Having reviewed this application, it is clear that step free access to the proposed dwellings above ground floor would not be possible for wheelchair users and other persons unable to use a staircase. Given that lift access is not proposed, the application should not be supported on the grounds of non-compliance with Policy D5 of the London Plan (2021). Conclusion: unacceptable. The scheme should be redesigned to incorporate lift access.

#### **HIGHWAYS AND TRANSPORTATION:**

##### **Site Characteristics & Background**

The site is located within a residential catchment on Joel Street in Eastcote which is one of the main arterial routes in the borough and is therefore designated as 'classified' in the Council's hierarchy of roads. There is an existing detached dwelling which is to be demolished with a proposed replacement build that will consist of a single building containing 9 flatted units (3x1, 5x2 & 1x3 bedrooms). A single carriageway crossing that serves the site envelope will be extinguished and a new 'centralised' crossing/access will be provided to serve the new build.

The address is generally devoid of on-street parking controls and exhibits a public transport accessibility level (PTAL) rating of 2 which is considered low and therefore heightens dependency on the ownership and use of private motor transport.

##### **Parking Provision & Internal Road Layout**

Local Plan: Part 2 Policy - DMT 6 requires that new development will only be permitted where it accords with the Council's adopted parking standards unless it can be demonstrated that a deviation from the standard would not result in a deleterious impact on the surrounding road network.

It is proposed to provide 9 residential flats comprised of 3x1, 5x2 & 1x3 bedroom units. The maximum standard requires 1-1.5 spaces per unit (for flats up to a scale of 2 bedrooms) with 2 spaces per flat for the larger 3 bedroom units. Hence a quantum of between 10-14 spaces should be provided on-site in order to comply with the adopted parking standard. The proposed quantum of 4 spaces is therefore significantly below this requirement.

Determination of the acceptability (or otherwise) of a lower on-plot parking provision can be reliant on a number of extraneous factors to the site itself. The local characteristics of the surrounding network is one of the main factors that needs to be taken into consideration as this can significantly influence

the need for new residential occupiers owning private motor transport.

With this particular location, it is apparent that the vast majority of the surrounding highly trafficked 'classified' road network is devoid of parking restrictions which would allow for an unfettered use of the highway by any displaced vehicles affiliated to the proposal and would therefore be potentially prejudicial to traffic free flow and highway safety. In addition the low PTAL rating encourages ownership and use of private motor transport which would lead to a higher car ownership level at this address resulting in additional on-street parking.

The applicant has undertaken parking stress surveys earlier this year and indicates that spare capacity exists on the local highway network. However this does not remedy the fact that vehicles related to the site are likely to be displaced onto the immediate highway which is designated as 'classified' and is therefore a highly trafficked thoroughfare. Such displacement has the potential to be obstructive and detrimental to general road safety which is heightened by the category of road and hence cannot be condoned.

The applicant also cites a number of local appeal decisions where the Planning Inspectorate had accepted a lower on-plot parking provision for flatted proposals (albeit for a smaller number of units). However it is considered that each site should be appraised on its merits and given that a number of local sites received permission upon appeal with the Inspectors acknowledgement that there would be a level of on-street displacement, this specific proposal would further add to displaced vehicle numbers. This cumulative impact does raise issue on obstruction and safety grounds and for that reason is not supported by this authority.

For the above reasons there is a refusal reason put forward on the basis of on-plot parking under-provision.

Notwithstanding the above - the following applies to the proposal as presented:-

#### Electric Vehicle Charging Points (EVCPs)

In line with the emerging London Plan, within any final parking quantum there is a requirement for a 20% 'active' EVCP provision with all remaining spaces being designated as 'passive' provisions. This would equate to 1 EVCP allocated for 'active' provision with the remaining 3 spaces designated as 'passive' provisions. This requirement can be secured via planning condition.

#### Disabled Compliant Parking Provision

In accord with the parking standard - 10% of parking spaces should be disabled compliant equating to 1 space. This is not indicated on-plan and would require rectification with an increase in one of the parking bay widths. Once appropriately depicted on plan (or secured by planning condition) this level of provision would be considered acceptable.

#### Cycling Provision

In terms of cycle parking there should be a provision of 1 secure and accessible space for each of the 1-2 bedroom flats with 2 spaces for the 3 bedroom unit (totalling 10 spaces) in order to conform to the adopted minimum borough cycle parking standard. 20 are proposed with an acceptable positioning of a secure and accessible compound to the rear of the site. This level of provision is therefore considered satisfactory.

#### Vehicular Access Arrangements

The existing single carriageway crossing would be made redundant and the new site layout would

demand a new 'central' access and carriageway crossing on Joel Street. In terms of dimension and positioning, the proposal conforms to the Council's 'Domestic Vehicle Footway Crossover' Policy. It is highlighted that once the existing access point is extinguished it will be necessary to reinstate raised kerbing on the public footway in order to maintain footway/roadway continuity. Final provisions would be arranged and implemented post-permission at the applicant's expense via a formal/legal agreement i.e. under s184 of The Highways Act 1980 or suitable alternative arrangement.

In highway safety terms, effort should be made to maintain a relatively low frontage wall treatment onto Joel Street in order to help ensure conformity to the relevant mutual inter-visibility sight-line requirements, as per DfT (Manual for Streets (MfS) circa 2007) best practice for new development road and parking layouts guidance, between vehicles leaving the site and extraneous vehicles/pedestrians on Joel Street itself. In order to assist with improving sight-lines at the new site entrance for vehicles and pedestrians, it is therefore recommended that the front boundary wall height should not exceed 0.6m for at least 1-2m on either side of the new opening in order to achieve the aim of satisfactory visibility. This aspect can be secured via planning condition. There are no further observations made on this aspect.

#### Vehicular Trip Generation

Local Plan: Part 2 Policies - DMT 1 and DMT 2 require the Council to consider whether the traffic generated by proposed developments is acceptable in terms of the local highway and junction capacity, traffic flows and conditions of general highway or pedestrian safety.

The proposal would marginally increase traffic generation from the site as compared to the existing single dwelling unit. However peak period traffic movement into and out of the site is expected to rise by up to 2-3 additional vehicle movements during the peak morning and evening hours hence this uplift is considered marginal in generation terms and therefore can be absorbed within the local road network without notable detriment to traffic congestion and road safety.

#### Operational Refuse Requirements

Refuse collection will continue to be conducted via Joel Street. In order to conform to the Council's 'waste collection' maximum distance collection parameter of 10m i.e. distance from a refuse vehicle to the point of collection, arrangements should ensure that waste bin storage is positioned at a collection point within this set distance. In addition the maximum carrying distance from each flat should not exceed 30m. A specific bin store location is depicted on plan on the site frontage hence the 10m distance parameter from the highway is met. There are no further observations.

#### Construction/Demolition Logistics Plan (CLP)

A full and detailed CLP will be a requirement given the constraints and sensitivities of the local residential and main road network in order to minimize/avoid potential detriment to the public realm. It will need to be secured under a suitable planning condition.

#### Conclusion

Refusal on insufficient on-plot parking grounds is recommended as follows:-

"The proposed development fails to provide sufficient off-street parking, and therefore the development is considered to result in substandard car parking provision, potentially leading to undue on-street parking to the detriment of public and highway safety contrary to Local Plan: Part 2 Development Plan (2020) Policies DMT 1, DMT 2 & DMT 6 and Policies T6 and T6.1 of the London Plan (2021)."

## TREE OFFICER:

This site is occupied by a substantial two-storey dwelling on a double width plot located on the east side of Joel Street, close to the junction with Middleton Drive. The deep front garden has almost totally been hard paved to provide parking and manoeuvring space for cars. According to the aerial photographs the back garden is largely laid to grass with a couple of outbuildings. There are no TPO's or Conservation Area designations affecting the site. COMMENT No trees or other landscape features of merit will be affected by the proposal. The proposed layout indicates an improvement to the local streetscape with a reduced size car park and soft landscape enhancement of the front boundary. Cycle storage is to the rear. The back garden should be designed and set out to be accessible, robust and attractive for shared use as a communal garden.

RECOMMENDATION No objection subject to conditions RES9 (parts 1,2,4 and 5).

## PLANNING POLICY TEAM

Policy DMH4 of the Hillingdon Local Plan: Part Two (LPP2) states that residential conversions and the redevelopment of dwellings into new blocks of flats will only be permitted where:

- i) it is on a residential street where the proposal will not result in more than 10% of properties being redeveloped into flats;
- ii) On residential streets longer than 1km the proposed redevelopment site should be taken as the midpoint of a 1km length of road for assessment purposes;
- iii) the internal floor area of the original building to be converted is at least 120 sqm; and
- iv) units are limited to one unit per floor for residential conversions. Points iii) and iv) do not apply in this case as the scheme is a proposed redevelopment of the site rather than a conversion.

The onus is on applicants to demonstrate that point i) of Policy DMH4 is met and more than 10% of properties have not been redeveloped into flats.

## Affordable Housing

Part D iii) of Policy DMH7 of the LPP2 states that in order to ensure that local affordable housing policy is applied consistently and fairly on all proposed housing developments, the requirement for affordable housing will apply to additional units created through or subsequently amended planning applications, whereby the amount of affordable housing required will be calculated based on the new total number of units on the site. Affordable housing will be required where a development under the 10 unit threshold is amended to have 10 or more housing units in total (gross).

The currently proposed development is for 9 units, which is below the 10 unit threshold for affordable housing provision, however any future amendments to the scheme which take it above the 10 unit threshold will trigger the application of Part D iii) of Policy DMH7.

## Housing Mix

Policy DMH2 of the HLPP2 states that the Council will seek a housing mix which reflects the needs identified in the most up to date housing needs assessment. The Council's current information on housing need set out in the SHMA 2016 indicates a substantial borough-wide requirement for larger affordable and private market units, particularly three bedroom properties. The currently proposed housing mix is for 3 x 1 bed, 5 x 2 bed and 1 x 3 bed dwellings. A higher number of 3+ bed dwellings would be ideal however given the size of the site and the need to provide parking and amenity space, the proposed mix may be suitable, subject to other considerations. However, in accordance with policy DMH 2, developments should demonstrate how the provision of family housing (>3 bedroom units) has been optimised in relation to this site and demonstrate that this is the most appropriate mix for the site taking local housing needs into consideration.

## Density

The density of the development should be assessed against table 5.2 of the Hillingdon Local Plan Part Two: Development Management Policies. The densities outlined in this table are a starting point and the density standards will be applied in a flexible manner, according to local circumstances. The site is approximately 0.1ha in size and would be classed as a residential area with suburban character within 800m of a town centre. According to table 5.2, the proposed development is within the generally acceptable density range of 50-110 u/ha.

## Private amenity space

As stated in policy DMHB 18, all new residential development should provide good quality and useable private outdoor amenity space. Amenity standards should be provided in accordance with the standards set out in table 5.3 of the Local Plan: Part Two (2020).

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

The use of previously development land for housing and in particular family housing is supported by the Council's Local Plan, Furthermore increasing the housing supply is also supported by the London Plan and the National Planning Policy Framework. As such there are no in principle objections to the creation of additional residential units in this location in land use terms, subject to compliance with all other relevant Local Plan and London Plan policies as well as the NPPF.

### **7.02 Density of the proposed development**

Notwithstanding the comments provided by the Council's Policy Team which state that the density of the development would be in line with the general criteria contained within the London Plan, given the concerns raised with respect to parking and the internal and external amenity of the development it is considered that this would constitute an overdevelopment of the site. These matters are discussed further in the report below.

### **7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

Not applicable to this application.

### **7.04 Airport safeguarding**

Not applicable to this application.

### **7.05 Impact on the green belt**

Not applicable to this application.

### **7.07 Impact on the character & appearance of the area**

Policy DMHB 12 of the Hillingdon Local Plan: Part Two requires all new development to maintain the quality of the built environment including providing high quality urban design. Furthermore the policy resists any development which would fail to harmonise with the existing street scene and seek to ensure any new development complements the amenity of the area.

There are many similarities between the existing and proposed buildings. For example the overall height and width of the proposed building would almost be the same as existing. In terms of detailed design and appearance the proposal would have a similar roof form and would include a double gable and front dormer. Generally it is considered that the overall appearance of the building would be acceptable.

Notwithstanding this, however the development would include two, large glazed openings at roof level which would be at odds with the traditional character of the existing and proposed buildings as well as those in the surrounding area. As a result these features

would appear incongruous within the streetscene.

The existing house is the largest house in the street and differs significantly in terms of its width, scale and footprint to the other houses in the immediate street scene which are much smaller properties situated on narrower plots. At present the house does not appear overly dominant or cramped within its setting because it is set back such a substantial distance from the street. This proposal would be much closer to the street. As discussed above it would be level with the neighbouring property at No. 171 and just behind the front elevation at No. 167. As a result it would appear cramped within its setting given its substantial width and close proximity to the neighbouring properties. The building would completely dwarf the neighbouring dwellings in terms of its bulk, massing and scale and would lead to a 'terracing effect'. The building would be in stark contrast with the modest character and appearance of the neighbouring properties and the those in the surrounding area and its forward position would exacerbate the situation.

In terms of the layout of the site, although there is little opportunity for soft landscaping in the site frontage it is noted that there is already a large area of hardstanding to the front of the existing property.

Concerns have been raised that the development would harm to setting of the locally listed building at Joel Street Farm House. This property is over 100m from the site as such there are no particular concerns relating to the impact upon the setting of this property.

Overall it is considered that the development in terms of its detailed design, specifically the glazed openings at roof level as well as its proximity to the street would be harmful to the character and appearance of the existing site, the streetscene and the wider area, contrary to policies DMHB 11 and DMHB 12 of the Local Plan.

#### **7.08 Impact on neighbours**

Policy DMHB 11 of the Hillingdon Local Plan: Part Two states that buildings should be laid out so that adequate daylight and sunlight can penetrate into and between them, and the amenities of existing houses are safeguarded. Furthermore the policy stresses the importance of new buildings and extensions providing adequate amount of external amenity space, that not only protects the amenity of the occupants of the proposed development, but also of those of the surrounding buildings, as well as protecting both parties privacy.

At present the existing building is situated a substantial distance from the street. It is set back to a point where the front elevation of the existing dwelling is almost level with the rear elevation of the neighbouring properties. As a result the existing building extends a significant depth from the main rear elevations of the neighbouring properties.

The proposed building would be positioned further forward towards the street and would be a similar height and depth as the existing building. As a result the proposed building would not extend as far beyond the rear elevations of the neighbouring properties and given its similar overall height it is considered that the proposal would not give rise to significant loss of light or outlook to the rear windows located within the neighbouring semi-detached dwellings.

There are side facing windows at both of the neighbouring houses that would be affected by the proposal, however it would appear that these are non habitable or secondary windows with the primary windows located to the front and rear of the house. As such it is considered that moving the built form closer to the side elevations of these dwellings is unlikely to result in harm to the residential amenity of their occupants in terms of loss of

light or outlook.

However there is a concern in relation to the loss of privacy or a perceived loss of privacy as a result of various windows proposed to the side elevations of the development which would directly face towards the neighbours windows as discussed above. The plans indicate that all side facing windows would be obscure glazed, however some of their windows would serve habitable rooms such as bedrooms and kitchens, therefore it would not be appropriate to have them obscure glazed (discussed further below). Moreover although they are indicated as being obscure glazed they would not be fixed shut as such it would be possible for future occupants to directly look out towards the side facing windows at the neighbouring properties at close proximity.

Concerns have been raised that the development would involve the introduction of balconies to the rear elevation of the property. However the proposed development would be well over 21 metres from the houses to the rear and the balconies would be obscure glazed on each side in order to ensure that there is no direct overlooking into the neighbouring gardens. Further screening could be secured by way of condition in the event of an approvable scheme.

In terms of impact upon the front elevation windows at the neighbouring properties it is considered that there would not be a negative impact given that the main building line would be level with No. 171 and the proposed building would sit back from the front elevation of No. 167.

Concerns have been raised regarding the general noise and disturbance that would be generated during construction and from the additional residents residing at the property particularly during the Covid 19 pandemic. Given that this is a residential area and this is a residential development it is considered that the increase in the number of residents would not give rise to general noise and disturbance. In the event of an approvable scheme planning conditions would be recommended with respect to restricting hours of construction.

Overall it is considered that the development would result in harm to the amenity of both of the neighbouring properties at No. 167 and No.171 by way of loss of privacy, contrary to policy DMHB 11 of the Council's adopted Local Plan.

#### **7.09 Living conditions for future occupiers**

In terms of floor area, each of the proposed flats would provide an adequate level of internal amenity. However as discussed above there are a number of bedrooms that are located facing towards the neighbouring properties and in order to reduce any loss of privacy that may arise these windows are to be mainly obscure glazed. It is considered that obscure glazing on bedroom windows which are primary habitable rooms is completely inappropriate. The result is that these rooms would not be afforded an adequate level of outlook harmful to the amenity of the future occupants of the development.

Furthermore the Council's Access Officer has commented as follows:-

'Having reviewed this application, it is clear that step free access to the proposed dwellings above ground floor would not be possible for wheelchair users and other persons unable to use a staircase. Given that lift access is not proposed, the application should not be supported on the grounds of non-compliance with Policy D5 of the London Plan (2021)'.

Policy DMHB 18 of the Council's adopted Local Plan states that new residential

developments should provide an adequate level of private amenity space for occupiers that is good quality and usable. A large area of communal garden space would be provided with all of the ground floor flats having direct access from within the building. All but one of the flats on the upper floors would have access to a terrace area. The flat without access to a terrace at first floor level to the front of the building would not have any direct access to a form of private amenity space. Residents of this flat would have to navigate the stairs and then walk along the corridor to the rear of the building. This would be unacceptable and contrary to policy DMHB 18.

#### **7.10 Traffic impact, Car/cycle parking, pedestrian safety**

Policy DMT1 of the Hillingdon Local Plan: Part 2 considers whether the traffic generated by proposed developments is acceptable in terms of the local highway and junction capacity, traffic flows and conditions of general highway or pedestrian safety. Policy DMT 6 of the Hillingdon Local Plan seeks to ensure that all development is in accordance with the Council's adopted Car Parking Standards.

As per the Council's Highways Engineers comments the site is generally devoid of on-street parking controls and exhibits a public transport accessibility level (PTAL) rating of 2 (low) and therefore heightens dependency on the ownership and use of private motor transport.

It is proposed to provide 9 residential flats comprised of 3x1, 5x2 & 1x3 bedroom units. The maximum standard requires 1-1.5 spaces per unit (for flats up to a scale of 2 bedrooms) with 2 spaces per flat for the larger 3 bedroom units. Hence a quantum of between 10-14 spaces should be provided on-site in order to comply with the adopted parking standard. The proposed quantum of 4 spaces is therefore significantly below this requirement. As the Council's Highways Officer states:-

'Determination of the acceptability (or otherwise) of a lower on-plot parking provision can be reliant on a number of extraneous factors to the site itself. The local characteristics of the surrounding network is one of the main factors that needs to be taken into consideration as this can significantly influence the need for new residential occupiers owning private motor transport.

With this particular location, it is apparent that the vast majority of the surrounding highly trafficked 'classified' road network is devoid of parking restrictions which would allow for an unfettered use of the highway by any displaced vehicles affiliated to the proposal and would therefore be potentially prejudicial to traffic free flow and highway safety. In addition the low PTAL rating encourages ownership and use of private motor transport which would lead to a higher car ownership level at this address resulting in additional on-street parking.

The applicant has undertaken parking stress surveys earlier this year and indicates that spare capacity exists on the local highway network. However this does not remedy the fact that vehicles related to the site are likely to be displaced onto the immediate highway which is designated as 'classified' and is therefore a highly trafficked thoroughfare. Such displacement has the potential to be obstructive and detrimental to general road safety which is heightened by the category of road and hence cannot be condoned.

It should be noted that the application has a significant number of individual objections and a high proportion of the objections relate to concerns regarding the parking ratio and reference existing parking stress.

The applicant also cites a number of local appeal decisions where the Planning

Inspectorate had accepted a lower on-plot parking provision for flatted proposals (albeit for a smaller number of units). However it is considered that each site should be appraised on its merits and given that a number of local sites received permission upon appeal with the Inspectors acknowledgement that there would be a level of on-street displacement, this specific proposal would further add to displaced vehicle numbers. This cumulative impact does raise issue on obstruction and safety grounds and for that reason is not supported.

For the above reasons there is a refusal reason put forward on the basis of on-plot parking under-provision'.

It is considered that the level of cycle parking is acceptable as are the general access arrangements. All other matters relating to electric charging vehicles, the provision and a disabled parking bay and issues around the construction and operational arrangements of the site could be secured by way of condition in the event of an approvable scheme.

Overall it is considered that there would be a significant lack of on site parking for the development which would lead to conditions prejudicial to pedestrian and highway safety, contrary to Policies DMT 1 and DMT 2 of the Local Plan.

#### **7.11 Urban design, access and security**

Overall it is considered that the development would result in the provision of substandard accommodation for all future occupants contrary to both Local Plan and London Plan policies. The lack of suitable access to garden space, lift access, issues around the glazing for several of the bedrooms as well as the lack of parking as discussed above mean that it is evident that this would be an overdevelopment of the site.

Refer to 'Impact on the character and appearance of the area' for a full assessment.

#### **7.12 Disabled access**

Refer to 'Living conditions for future occupants'.

#### **7.13 Provision of affordable & special needs housing**

Overall it is considered that there would not be scope to provide a higher number of units on this site that would trigger a requirement for affordable housing to be provided given that the number of units proposed in this case would constitute an overdevelopment of the site.

#### **7.14 Trees, landscaping and Ecology**

The Council's Tree Officer has considered the application and considers that there would be no harm in terms of trees subject to safeguarding conditions.

#### **7.15 Sustainable waste management**

Not applicable to this application.

#### **7.16 Renewable energy / Sustainability**

Not applicable to this application.

#### **7.17 Flooding or Drainage Issues**

In the event of an approvable scheme an informative would be included with respect to the Critical Drainage Area.

#### **7.18 Noise or Air Quality Issues**

Not applicable to this application.

#### **7.20 Planning obligations**

Not applicable to this application.

#### **7.21 Expediency of enforcement action**

Not applicable to this application.

## 7.22 Other Issues

### Health:

In terms of potential health concerns, the applicant has confirmed that the proposed installation complies with the ICNIRP (International Commissions for Non Ionising Radiation Protection) guidelines. Accordingly, in terms of Government policy advice, there is not considered to be any direct health impact. Therefore, further detailed technical information about the proposed installation is not considered relevant to the Council's determination of this application.

## 8. Observations of the Borough Solicitor

### General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

### Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

### Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

### Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic.

Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

#### **9. Observations of the Director of Finance**

Not applicable to this application.

#### **10. CONCLUSION**

Overall the development would constitute an overdevelopment of the site which would fail to provide an adequate level of off-street parking and would result in the provision of substandard accommodation. Furthermore the forward position of the development coupled with its significant width close to the site boundaries as well as its detailed design would be harmful to the character and appearance of the site, the street scene and its wider setting. The development would also result in a perceived loss of privacy to both the neighbouring properties at 167 and 171 Joel Street.

As such the development would be contrary to Local Plan policies DMHB 11, DMHB 12, DMHB 18, DMHB 16, DMT 1, DMT 2, DMT 6 and the London Plan and the NPPF.

Refusal of the application is therefore recommended.

#### **11. Reference Documents**

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)  
Hillingdon Local Plan: Part Two - Development Management policies (January 2020)  
The London Plan (2021)  
National Planning Policy Framework (2019)

**Contact Officer:** Kelly Sweeney

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

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Site Address:

**169 JOEL STREET  
 EASTCOTE  
 PINNER**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**22642/APP/2020/3181**

Scale:

**1:1,250**

Planning Committee:

**North Page 42**

Date:

**March 2021**



**HILLINGDON**  
 LONDON

## Report of the Head of Planning, Transportation and Regeneration

**Address** 33 WIELAND ROAD NORTHWOOD

**Development:** Part two, part three storey rear extension, subterranean garage/services and storage room, structural underpinning, external remodelling including new roof construction

**LBH Ref Nos:** 14787/APP/2020/3531

**Drawing Nos:** Tree Report  
401.109.04.RevD  
401.109.15.RevE  
401.109.01.RevB  
401.109.05.RevB  
401.109.12.RevF  
401.109.11.RevI  
401.109.16.RevF  
401.109.14.RevJ  
Design and Access Statement - P.1-4  
Design and Access Statement - P.5-10  
Design and Access Statement - P.11-16  
Design and Access Statement - P.17-20

**Date Plans Received:** 30/10/2020                      **Date(s) of Amendment(s):**  
**Date Application Valid:** 18/11/2020

### 1. CONSIDERATIONS

#### 1.1 Site and Locality

The application site is located on the South Eastern side of Wieland Road. The application site comprises a 2-storey detached property which includes an integral garage. The property is located on a sloping site and it sits approximately 3 m above road level, and it is set back approximately 22m from the road. A level terrace provides car parking, with a grassed bank sloping down to the road in front. The street scene is residential in character and appearance comprising large detached properties.

The application site lies within the Gatehill Farm Estate Area of Special Local Character. The area on the South Eastern boundary is designated as Green Belt. The site is also covered by Tree Preservation Order (TPO) 172. The application site falls within a Critical Drainage Area.

#### 1.2 Proposed Scheme

The proposal is seeking planning permission for part two/part three storey rear extension, subterranean garage/services and storage room, structural underpinning, external remodelling including new roof construction.

In 2009 an application was approved (Ref: 14787/APP/2009/1154) for the demolition of the

existing dwelling, and erection of a 6 bedroom dwelling with associated parking. The consent allowed a new dwelling to be of timber frame and larger than the footprint of the existing dwellinghouse. In 2012 the granted permission was renewed under application Ref: 14787/APP/2012/1034. During 2018 an application to fell one Spruce and to prune three Oaks on TPO 172 was refused.

The above mentioned previous planning consents are a material consideration in assessing the current application. Nonetheless the 2012 permission has lapsed and in 2020 the Council adopted its new Local Plan Part 2, which is an important material planning consideration.

### 1.3 Relevant Planning History

14787/APP/2009/1154 33 Wieland Road Northwood

Erection of a two-storey, 6 bedroom dwelling with associated parking involving demolition of existing dwelling.

**Decision Date:** 28-08-2009      **Approved**      **Appeal:**

14787/APP/2012/1034 33 Wieland Road Northwood

Application for a new planning permission to replace an extant planning permission in order to extend the time limit for implementation, reference 14787/APP/2009/1154 dated 28/08/2009 (Erection of a two-storey, 6 bedroom dwelling with associated parking involving demolition of existing dwelling)

**Decision Date:** 09-07-2012      **Approved**      **Appeal:**

14787/PRC/2018/144 33 Wieland Road Northwood

2 storey rear extension, subterranean garage/plant room, external remodelling

**Decision Date:** 18-10-2018      **OBJ**      **Appeal:**

14787/PRC/2020/107 33 Wieland Road Northwood

2 storey rear extension, subterranean garage/plant room contemporary external remodelling including shoring up sections of the site with subsidence

**Decision Date:** 05-08-2020      **OBJ**      **Appeal:**

14787/TRE/2004/117 33 Wieland Road Northwood

TO FELL ONE OAK (T3) ON TPO 172

**Decision Date:** 31-10-2005      **Not Determined**      **Appeal:** 31-OCT-05      **Dismissed**

#### Comment on Planning History

14787/APP/2009/1154) for the demolition of existing, and erection of a 6 bedroom dwelling with associated parking. Approved

14787/APP/2012/1034: Application for a new planning permission to replace an extant planning permission in order to extend the time limit for implementation, reference 14787/APP/2009/1154 dated 28/08/2009 (Erection of a two-storey, 6 bedroom dwelling with associated parking involving demolition of existing dwelling). Approval

It should be noted that the 2009/2012 consent related to an innovative modern design replacement dwelling which was considered, overall, to enhance the ASLC due to its very high quality design, which had been carefully negotiated. Although it had a small third floor

element, this was recessed and almost hidden in the middle of the dwelling, such that the dwelling appeared two storey in the streetscene. As such despite its modern appearance the massing was not in itself out of keeping with its surroundings.

## **2. Advertisement and Site Notice**

**2.1** Advertisement Expiry Date:- Not applicable

**2.2** Site Notice Expiry Date:- Not applicable

## **3. Comments on Public Consultations**

4 neighbouring properties, Northwood Hills Resident Association, Northwood Residents Association and Gatehill (Northwood) Residents Association were consulted via letters on 23-11-20. 4 representations and a petition with 26 signatures were received objecting to the proposal on the following grounds;

1. Impact on residential amenity of the neighbouring properties in terms of light, loss of privacy, overshadowing and over-dominance.

2. The proposed dwelling, by reason of its size, scale, bulk, excessive width and proximity to the side boundary, would result in an incongruous addition which would be detrimental to the architectural composition of the original semi-detached dwelling, would be detrimental to the character, appearance and symmetry of the pair of semi-detached houses.

3. The two first floor windows on the side elevation facing my property will have a substantial overview of my house and garden and must be made opaque. My living room windows are around 8 metres from the neighbours. I note that there will be 2 new windows at ground floor level and would expect them to be opaque as they are for a toilet and changing room. The hedge shown is limited to halfway along our property line and then the plan shows Vertical Ivy. Vertical Ivy will offer no privacy to our side and we strongly object. The hedge detailed should be continuous all the way until the Laurels and the Vertical Ivy omitted. Since the original hedge was removed we have been very overlooked. The height of the roof will be overpowering and will obstruct my line of sight and take away any view I have from my bedroom . If additional windows were to be added at a later date under permitted development rules I would be very overlooked and lose much privacy. The TPO spruce tree should be preserved and protected . It is a very rare and special tree and can be viewed from many points along the estate.

4. Three storey front and rear extensions and alterations will transform the traditional Arts and Crafts based property into a modern style, so substantially contrary to policy for properties in an ASLC under DMHB 5. Creating a full height third floor is contrary to DMHD 1 The street scene will be adversely affected. In the Design & Access Statement using the poor condition of the property as a reason to grant planning permission is not a valid planning ground. Why isn't the full Arboricultural Report attached? The full report should be requested and put on the portal as that is, in any case, a requirement for validation of an application that requests the removal of a tree protected by a TPO. The loss of the tree involved will adversely affect the street scene. Why is there no report regarding the basement? This should be required now as the basement garage is adjacent to the boundary.

5. The Application states that "all" properties bordering their property have had some

extension work done ... we want to clarify that #7 Shefton Rise - which share a common garden boundary has had no extension work done since it was built in the 1970's. It is not appropriate to provide misleading information in the application, the applicant could have said "most" properties however chose to say "all" which is factually incorrect. 2. The height of the proposed roof will be overpowering and will obstruct my line of sight and take away any view i have from my south facing rooms i.e. bedroom balcony, living room, dining room and even the garden patio. We would strongly object and the height of the proposed property should not be higher than the current building. 3. The two first floor windows on the side elevation facing my property will have a substantial overview of my house and garden and must be made opaque. 4. If additional windows are to be added at a later date under permitted development rules to the side elevation, I would be loose privacy to all the rooms that are south facing i.e. Living Room, Bed Room, Bed Room Balcony, Dining Room.

Officer Comment: All material planning objections are addressed in the planning report.

Internal Consultees

Conservation and Design Officer

Assessment - Background

The existing property is a 1930s detached dwelling set on a spacious plot within the Gatehill Farm Estate, Northwood ASLC. The property is situated on an elevated site within the Gatehill Farm Estate allowing for long views across the estate and area in general. The building is character by its stock brick exterior with red brick dressing. There is a slight projecting element to the front, highlighting the entrance door, which is finished in hung tiles at first floor. The substantial chimney stacks, splayed eaves and original windows with lead detailing, contributes to the character of the property, in keeping with the ASLC. The existing building reflects the local vernacular in terms of built form, design and materiality and has a strong Arts and Crafts aesthetic, positively contributing to the street scene and ASLC.

The Gatehill Farm Estate originates from the early 20th century development by Messrs Harry Neal Ltd on land formerly associated to Gatehill farm. The area continued to be developed after WWII into the 1960s and 1980s however the later properties whilst simpler is character respected the style of earlier dwellings. Properties from the early 20th century are well-defined by their Arts and Crafts influence by the use of traditional external materials and half-timbering detailing, hung tiles, tall chimneys, gable ends, dormers and barge boards.

The 16th century farmhouse was retained as part of the development and is located to the north of the site. The farmhouse is a Grade II listed, originally of timber frame construction however benefitted from re-fronting in the 18th century. It is an attractive property that positively contributes to the history, character and appearance of the area. It is prominent reminder of Northwood's once rural past. Unfortunately the agricultural outbuildings were demolished as part of the development significantly suburbanising its original setting.

Archibald Soutar was the Architect commissioned to plan the estate with influences from his work on the Hampstead Garden Suburb in terms of layout and architectural style. Control over issues such as density, fencing, and maintenance of roads by means of covenants were adopted at that time ensuring the conservation and preservation of the estate and its interesting character. As existing the Gatehill Farm Estate has retained its

original grain of development with detached dwellings set on spacious plots with vegetation such as hedges marking boundaries. The curving layout of the street and undulating topography positively contributes to the townscape.

Permission was granted in 2009 and renewed in 2012 for the demolition of the existing property (refs: 14787/APP/2009/1154 and 14787/APP/2012/1034), establishing the principle of developing the site. The approved development comprised of a very contemporary building. Whilst starkly contrasting to the well-defined character of the Gatehill Farm Estate a compromise had been made taking into account the condition of the existing building and argument made in terms of the structural viability of the existing building. The quality of the approved development's design concept, appearance and well-defined palette of materials was carefully negotiated with the Local Planning Authority's Conservation Officers ensuring the innovative approach was expressed throughout the development adding interest to the area rather than detracting from it.

Development of this site requires careful consideration in accordance to current policies and legislation.

#### Assessment - Impact

The description of works proposes extensions to the existing building. However large parts of the existing building would inevitably be demolished in order to accommodate the proposed extensions and remodelling of the house. It is surprising that the existing building is still extant and able to accommodate the proposed extensions considering previous arguments made 10 years ago in relation to its structural integrity. It would need to be noted that the contemporary design approach was deemed appropriate as the site was previously approved to be redeveloped in full. There are serious concerns that proposed extensions and contemporary remodelling of the dwelling would set an unwelcome and potentially devastating precedent within the area.

The loss of the original character of the existing property would be entirely regrettable from a historic environment perspective. The building relates to the original character and development of the Gatehill Farm Estate, comprising of individual dwelling houses, built on spacious plots in a traditional manner. Whilst properties are individually designed, the area has an overarching garden suburb character with many properties including characteristics with an Arts and Crafts influence.

It is recognised that the footprint of the proposed building would comfortably sit within the site respecting adjacent boundaries with neighbouring properties. However, the 3-storey nature of the building and angular roof form would be starkly contrasting when compared to the well-established traditional roof scape. This would be further emphasised by the long vertical window openings, particularly along the street scene elevation. The central portion of the building embodies modern contemporary design. The previously proposed crown roof has been, commendably, omitted however, the built element to the side would still appear as an add on. Disappointingly, it would not entirely evoke the innovative design aesthetic of the main building.

Whilst there are concerns from a historic environment perspective, the quality of the contemporary design and improving the sustainability and efficiency of the building and site should not be dismissed in this instance. The modern minimalistic appearance to the building would be detailed by clean lines defining the window openings. The external materials and finishes would add elements of texture and interest to the building's exterior

as well as serving an environmental purpose. However, it is important the external materials are of an extremely high quality. The colour palette should be reflective of the tones in existence within the ASLC. Further details and samples of the external materials and finishes proposed would be required. If approved, this would need to be covered by way of a condition.

The loss of the existing character of the site would erode the significance of the ASLC and principles in which the estate had been originally developed. The design and appearance of the development would be blatantly contrasting to the established local distinctiveness of the ASLC, resulting in harm. However, as per previous comments in relation to this site, consideration in this instance would also need to be given towards the potential benefits on the natural environment, and previous planning history. An on-balance decision would need to be considered in line with current policies and paragraph 197 of the National Planning Policy Framework (NPPF, 2019).

#### Trees and Landscape Officer

This site is occupied by a two-storey detached house set within a spacious plot which slopes up from the road to the north and east, towards the rear boundary. The site lies within the area covered by TPO 172 and there are several protected trees within the front and rear garden. T1 is a Blue Spruce in the front garden and T2, T3 and T4 are mature oaks along the rear boundary. The site lies with the Gatehill Farm Estate Area of Special Local Character, a designation which owes much to its topography and tree cover, as well as the built forms.

#### COMMENT

The site was the subject of a previous pre-application ref. PRC/2020/107. There was also tree work application part of which specified the removal of the protected spruce from the front garden, ref. 14787/TRE/2018/2. The tree work application was refused. The Design & Access Statement refers to tree / landscape issues in section 4.5, acknowledging that the protected spruce in the front garden will be removed to facilitate the development. It also refers to a tree report which has not been submitted as part of this application.

The current submission provides a topographical survey, which shows the complicated existing levels and locations of trees. Following a review of the pre-application submission (PRC/2020/107), the Council's trees and woodlands manager (Sophie Coughlan), considered that the loss of the spruce may be acceptable subject to a full tree report and justification / mitigation for any tree loss. In this case the application proposes to replace the spruce with three birch trees.

#### Flood and Water Management

The proposal lies in a critical drainage area and no information on drainage has been provided.

It is noted it is only a part basement on the south side and the Garage is to be set into the current slope down towards no 35. Consideration of this should be incorporated in any drainage design for the site. All hard standing should be made permeable and surface water controlled on site.

## **4. Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

Part 2 Policies:

DMHB 11 Design of New Development

DMHB 12 Streets and Public Realm

DMHB 14 Trees and Landscaping

DMHB 5 Areas of Special Local Character

DMHB 6 Gatehill Farm Estate and Copse Wood Estate Areas of Special Local Character

DMHD 3 Basement Development

DMHD 1 Alterations and Extensions to Residential Dwellings

DMT 6 Vehicle Parking

NPPF- 11 NPPF-11 2018 - Making effective use of land

NPPF- 12 NPPF-12 2018 - Achieving well-designed places

NPPF- 16 NPPF-16 2018 - Conserving & enhancing the historic environment

LPP D10 (2021) Basement development

LPP D4 (2021) Delivering good design

LPP D5 (2021) Inclusive design

LPP D8 (2021) Public realm

## **5. MAIN PLANNING ISSUES**

Development Plan

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)

The Local Plan: Part 2 - Development Management Policies (2020)

The Local Plan: Part 2 - Site Allocations and Designations (2020)

The West London Waste Plan (2015)

The London Plan (2021)

Material Considerations

The National Planning Policy Framework (NPPF) (2019) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

Main Planning Issues

The main considerations for this application are impact on the character and appearance of the area, impact on the residential amenity of neighbouring properties and the proposed basement development.

#### Impact on character and appearance

Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (November 2012) requires all new development to improve and maintain the quality of the built environment in order to create successful and sustainable neighbourhoods.

Policy DMHB 5: Areas of Special Local Character requires that:

A) Within Areas of Special Local Character, new development should reflect the character of the area and its original layout. Alterations should respect the established scale, building lines, height, design and materials of the area.

B) Extensions to dwellings should be subservient to, and respect the architectural style of the original buildings and allow sufficient space for appropriate landscaping, particularly between, and in front of, buildings.

C) The replacement of buildings which positively contribute to the character and local importance of Areas of Special Local Character will normally be resisted.

Policy DMHB 6: Gatehill Farm Estate and Copse Wood Estate Areas of Special Local Character

Within the Gatehill Farm and Copse Wood Estates, states that new houses should:

i) be constructed on building plots of a similar average width as surrounding residential development;

ii) be constructed on a similar building line (formed by the front main walls of existing houses) and be of a similar scale, form and proportion as adjacent houses and reflect the materials, traditional roof design, design features and architectural style predominant in the area;

iii) ensure that boundary treatment is unobtrusive and of the natural materials appropriate to the character and appearance of the estate;

iv) ensure that new dwellings retain an absolute minimum of 1.5m distance to side boundaries;

v) preserve the mature trees including boundary planting to reinforce existing landscaping and Estate settings.

Policy DMHB 11: Design of New Development requires that:

A) All development, including extensions, alterations and new buildings will be required to be designed to the highest standards and, incorporate principles of good design including:

i) harmonising with the local context by taking into account the surrounding:

- scale of development, considering the height, mass and bulk of adjacent structures;

- building plot sizes and widths, plot coverage and established street patterns;

- building lines and setbacks, rooflines, streetscape rhythm, for example, gaps between structures and other streetscape elements, such as degree of enclosure;

- architectural composition and quality of detailing;

- local topography, views both from and to the site; and

- impact on neighbouring open spaces and their environment.

ii) ensuring the use of high quality building materials and finishes;

iii) ensuring that the internal design and layout of development maximises sustainability and is adaptable to different activities;

iv) protecting features of positive value within and adjacent to the site, including the safeguarding of heritage assets, designated.

Policy DMHD 1: Alterations and Extensions to Residential Dwellings states that:

A) Planning applications relating to alterations and extensions of dwellings will be required to ensure that:

- i) there is no adverse cumulative impact of the proposal on the character, appearance or quality of the existing street or wider area;
- ii) a satisfactory relationship with adjacent dwellings is achieved;
- iii) new extensions appear subordinate to the main dwelling in their floor area, width, depth and height;
- iv) new extensions respect the design of the original house and be of matching materials;
- v) there is no unacceptable loss of outlook to neighbouring occupiers;
- vi) adequate garden space is retained;
- vii) adequate off-street parking is retained, as set out in Table 1: Parking Standards in Appendix C;
- viii) trees, hedges and other landscaping features are retained; and
- ix) all extensions in Conservation Areas and Areas of Special Local Character, and to Listed and Locally Listed Buildings, are designed in keeping with the original house, in terms of layout, scale, proportions, roof form, window pattern, detailed design.

The existing property is a 1930s detached dwelling set on a spacious plot within the Gatehill Farm Estate, Northwood ASLC. The property is situated on an elevated site within the Gatehill Farm Estate allowing for long views across the estate and area in general. The building is character by its stock brick exterior with red brick dressing. There is a slight projecting element to the front, highlighting the entrance door, which is finished in hung tiles at first floor. The substantial chimney stacks, splayed eaves and original windows with lead detailing, contributes to the character of the property, in keeping with the ASLC. The existing building reflects the local vernacular in terms of built form, design and materiality and has a strong Arts and Crafts aesthetic, positively contributing to the street scene and ASLC. As existing the Gatehill Farm Estate has retained its original grain of development with detached dwellings set on spacious plots with vegetation such as hedges marking boundaries. The curving layout of the street and undulating topography positively contributes to the townscape.

The local area comprises of collection of houses of mixed styles and design. Many of these properties have benefited from various extensions over the years. Architecturally, these properties range widely in terms of style and form. Certain prevailing character features exist, including the dwellings being of two storeys and having clear gaps between them, which enable views of the many trees across the estate which are also a key character feature.

The application site is a built-up residential area where extensions to properties are considered acceptable in principle. The proposal would involve a part two/part three storey rear extension, subterranean garage/services and storage room, structural underpinning, external remodelling including new roof construction.

Permission was granted in 2009 and renewed in 2012 for the demolition of the existing property (refs: 14787/APP/2009/1154 and 14787/APP/2012/1034), establishing the principle of developing the site. The approved development comprised of a very contemporary building. Whilst starkly contrasting to the well-defined character of the Gatehill Farm Estate a compromise had been made taking into account the condition of the existing building and an argument made in terms of the structural viability of the existing building. The quality of the previously approved development's design concept, appearance and well-defined palette of materials was carefully negotiated with the Local Planning

Authority's Conservation Officers ensuring the innovative approach was expressed throughout the development adding interest to the area rather than detracting from it. Of particular relevance the dwelling had conventional eaves height on the side boundaries and appeared two storey in the streetscene (despite a small central third storey section, almost hidden in the roof form).

The development proposed is tantamount to the construction of a new dwellinghouse due to the extent of the works proposed. The footprint of the building would sit comfortably within the site and there is no objection to the proposed footprint of the extension. The proposed development will result in significant alterations to the property, materially changing its appearance. The resultant property will be a slimmer and elongated building when compared with the host dwelling. The three storey front gable would be the dominating visual feature. This three storey feature would be at odds with surrounding property, which although of varying ages and design is two storey in height, unless there are subservient roof alterations such as dormer windows (two storeys with rooms in the roof in effect).

The loss of the original character of the existing property would be entirely regrettable from a historic environment perspective. The building relates to the original character and development of the Gatehill Farm Estate, comprising of individual dwelling houses, built on spacious plots in a traditional manner. Whilst properties are individually designed, the area has an overarching garden suburb character with many properties including characteristics with an Arts and Crafts influence.

It is recognised that the footprint of the proposed building would comfortably sit within the site respecting adjacent boundaries with neighbouring properties. However, the 3-storey nature of the building and angular roof form would be starkly contrasting when compared to the well-established traditional roofscape. This would be further emphasised by the long vertical window openings, particularly along the street scene elevation. The central portion of the building embodies modern contemporary design. The previously proposed crown roof has been, commendably, omitted however, the built element to the side would still appear as an add on. Disappointingly, it would not entirely evoke the innovative design aesthetic of the main building.

The advice within Paragraph 127 of the NPPF (2019) encourages that Planning policies and decisions should ensure that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks;

Paragraph 197 of the NPPF states that. the effect of an application on the significance of a

non-designated heritage asset should be taken into account in determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset.

Having regard to the scale of impact of the proposed development directly or indirectly on the non-designated heritage asset (ASLC), it is considered on balance that the proposed development will significantly harm the character and appearance of the local area from design perspective. Regardless of the previous planning permissions, the benefits in this instance will not outweigh the harm that would be caused to the ASLC.

Therefore, the proposal by reason of its design, scale, massing and siting is considered to significantly harm the ASLC and would not accord the above policies and advice within the NPPF.

#### Impact on the residential amenity of neighbouring properties

Policy DMHD 1 section B states that all new development must ensure satisfactory relationship with adjacent dwellings is achieved; v) there is no unacceptable loss of outlook to neighbouring occupiers;

The applicant has proposed to erect a part two/part three storey rear extension, subterranean garage/services and storage room, structural underpinning, external remodelling including new roof construction.

The application property is a standalone building, bordered by residential properties on all sides with a degree of separation distance. The proposed building would comfortably sit within the site respecting adjacent boundaries with neighbouring properties.

Taking into account the location, size, scale and relationship of the proposal with neighbouring properties, it is considered that the proposed development would not have a detrimental impact upon neighbouring residential amenity in terms of loss of natural light, overshadowing, overbearing.

Whilst there are proposed windows along the flank elevations of the two/three storey extensions facing adjoining properties, it is not considered to harm the amenity of the neighbouring properties in terms of overlooking or loss of privacy given that these windows are all obscure glaze and non-opening below 1.7m.

Overall, the proposal would have a satisfactory relationship with adjacent properties and accord the above policies.

Furthermore, it is considered that all the proposed habitable rooms, and those altered by the extension, would maintain an adequate outlook and source of natural light, therefore complying with the Mayor of London's Housing Standards Minor Alterations to The London Plan (March 2016).

Policy DMHD 1 section A (vi) states that sufficient garden space to be retained as a consequence of an extension. The property benefits from a good sized rear garden and sufficient private amenity space will be retained in accordance with the above policy.

The proposal will provide two off-street parking spaces within the new subterranean garage

served by existing vehicular access. Therefore, the proposal would not have a negative impact on the highway and pedestrian safety, and comply with Policies DMT 2 and DMT6 of the Local Plan Part 2 - Development Management Policies: January 2020.

#### Policy DMHB 14: Trees and Landscaping

- A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.
- B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.
- C) Where space for ground level planting is limited, such as high rise buildings, the inclusion of living walls and roofs will be expected where feasible.
- D) Planning applications for proposals that would affect existing trees will be required to provide an accurate tree survey showing the location, height, spread and species of trees. Where the tree survey identifies trees of merit, tree root protection areas and an arboricultural method statement will be required to show how the trees will be protected. Where trees are to be removed, proposals for replanting of new trees on-site must be provided or include contributions to offsite provision.

The site lies within the area covered by TPO 172 and there are several protected trees within the front and rear garden. T1 is a Blue Spruce in the front garden and T2, T3 and T4 are mature oaks along the rear boundary. The site lies with the Gatehill Farm Estate Area of Special Local Character, a designation which owes much to its topography and tree cover, as well as the built forms.

The Tree/Landscaping Report acknowledges that the protected Blue spruce tree, T3 on the survey, will be removed to facilitate the development. Table 1 acknowledges that other trees which may be adversely affected by the development due to a number of causes. Among these are three 'B' grade trees; T2 a prominent Lawson cypress in the front garden and two protected oaks T7 and T8 (on the survey). The challenge of safeguarding these trees throughout the development process will be exacerbated by the natural slope of the site and the planned change of levels which will render these trees vulnerable to the development proposal.

The Tree and Landscape Officer was consulted and referred to a tree work application, part of which specified the removal of the protected spruce from the front garden, ref. 14787/TRE/2018/2. The tree work application was refused. Because the proposal necessitates the loss of a 'B' grade tree protected by TPO, the proposal is unacceptable and fails to satisfy policies DMHB 6, DMHB 11 and DMHB 14 of the London Borough of Hillingdon Local Plan Part 2 - Development Management Policies (2020).

#### Proposed basement development

##### Policy DMHD 3: Basement Development

- A) When determining proposals for basement and other underground development, the Council require an assessment of the scheme's impact on drainage, flooding, groundwater conditions and structural stability. The Council will only permit basement and other underground development that does not cause harm to the built and natural environment and local amenity and does not result in flooding or ground instability. Developers will be required to demonstrate by methodologies appropriate to the site that their proposals:
  - i) avoid adversely affecting drainage and run-off or causing other damage to the water environment;

- ii) avoid cumulative impacts upon structural stability or the water environment in the local area;
- B) Schemes should ensure that they:
  - i) do not harm the amenity of neighbours;
  - ii) do not lead to the loss of trees of townscape or amenity value;
  - iii) do provide satisfactory landscaping, including adequate soil depth;
  - iv) do not harm the appearance or setting of the property or the established character of the surrounding area, for example through the introduction of front lightwells; and
  - v) do protect important archaeological remains.
- C) The Council will not permit basement schemes which include habitable rooms and other sensitive uses in areas prone to flooding.
- D) The Council will not permit basement schemes in Listed Buildings and will not permit them in Conservation Area locations where their introduction would harm the special architectural or historic character of the area.

Paragraph A1.37 of the Adopted Local Plan states that the Council will require the submission of a flood risk assessment with applications for subterranean development in areas identified as being at risk of flooding or in an area where watercourses are present, in line with the criteria set out in Technical Guidance to the NPPF. Consideration should be given to the addition of a positive pumped device to ensure basements are protected from sewer flooding.

Paragraph A1.38 of the Adopted Local Plan states that For subterranean developments the Council will also require assessments, including a site investigation, to determine the flood risk and demonstrate the level of groundwater within the site over a period of time. Geo-technical and hydrological investigations and modelling will also be required from applicants to ensure that an understanding of the environmental implications are taken into account during the design phase, which can then demonstrate that basement developments do not harm the built and natural environment or local amenity. As a result there may be a requirement for structural engineering information to demonstrate that the proposal can be designed to provide suitable mitigation for any potential wider impacts of subterranean schemes and the impacts of cumulative subterranean development proposals.

The application will involve a subterranean development in an area identified as being at risk of flooding being within a Critical Drainage Area. In this instance, the applicant has failed to submit a flood risk assessment and other required assessments, including a site investigation, Geo-technical and hydrological to determine the flood risk and demonstrate the level of groundwater within the site over a period of time to ensure that an understanding of the environmental implications are taken into account during the design phase, which can then demonstrate that basement developments do not harm the built and natural environment or local amenity.

For the reasons set out in this report, this application is recommended for refusal.

## 6. RECOMMENDATION

**REFUSAL for the following reasons:**

1            NON2            Non Standard reason for refusal

The development proposed by reason of its design, scale and massing (in particular the three storey front gable would directly harm the significance of the Gatehill Farm Estate Area of Special Local Character. The proposal, therefore, will not accord with Policies DMHD 1, DMHB 5, DMHB 6 and DMHB 11 London Borough of Hillingdon Local Plan Part 2 - Development Management Policies (2020) and the National Planning Policy Framework (2019).

**2 NON2 Non Standard reason for refusal**

The proposed development by reason of its design, scale and siting will result in the loss of a 'B' grade tree protected by TPO and will pose a serious threat of damage to, or loss of, additional protected and 'B' grade trees in spite of the prescribed tree protection measures. The cumulative loss of the trees will be detrimental to the character and appearance of the Gatehill Farm Estate Area of Special Local Character. The proposal, therefore, is unacceptable and fails to satisfy the requirements of Policies DMHB 6, DMHB 11 and DMHB 14 of the London Borough of Hillingdon Local Plan Part 2 - Development Management Policies (2020).

**3 NON2 Non Standard reason for refusal**

The applicant has failed to submit a flood risk assessment and other required assessments, including a site investigation, Geo-technical and hydrological assessments to determine the flood risk and demonstrate the level of groundwater within the site over a period of time to ensure that an understanding of the environmental implications are taken into account during the design phase, which can then demonstrate that basement developments do not harm the built and natural environment or local amenity. The proposal, therefore, fails to satisfy the requirements of Policies DMHB 11 and DMHD 3 of the London Borough of Hillingdon Local Plan Part 2 - Development Management Policies (2020).

## **INFORMATIVES**

### **Standard Informatives**

- 1 The decision to REFUSE planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).
- 2 The decision to REFUSE planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

Part 1 Policies:

PT1.BE1 (2012) Built Environment

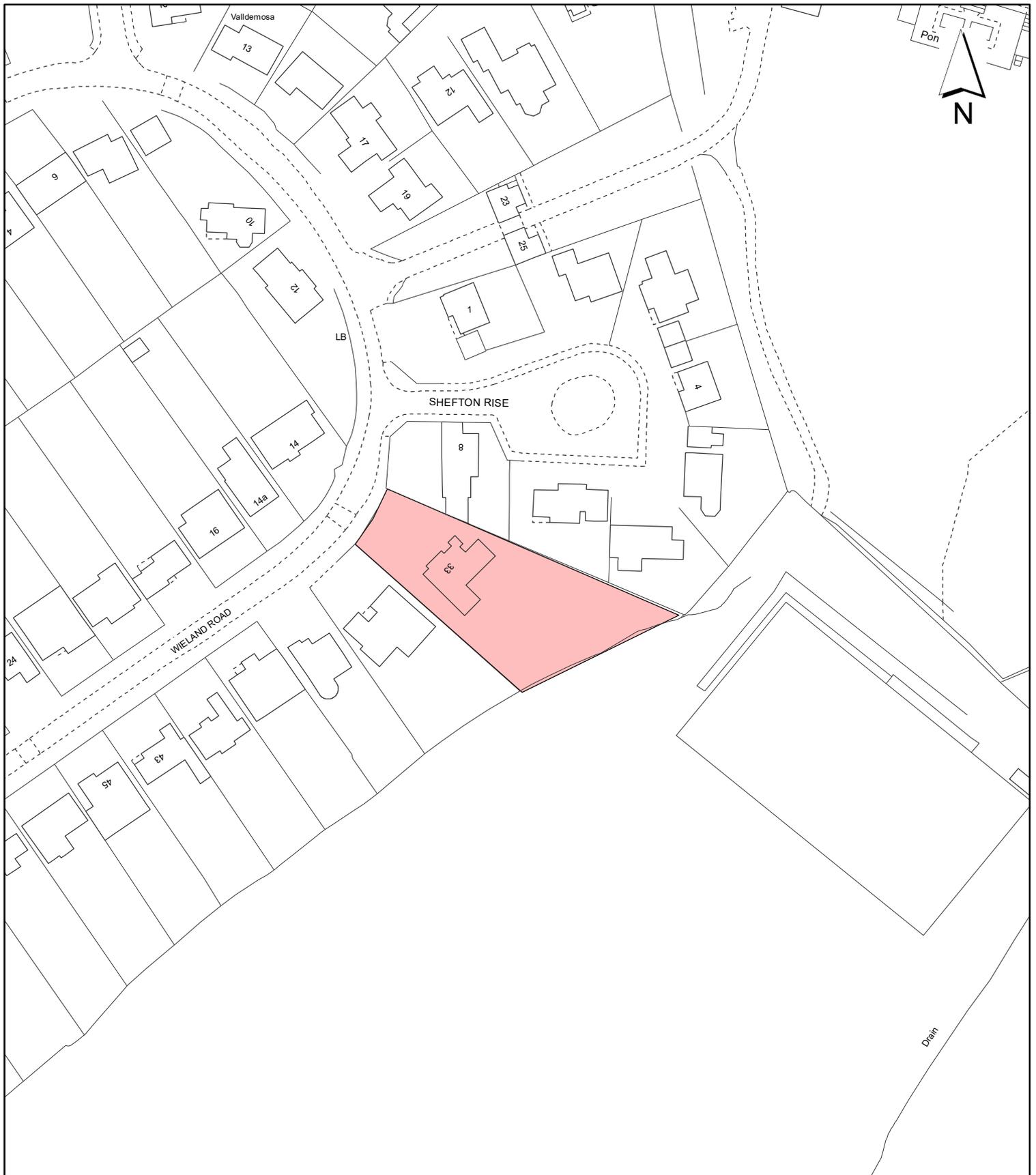
Part 2 Policies:

DMHB 11 Design of New Development

DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 5	Areas of Special Local Character
DMHB 6	Gatehill Farm Estate and Copse Wood Estate Areas of Special Local Character
DMHD 3	Basement Development
DMHD 1	Alterations and Extensions to Residential Dwellings
DMT 6	Vehicle Parking
NPPF- 11	NPPF-11 2018 - Making effective use of land
NPPF- 12	NPPF-12 2018 - Achieving well-designed places
NPPF- 16	NPPF-16 2018 - Conserving & enhancing the historic environment
LPP D10	(2021) Basement development
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D8	(2021) Public realm

**Contact Officer:** Batatunde Aregbesola

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**33 WIELAND ROAD  
 NORTHWOOD**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**14787/APP/2020/3531**

Scale:

**1:1,250**

Planning Committee:

**North Page 58**

Date:

**March 2021**



**HILLINGDON**  
 LONDON

## Report of the Head of Planning, Transportation and Regeneration

<b>Address</b>	40 STATION APPROACH SOUTH RUISLIP
<b>Development:</b>	Erection of a three storey building comprising a dental surgery and 7 residential units (6 no. 1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery
<b>LBH Ref Nos:</b>	27354/APP/2020/4256
<b>Drawing Nos:</b>	001_2000A 001_2001A 001_2002A 001_2003A 001_2005A Design and Access Statement (Part 1 of 2) Design and Access Statement (Part 2 of 2) Sustainable Drainage Strategy and Report Geoenvironmental and Geotechnical Desk Study Schedule of Submission Documents 22466-01 Transport Analysis Letter and Survey Transport Technical Note 001_0006 Rev. 00 Existing Site Plan Temporary Dental Surgery Plan(1 CIL Form Notice under Article 13 001_0001 Rev. 01 001_0020 Rev. 01 001_0021 Rev. 01 001_0022 Rev. 01 001_0023 Rev. 01 001_0030 Rev. 01 Covering Letter (18/12/2020) Drawing Schedule Planning Statement 001_0000 Rev. 01 001_2020 Rev. 01 001_2021 Rev. 01 001_2022 Rev. 01 001_2023 Rev. 01 001_2030 Rev. 01 001_2031 Rev. 01 001_2040 Rev. 00 001_2041 Rev. 00 001_2042 Rev. 00 GE0001 Rev. X 001_0005 Rev. 01 001_2006 Rev. 01

**Date Plans Received:** 21/12/2020  
**Date Application Valid:** 06/01/2021

**Date(s) of Amendment(s):** 21/12/2020

## 1. SUMMARY

The current application seeks planning permission for the erection of a three storey building comprising a dental surgery and 7 residential units (6 no.1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery.

## 2. RECOMMENDATION

**REFUSAL for the following reasons:**

### 1 NON2 Non Standard reason for refusal

The proposed development, by reason of the overall size and scale of the building would result in an incongruous and overly dominant form of development that would be detrimental to the character, appearance and visual amenities of the site, the street scene and the wider area. Furthermore the proposed roof access enclosure and lift over run by reason of its size, scale, bulk and design would result in an intrusive and incongruous roof addition that adds visual clutter and detracts from the appearance of the building and wider streetscape. As such the proposal would be contrary to Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policy D4 of the London Plan (2021) and the NPPF (2019).

### 2 NON2 Non Standard reason for refusal

The proposed development leads to the loss of a family sized residential unit and by reason of the unit mix fails to provide sufficient family sized units, as required by the latest information on housing need. The proposed development fails to contribute towards mixed and balanced communities and the range of housing types required within the Borough contrary to Policy DMH 2 of the Hillingdon Local Plan: Part Two - Development Management Policies (January 2020), Policy H10 of the London Plan (2021) and the NPPF (2019).

## INFORMATIVES

### 1 I59 Councils Local Plan : Part 1 - Strategic Policies

On this decision notice policies from the Councils Local Plan: Part 1 - Strategic Policies appear first, then relevant Local Plan Part 2 (2020), then London Plan Policies (2016). Hillingdon's Full Council adopted the Hillingdon Local Plan: Part 1 - Strategic Policies on 8 November 2012 and the Hillingdon Local Plan Part 2 on 16 January 2020.

### **3. CONSIDERATIONS**

#### **3.1 Site and Locality**

The application site is located on the prominent corner of Station Approach and Northolt Avenue. The site is rectangular in shape totalling 396 metres square. The premises is a 2-storey brick building that is currently occupied by a dental practice on ground floor and used as a residential dwelling on first floor. The front elevation of the site is set back from its adjoining neighbour on the north-east, No. 42B Station Approach. The front hardstanding area faces onto Station Approach and is used as parking for the dental surgery while the rear of the property is also hardstanding with access onto Northolt Avenue.

The application site is located 230 m south-west of South Ruislip station and has a PTAL score of 3. The property is within the South Ruislip Local Centre, Hillingdon Air Quality Management Area and Air Quality Focus Areas: A40/ South Ruislip Focus Area.

The surrounding area comprises mainly of residential dwellings up to 3 storeys high including, residential flats, semi-detached houses, bungalows and mix use buildings (residential above and commercial on ground level).

#### **3.2 Proposed Scheme**

The current application seeks planning permission for the erection of a three storey building comprising a dental surgery and 7 residential units (6 no. 1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery. One car space to the front for the dentist surgery and 4 car spaces, bin store and cycle storage to the rear. Access to the parking spaces is via Northolt Avenue.

The proposal includes:

Ground Floor:

- Dentist Surgery (80.3 sqm)
- 2 bed flat (75.9 sqm) with balcony (6 sqm)

First Floor:

- 1 Bed Flat (51.0 sqm) with balcony (6 sqm)
- 1 Bed Flat (51.1 sqm) with balcony (6 sqm)
- 1 Bed Flat (54.8 sqm) with balcony (7 sqm)

Second Floor:

- 1 Bed Flat (51.0 sqm) with balcony (6 sqm)
- 1 Bed Flat (51.1 sqm) with balcony (6 sqm)
- 1 Bed Flat (54.8 sqm) with balcony (7 sqm)

During the construction of the development, a temporary, single storey building to provide a dental surgery, totalling 35.8 sqm GIA, is to be located to the rear of the site.

#### **3.3 Relevant Planning History**

##### **Comment on Relevant Planning History**

SITE HISTORY:

An advertisement consent under planning reference 27354/ADV/2000/59 was granted on 12-10-00 for the installation of 2 wall mounted illuminated box signs and one non-illuminated free standing sign.

A planning application under planning reference 27354/D/90/1464 was granted on 11-12-90 for additional use of part of residential property as dental surgery.

A planning application under planning reference 27354/A/86/1182 was granted on 27-08-86 for a Householder development - residential extension(P).

A planning application under planning reference 27354/79/1638 was granted on 30-10-79 for a Householder development - residential extension(P).

#### **4. Planning Policies and Standards**

##### Development Plan

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)

The Local Plan: Part 2 - Development Management Policies (2020)

The Local Plan: Part 2 - Site Allocations and Designations (2020)

The West London Waste Plan (2015)

The London Plan (2021)

##### Material Considerations

The National Planning Policy Framework (NPPF) (2019) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

#### **UDP / LDF Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

##### Part 1 Policies:

PT1.BE1 (2012) Built Environment

##### Part 2 Policies:

DMCI 2 New Community Infrastructure

DMH 2 Housing Mix

DME 1 Employment Uses in Designated Sites

DMEI 14 Air Quality

DMHB 11 Design of New Development

DMHB 12 Streets and Public Realm

DMHB 14 Trees and Landscaping

DMHB 16 Housing Standards

DMHB 18	Private Outdoor Amenity Space
DMT 1	Managing Transport Impacts
DMT 2	Highways Impacts
DMT 6	Vehicle Parking
EM6	(2012) Flood Risk Management
LPP 3.5	(2016) Quality and design of housing developments
LPP 4.3	(2016) Mixed use development and offices
LPP 5.13	(2016) Sustainable drainage
LPP 5.21	(2016) Contaminated land
LPP 5.3	(2016) Sustainable design and construction
LPP 6.3	(2016) Assessing effects of development on transport capacity
LPP 6.9	(2016) Cycling
LPP 6.13	(2016) Parking
LPP 7.4	(2016) Local character
LPP 7.5	(2016) Public realm
LPP 7.6	(2016) Architecture

## **5. Advertisement and Site Notice**

**5.1** Advertisement Expiry Date:- Not applicable

**5.2** Site Notice Expiry Date:- Not applicable

## **6. Consultations**

### **External Consultees**

57 neighbouring properties and the local residents association were consulted by letter dated 12/1/2021. The consultation period expired on 2/2/2021.

One written representation has been received together with a signed petition with 31 signatures objecting to the proposal summarised as follows:-

Written representation comments

Noise and disturbance from construction work  
 No need for more flats in the area  
 House prices devalued

Officer response - Noise due to construction works is of a transient nature and the latter two points are not material planning considerations.

Petition comments

1. Too high and would impact on the character of the surrounding area - especially being located on a corner plot
2. Would impact on the privacy on neighbouring properties in Northolt Avenue
3. Would add parking stress to an already crowded road network with all adjacent streets already part of the SR parking management scheme

#### 4. Failure to provide much needed 3 bedroom family homes to South Ruislip

Officer response - The concerns raised above are material planning considerations and are addressed within the body of this report.

NHS Clinical Commissioning Group - Were consulted but have not commented on the application.

#### **Internal Consultees**

##### PLANNING POLICY

Planning Policy commented as follows:-

Designations · Local Centre - South Ruislip

The applicant is proposing 6 x 1 bedroom flats and 1 x 2 bedroom flat. The application site is within South Ruislip Local Centre and it has a PTAL of 3.

##### Dental Surgery

The applicant is proposing the demolition of the existing dental surgery and a replacement dental surgery on the ground floor of the new four storey building. A temporary dental facility is proposed on site for use during the construction of the new building.

Policy DMCI 2 of the Hillingdon Local Plan Part Two (LPP2) states that proposals for the refurbishment and re-use of existing premises for community facilities will be supported. Policy S1 of the Publication London Plan states that "Development proposals that provide high quality, inclusive social infrastructure that addresses a local or strategic need and supports service delivery strategies should be supported."

Policy S2 of the London Plan (2021) states that "Development proposals that support the provision of high-quality new and enhanced health and social care facilities to meet identified need and new models of care should be supported." Paragraph 5.1.5 of the London Plan states that "The loss of social infrastructure can have a detrimental effect on a community. Where possible, boroughs should protect such facilities and uses, and where a development proposal leads to the loss of a facility, require a replacement that continues to meet the needs of the neighbourhood it serves."

In principle, the re-provision of a dental surgery of higher quality and increased floor space is supported. It is advised that the CCG are consulted to provide a view on the quality and suitability of the proposed dental surgery to be able to fully assess the proposal. The development proposal does not lead to a loss of social infrastructure however there is concern that the temporary facility, in terms of accessibility and size, at 35.8 sq. m, may not be adequate to function effectively as a dental surgery. It is advised that the CCG is consulted so that they can provide a more informed view on the suitability of the temporary facility as well as the new permanent facility measuring 80 sq. m.

Officer Comment: The proposed surgery is larger (80sq.m) in floor area than the existing surgery (42sq.m), as such it cannot be argued there is a loss of or reduction in scale or form of the community facility. Whereas the temporary facility is smaller than the existing facility, had the development been acceptable in other respects a condition could have been imposed seeking to control (to as short as period as possible) the use of the temporary building. As such it is not considered that an objection can be raised to the proposals in principle. The CCG were consulted, but no comments have been received.

Residential Use

The principle of residential uses on the site has been established through the existing residential unit and it is not an inappropriate place for residential development. There is an existing 3-bedroom flat on the site, and the applicant is proposing the demolition of the existing building and replacement with 7 flats (6 x 1-bed and 1 x 2-bed), a net gain of 6 units.

## Housing Mix

Policy H10 of the London Plan (2021) states that schemes should generally consist of a range of unit sizes and sets out a number of factors which should be considered when determining the appropriate housing mix on a particular scheme. This includes local evidence of need.

Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires the provision of a mix of housing units of different sizes in schemes of residential development to reflect the Council's latest information on housing need. Paragraph 4.6 outlines that there is a substantial borough-wide requirement for larger affordable and private market units, particularly three-bedroom properties

In terms of factors specific to a site, Policy H10 also includes a need to consider, the mix of uses in the scheme, the range of tenures in the scheme and the nature and location of the site, with a higher proportion of one and two bed units generally more appropriate in locations which are closer to a town centre or station or with higher public transport access and connectivity. Family housing is defined within the glossary of the Publication London Plan (2020) and outlines it must generally be of a size that has three or more bedrooms. It is worth noting that the Secretary of State directed changes to Policy H10, in order address the need for new family housing, to prevent families from being forced to move outside of London. These changes are incorporated into the Publication London Plan (2020).

The proposal consists of the following housing mix:

- 6 No. of One Bedroom Units (86%)
- 1 No. of Two Bedroom Units (14%)

The proposal involves the demolition of an existing 3 bed dwelling and its replacement with 7 residential units comprising 6 x 1 bed and 1 x 2 bed dwellings. As stated in Policy H10, as well as consideration of local housing need, development proposals should also have regard to the mix of uses in the scheme, the range of tenures in the scheme and the nature and location of the site.

There is an existing family sized unit in the current building and as a minimum the Council would expect the re-provision of at least one 3/3+ bed unit in the proposed development. Although the site is in a designated local centre, it also has a PTAL of 3 and generally speaking it would be suited to family housing as schemes in these locations generally tend to be less constrained and there is more capacity to provide car parking and adequate amenity space.

The most recent assessment of local housing need, the 2018 Strategic Housing Market Assessment also indicates a significant borough-wide need for family units (3 bed or larger) and it is considered that this site is capable of accommodating at least a replacement family dwelling as part of the proposed development. Given this need a net loss of a family unit without suitable justification would be considered contrary to policy.

This particular site is constrained in some ways given the need for the re-provision of a larger dental surgery on the ground floor and a limited site capacity to the rear of the site to provide both adequate private amenity space for residents and adequate levels of car parking. As such, some flexibility in the housing mix required on site would be applied. Currently, however, the proposed housing mix is not considered policy compliant even with some flexibility applied.

Policy H10 also states that schemes should have regard to "the need to deliver a range of unit types at different price points across London" and "the requirement to deliver mixed and inclusive neighbourhoods". The currently proposed housing mix is considered to be contrary to the requirements of Policy H10 and a higher proportion of 2-bed dwellings, as well as re provision of the existing family unit is expected. The 1-bed units equate to 86% of the total units provided in the scheme, which is considered to be contrary to the need to deliver a range of unit types at different price points across London and the requirement to deliver mixed and inclusive neighbourhoods. The scheme as is results in an over-dominance of 1-bed units and the loss of an existing family dwelling which is contrary to policy.

The applicant has not provided any justification or evidence to demonstrate why a housing mix more in line with policy cannot be provided as part of the proposed development. As a suggestion to the applicant, amalgamation of some of the units on the first and second floor to provide larger units seems very possible and it would allow for some of the design issues of the scheme, namely in relation to the single aspect dwellings on the first and second floor, to potentially be resolved and also provide a more policy compliant housing mix. As stated, the principle of residential development on this site is established. It is advised that the applicant revise the proposed housing mix to take account of the above comments.

#### Private amenity space

As stated in policy DMHB 18, all new residential development should provide good quality and usable private outdoor amenity space. Amenity standards should be provided in accordance with the minimum standards set out in table 5.3 of the Local Plan: Part Two (2020). A minimum of 20 sq. m of private outdoor amenity space for each of the 1-bed flats and a minimum of 25 sq. m for the 2-bed flat should be provided. Communal provision of outdoor space instead of private amenity space for each flat is generally not supported unless there are strong planning reasons and the proposed scheme is of a high quality with clear planning merits, as stated in paragraph 5.70 of the LPP2. A combination of a communal roof terrace of 145 sq. m as well as private balconies for each of the flats have been proposed as part of the scheme. However, floor space figures for each of the private balconies have not been annotated in the drawings provided. These are needed to make a full assessment.

#### Housing standards

All dwellings should meet or exceed the internal space standards set out in Table 5.1 and provide at least 10% of new housing to be accessible or easily adaptable for wheelchair users.

#### Living Walls/Roofs

Policy DME1 of the HLLP2 states that all major development (i.e. residential development of 10 dwellings or more) should incorporate living roofs and/or walls into the development. Suitable justification should be provided where living walls/roofs cannot be provided.

#### TREES AND LANDSCAPING:

This site is occupied by a two-storey brick building located on a corner plot at the junction with Northolt Avenue. The ground-floor houses a dental surgery, with a residential unit on the first floor. An area of hard-standing to the front is currently used for parking and there is a rear service road which provides access to additional parking to the rear of the building. According to Google Streetview there are a two small trees to the rear of the building, however, there are no TPO's or Conservation Area designations affecting the site.

COMMENT This submission follows pre-application ref. 27354/PRC/2020/181. The two trees will be

removed to facilitate car parking and bin / bike stores to the rear. The D&AS refers to secure and enclosed cycle storage and bin storage. It also refer to landscaping to the front of the building to enhance the public realm. Modest areas of soft landscape and green roofs are indicated on the landscape plan. There is an annotation referring to the use of native species. Where the soft landscape is part of the visual amenity for residents, it is unlikely that the use of native species will be suitable, however, there are ornamental plants which are also nectar-bearing species which will encourage biodiversity. Private outdoor amenity space is to be provided in the from of private balconies. These should be of sufficient size to be accessible and functional. The provision of soft landscape enhancement in front of the building for the benefit of the public realm is welcome, subject to detail. Its success will depend on the quality of design, implementation and future maintenance.

## HIGHWAYS AND TRANSPORTATION: -

### Site Characteristics & Background

The address is situated in South Ruislip on the corner of Station Approach and Northolt Avenue. The existing building comprises of a dental surgery at the ground floor with a residential unit on the 1st floor. There are areas of hardstanding to the front (a private forecourt) and rear which are currently used for parking purposes and accessed via established vehicular crossovers from both Station Approach and Northolt Avenue respectively.

It is proposed to demolish the existing building with a 4-storey replacement build incorporating a new Dental surgery (GIFA -78m<sup>2</sup>) on the ground floor with 7 flats above (6 x 1 & 1 x 2 bedroom units). The surgery would continue operation throughout the build programme with a temporary modular facility provided to the rear of the site. 5 parking spaces are proposed with one allocated to the surgery located on the frontage and 4 spaces (including 1 disabled compliant bay) positioned to the rear. Existing carriageway crossings would remain to serve the spaces.

During construction, a temporary Dental surgery (two surgery rooms - GIFA 35.8m<sup>2</sup>) acting as a decant facility would be positioned to the rear of the site where the final on-plot residential parking provisions are proposed.

The Public Transport Accessibility Level (PTAL) is rated at a level of 3 and the immediate highway is generally covered by extensive parking controls which include double yellow lines and a CPZ operating from 9am to 5pm - Monday to Friday.

### Parking Provision

Local Plan: Part 2 Policy DMT 6 requires that new development will only be permitted where it accords with the Council's adopted parking standards unless it can be demonstrated that a deviation from the standard would not result in a deleterious impact on the surrounding road network.

### Residential

The maximum parking provision for each of the 7 flatted units requires 1-1.5 spaces to be provided on-site in order to comply with the adopted parking standard. This would equate to 7-11 on-plot spaces. 4 spaces are proposed which falls significantly below the standard.

It is noted that the surrounding residential catchment and road network exhibits certain characteristics which arguably support a lower quantum of on-plot parking provision. To expand - the local area is covered by extensive daytime parking controls in the vicinity of the address which consist of a controlled parking zone (CPZ) which operates between 9am to 5pm - Monday to Friday. Double yellow lines are also present at key junction locations. Also, there are relatively generous off-

street parking facilities for most of the surrounding residential properties in the area such as Northolt Avenue which assists in reducing general on-street parking demand.

The applicant has also undertaken parking stress surveys within the locality in order to catalogue the levels of parking demand in all roads within a walking distance of 200m of the site. Industry recognised methodology has been applied and the findings indicate that during the recommended surveyed periods there appears to be in the region of 35% spare parking capacity which can be legitimately used on the surrounding roadways. As there is no evidence to the contrary as confirmed by random checks by Council officers, the findings are considered robust and relevant and therefore supportive of a lower on-plot parking quantum.

#### Disabled Compliant Parking

In accord with Hillingdon's Local Plan: Part 2 standard - 10% of parking spaces should be disabled compliant. 1 space is proposed which conforms to the standard in the context of the quantum of spaces proposed.

#### Electric Vehicle Charging Points (EVCP's)

In line with the emerging London Plan, within any final parking quantum there is a requirement for a minimum 20% 'active' EVCP provision with all remaining spaces being designated as 'passive' provisions. This equates to 1 'active' and 3 'passive' spaces which is proposed hence there are no further observations made.

#### Cycling Provision

In terms of cycle parking there should be a minimum provision of 1 secure and accessible space for each residential unit in order to conform to the adopted minimum borough cycle parking standard. 8 spaces are proposed within 'two tier' racks located to the rear of the site envelope which conforms to the standard.

#### Dental Surgery

The proposal indicates a single on-plot parking provision on the private forecourt on the frontage. The parking standard requires two spaces per consulting room. Hence given the proposed GIFA, there is a deficit in provision.

It is however acknowledged that there is little convenient 'free of charge' parking availability in the local area as it is extensively covered by parking restrictions throughout the working week which inherently assists in deterring any potential extraneous parking and traffic generated by the proposal. It is also possible that a proportion of patients would attend the surgery on a 'linked-trip' principle on-route to other destinations by, for example, being 'dropped off & picked up' which would further reduce potential new 'single destination' vehicular trips and resultant impacts generated by the proposal. This has, in fact, been demonstrated by the applicant who undertook a travel survey which indicated a high percentage attendance by way of sustainable travel modes. The findings are considered robust and relevant and therefore supportive of a lower on-plot parking quantum.

#### Cycling Provision

In terms of cycle parking there should be a minimum provision of 1 secure and accessible space for staff with 1 space per 10 patients. 3 'Sheffield' stands are proposed on the frontage which satisfies the requirement.

#### Temporary Dental Surgery

During construction, a temporary Dental surgery (two surgery rooms - GIFA 35.8m<sup>2</sup>) acting as a decant facility would be positioned to the rear of the site where the final on-plot parking provisions are proposed. This aspect of the proposal does not give rise for concern given current activities linked to the established Dental surgery use and its physical separation from the main construction site.

#### Vehicular Access Arrangements (Residential & Dental Practice)

In highway impact terms, there is no objection in principle to the parking layouts depicted on the frontage and to the rear of the site envelope and the access points are considered fit for purpose as demonstrated by the submitted vehicle swept path analysis.

#### Operational Refuse Requirements (Residential)

Refuse collection would occur from Northolt Avenue and a bin storage area is shown to the rear of the site. In order to conform to the Council's 'waste collection' maximum distance collection parameter of 10m i.e. distance from a refuse vehicle to the point of collection or bin store, arrangements should ensure that waste is positioned at a collection point within this set distance. With the arrangement as presented, the above parameter is met. There are no further observations.

#### Construction Management Plan (CMP)

The above will be a requirement given the constraints and sensitivities of the local road network in order to avoid/minimise potential detriment to the public realm. It will need to be secured under planning condition.

#### Conclusion

The application has been reviewed by the Highway Authority who are satisfied that the proposal would not discernibly exacerbate congestion or parking stress, and would not raise any measurable highway safety concerns, in accordance with Local Plan: Part 2 Development Plan Policies DMT 1, DMT 2 & DMT 6 and Policies 6.3, 6.9, and 6.13 of the London Plan (2016).

**RECOMMENDATIONS** No objection subject to conditions COM 9 (parts 1,2,3,4,5 and 6).

The Access officer commented on 12/01/2021 as follows: -

This proposal for a replacement dental surgery building, with 6 x 1 bed and 1 x 2 bed flats has been reviewed with reference to the 2016 London Plan and its contained policies 3.1, 3.8 and 7.2. Policies D5, D7, D12, T6.1 and T6.5 set out in the 2019 (intend to publish) London Plan have also been referenced, as well as the Council's Supplementary Planning Document 'Accessible Hillingdon' (adopted September 2017). No details have been submitted in respect of the proposed dental surgery internal layout, and an indicative layout should be submitted to ensure the principals of Inclusive Design can be achieved as per policy 7.2 referenced above.

The residential element plans are satisfactory for the purposes of development control, however, the following conditions should be attached to any approval:

Prior to works commencing, details of step free access via the principal private entrances, and all other points of entry and exit, including step free access to the roof garden, shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

REASON: To ensure that an appropriate standard of housing stock, in accordance with the 2016 London Plan policy 3.8(c) and policy D7 of the 2019 (Intend to Publish) London Plan, is achieved and maintained.

The development hereby approved shall provide access to the roof garden, via a lift compatible with the technical requirements set out in Approved Document M to the Building Regulations, Volume 1, with the lift remaining in place and in working order for the life of the building.

REASON: To ensure that an appropriate standard of housing stock, in accordance with the 2016 London Plan policy 3.8(c) and policy D7 of the 2019 (Intend to Publish) London Plan, is achieved and maintained.

The dwellings hereby approved shall be constructed to meet the standards for a Category 2 M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, and all such provisions shall remain in place for the life of the building.

REASON: To ensure that an appropriate standard of housing stock, in accordance with the 2016 London Plan policy 3.8(c) and policy D7 of the 2019 (Intend to Publish) London Plan, is achieved and maintained. Conclusion: further details in respect of the dental surgery layout are requested.

Following additional information submitted by the agent on 12/2/2021, the Access officer commented further on 14/02/2021 as follows:-

The additional details shown on revised plan 00\_2000A\_2005A have been reviewed, and the details shown are now satisfactory for the purposes of development control. However, given that an indicative layout for the proposed dental surgery has not been submitted, the following informative should be attached to any planning permission:

The Equality Act 2010 seeks to protect people accessing goods, facilities and services from discrimination on the basis of a 'protected characteristic', which includes those with a disability. As part of the Act, service providers are obliged to improve access to and within the structure of their building, particularly in situations where reasonable adjustment can be incorporated with relative ease. The Act states that service providers should think ahead to take steps to address barriers that impede disabled people.

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

The proposal seeks the demolition of the existing two storey building comprising a dentist's surgery on the ground floor and a single residential unit on the first floor, and the erection of a replacement three storey building comprising a dental surgery and 7 residential units (6 no. 1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery.

Policy S1 of the London Plan (2021) states that development proposals that provide high quality, inclusive social infrastructure that addresses a local or strategic need and supports service delivery strategies should be supported. It also states that new facilities should be easily accessible by public transport, cycling and walking and should be encouraged in high streets and town centres. This second policy requirement is mentioned again in reference to health and social care facilities in Policy S2. The site is located within a local centre and close to South Ruislip tube station and it is relatively well connected. The proposal includes the demolition but also re-provision of a dentist's surgery. This aspect of

the proposal is overall considered to be policy compliant and supported.

The proposed development also includes 7 residential units on the ground and upper 2 floors, a net gain of 6 units. The applicant is proposing 6 x 1 bed units and 1 x 2 bed units. The principle of residential development on site is established through the existing residential unit and a residential use on site in general would be considered to be policy compliant. Policy DMH 2 of the LPP2 states that the Council will seek a housing mix which reflects the needs identified in the most up to date housing needs assessment.

The Council's current information on housing need indicates a substantial borough-wide requirement for larger affordable and private market units, particularly three bedroom properties. The applicant does not need to provide affordable housing as the proposed development falls under the threshold for provision of affordable housing. That said, given the site's location and particular constraints due to site size and the fact that sufficient parking and private amenity space can be provided, it is considered that the principle of the current scheme is acceptable.

#### **7.02 Density of the proposed development**

Not relevant to this application

#### **7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

Not relevant to this application

#### **7.04 Airport safeguarding**

Not relevant to this application

#### **7.05 Impact on the green belt**

Not relevant to this application:

#### **7.07 Impact on the character & appearance of the area**

Policy DMHB 11 of The Local Plan: Part 2 - Development Management Policies (2020) states that:

A) All development, including extensions, alterations and new buildings will be required to be designed to the highest standards and, incorporate principles of good design including:

- i) harmonising with the local context by taking into account the surrounding:
  - scale of development, considering the height, mass and bulk of adjacent structures;
  - building plot sizes and widths, plot coverage and established street patterns;
  - building lines and setbacks, rooflines, streetscape rhythm, for example, gaps between structures and other streetscape elements, such as degree of enclosure;
  - architectural composition and quality of detailing;
  - local topography, views both from and to the site; and
  - impact on neighbouring open spaces and their environment.
- ii) ensuring the use of high quality building materials and finishes;
- iii) ensuring that the internal design and layout of development maximises sustainability and is adaptable to different activities;
- iv) protecting features of positive value within and adjacent to the site, including the safeguarding of heritage assets, designated and un-designated, and their settings; and
- v) landscaping and tree planting to protect and enhance amenity, biodiversity and green infrastructure.

B) Development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

C) Development will be required to ensure that the design safeguards the satisfactory re-development of any adjoining sites which have development potential. In the case of proposals for major development sites, the Council will expect developers to prepare master plans and design codes and to agree these with the Council before developing

detailed designs.

D) Development proposals should make sufficient provision for well designed internal and external storage space for general, recycling and organic waste, with suitable access for collection. External bins should be located and screened to avoid nuisance and adverse visual impacts to occupiers and neighbours.

The proposals is considered to be satisfactory from an accessibility perspective. The Access officer has recommended conditions in the event of an approval.

The application seeks planning permission to erect a three-storey building comprising a dental surgery and 7 residential units (6 no. 1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery.

There have been previous pre-application discussions for this site under app: 27354/PRC/2020/181 where number of design concerns were raised. The submitted scheme has taken into consideration some of the concerns but others remain and are discussed below.

40 Station Approach is located on a prominent corner site where Northolt Avenue meets Station Approach. It is situated within a primarily residential area which is interspersed with some shops/ commercial buildings along Station Approach eastwards towards South Ruislip Station.

The buildings are primarily two to three-storeys in height on the south side of the street. Third storeys, where they exist, are contained within the roof respecting the domestic scale of the surrounding streets. The buildings on the north side of the street have a different character with larger blocks of three full storeys.

The existing building frontage is set back from the street on a similar alignment to the residential properties to the west of Northolt Avenue. There would be no objection to the proposed new front building line that corresponds to the buildings directly to the east in which the development would have a more direct visual relationship.

The retention and slight increase of the rear building line taken together with the frontage increase, however, will result in a much larger block than those within the group and the relatively unbroken mass would make the building dominate the southern side of the street in this location.

It is considered that the rear portion of the block would need to be reduced in height and depth so that there is a more comfortable transition between the new development and the bungalows / two-storey houses to the south along Northolt Avenue. It would also help the development relate better to the rear building lines of the group of buildings in which it is most closely related (nos. 40-50).

There are concerns with the roof access enclosure and lift over run which result in an intrusive and incongruous roof addition that adds visual clutter and detract from the appearance of the new building and wider streetscape. This element would seek undue attention particularly given the building's prominent corner location where it would be seen in prominent views along Station Road as well as those looking north along Northolt Avenue and the green open space of Acol Crescent

The use of brick as the primary construction material would be supported in principle and would respect the palette of materials in the area subject to an appropriate colour, tone, texture and mortar being chosen.

Overall the scale and mass of the proposed building is considered to have an unacceptable impact on the character and appearance of the streetscene. The scheme in its current form is not considered appropriate for the local context.

The proposed development, by reason of the overall size and scale of the building would result in an incongruous and overly dominant form of development that would be detrimental to the character, appearance and visual amenities of the site, the street scene and the wider area. Furthermore the proposed roof access enclosure and lift over run by reason of its size, scale, bulk and design would result in an intrusive and incongruous roof addition that adds visual clutter and detracts from the appearance of the building and wider streetscape. As such the proposal would be contrary to Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policy D4 of the London Plan (2021) and the NPPF (2019).

#### **7.08 Impact on neighbours**

Policy DMHB 11 of the Local Plan: Part Two (2020) notes development should not have an adversely impact the amenity, daylight and sunlight of adjacent properties and open space

The proposed building would be set further forward on the site than the existing building and would be in line with the front building line of the neighbouring property to the north east at 42B Station Approach. In addition, it would not extend beyond this property's rear wall. Consequently it is considered that the proposal would not significantly impact on the amenity of the neighbouring occupiers by virtue of visual amenity, overbearing or loss of light. The principle windows serving the proposed units would face front and rear.

The side elevation windows would serve both habitable and non habitable rooms however, these together with the balconies on this south west elevation would face onto the public highway known as Northolt Avenue. The separation distance to the property on the opposite corner would be approximately 18 metres however, as this neighbouring property at 38 Station Approach has one first floor flank elevation window which serves a non habitable room there would be no adverse affect on this property's residential amenity.

The proposal also includes the siting of a temporary outbuilding which would be located to the rear of the site. The outbuilding would have a maximum height of 2.5 m and it is not considered that this would have any detrimental affect on the residential amenities of the 42B Station Approach. In addition, the outbuilding or modular 'Portakabin' would be a temporary structure that would be removed after the development is completed should planning permission be forthcoming.

As such the proposal would comply with Policy DMHB 11 of the Local Plan: Part Two (2020).

#### **7.09 Living conditions for future occupiers**

##### **LIVING CONDITIONS**

The DCLG's Technical Housing Standard (2015) guidance also requires that double bedrooms measure at least 11.5 square metres in floor space and single bedrooms measure at least 7.5 square metres in floor space.

Policy DMHB 16 of The Local Plan: Part 2 - Development Management Policies (2020)

states that all housing development should have an adequate provision of internal space in order to provide an appropriate living environment. To achieve this all residential development or conversions should: i) meet or exceed the most up to date internal space standards, as set out in Table 5.1; and ii) in the case of major developments, provide at least 10% of new housing to be accessible or easily adaptable for wheelchair users.

In accordance to the plans provided, the 1-bedroom units provide between 51.3 - 54.8 square metres and the 2-bedroom 4 person unit provides 75.9 square metres. Consequently, all the proposed units provide sufficient internal floor space in accordance with Policy 3.5 of The London Plan - Consolidated With Alterations (2016) and DCLG's Technical Housing Standard (2015) guidance.

#### EXTERNAL AMENITY SPACE

Policy DMHB 18 of The Local Plan: Part 2 - Development Management Policies (2020) requires all new residential development to provide good quality and usable private outdoor amenity space. Balconies should have a depth of not less than 1.5 metres and a width of not less than 2 metres. Any ground floor and/or basement floor unit that is non-street facing should have a defensible space of not less than 3 metres in depth in front of any window to a bedroom or habitable room. All new residential development should provide good quality and usable private outdoor amenity space. Amenity standards should be provided in accordance with the standards set out in table 5.3 of the Local Plan: Part Two (2020).

The minimum amenity space provision required:

- Flats - 1 bedroom, requires 20 square metres
- Flats - 2 bedroom, requires 25 square metres

Each residential unit is provided with its own private balcony ranging from 6 - 9.9 sqm, which is below the minimum space provision required. That said, the proposal includes a communal roof terrace which would provide a further 145 sqm of usable external amenity space.

It is considered that an argument that the amenity space is substandard may be hard to sustain on appeal given the relatively small scale of the development. Albeit there are concerns as regards how practicable the roof-space amenity area will be.

#### **7.10 Traffic impact, car/cycle parking, pedestrian safety**

The Public Transport Accessibility Level (PTAL) is rated at a level of 3 and the immediate highway is generally covered by extensive parking controls which include double yellow lines and a CPZ operating from 9am to 5pm - Monday to Friday.

#### Parking Provision

Local Plan: Part 2 Policy DMT 6 requires that new development will only be permitted where it accords with the Council's adopted parking standards unless it can be demonstrated that a deviation from the standard would not result in a deleterious impact on the surrounding road network.

#### Residential

The maximum parking provision for each of the 7 flatted units requires 1-1.5 spaces to be provided on-site in order to comply with the adopted parking standard. This would equate to 7-11 on-plot spaces. 4 spaces are proposed which falls significantly below the standard.

It is noted that the surrounding residential catchment and road network exhibits certain characteristics which arguably support a lower quantum of on-plot parking provision. To expand - the local area is covered by extensive daytime parking controls in the vicinity of the address which consist of a controlled parking zone (CPZ) which operates between 9am to 5pm - Monday to Friday. Double yellow lines are also present at key junction locations. Also, there are relatively generous off-street parking facilities for most of the surrounding residential properties in the area such as Northolt Avenue which assists in reducing general on-street parking demand.

The applicant has also undertaken parking stress surveys within the locality in order to catalogue the levels of parking demand in all roads within a walking distance of 200m of the site. Industry recognised methodology has been applied and the findings indicate that during the recommended surveyed periods there appears to be in the region of 35% spare parking capacity which can be legitimately used on the surrounding roadways. As there is no evidence to the contrary as confirmed by random checks by Council officers, the findings are considered robust and relevant and therefore supportive of a lower on-plot parking quantum.

#### Disabled Compliant Parking

In accord with Hillingdon's Local Plan: Part 2 standard - 10% of parking spaces should be disabled compliant. 1 space is proposed which conforms to the standard in the context of the quantum of spaces proposed.

#### Electric Vehicle Charging Points (EVCP's)

In line with the emerging London Plan, within any final parking quantum there is a requirement for a minimum 20% 'active' EVCP provision with all remaining spaces being designated as 'passive' provisions. This equates to 1 'active' and 3 'passive' spaces which is proposed hence there are no further observations made.

#### Cycling Provision

In terms of cycle parking there should be a minimum provision of 1 secure and accessible space for each residential unit in order to conform to the adopted minimum borough cycle parking standard. 8 spaces are proposed within 'two tier' racks located to the rear of the site envelope which conforms to the standard.

#### Dental Surgery

The proposal indicates a single on-plot parking provision on the private forecourt on the frontage. The parking standard requires two spaces per consulting room. Hence given the proposed GIFA, there is a deficit in provision.

It is however acknowledged that there is little convenient 'free of charge' parking availability in the local area as it is extensively covered by parking restrictions throughout the working week which inherently assists in deterring any potential extraneous parking and traffic generated by the proposal. It is also possible that a proportion of patients would attend the surgery on a 'linked-trip' principle on-route to other destinations by, for example, being 'dropped off & picked up' which would further reduce potential new 'single destination' vehicular trips and resultant impacts generated by the proposal. This has, in fact, been demonstrated by the applicant who undertook a travel survey which indicated a high

percentage attendance by way of sustainable travel modes. The findings are considered robust and relevant and therefore supportive of a lower on-plot parking quantum.

#### Cycling Provision

In terms of cycle parking there should be a minimum provision of 1 secure and accessible space for staff with 1 space per 10 patients. 3 'Sheffield' stands are proposed on the frontage which satisfies the requirement.

#### Temporary Dental Surgery

During construction, a temporary Dental surgery (two surgery rooms - GIFA 35.8m<sup>2</sup>) acting as a decant facility would be positioned to the rear of the site where the final on-plot parking provisions are proposed. This aspect of the proposal does not give rise for concern given current activities linked to the established Dental surgery use and its physical separation from the main construction site.

#### Vehicular Access Arrangements (Residential & Dental Practice)

In highway impact terms, there is no objection in principle to the parking layouts depicted on the frontage and to the rear of the site envelope and the access points are considered fit for purpose as demonstrated by the submitted vehicle swept path analysis.

#### Operational Refuse Requirements (Residential)

Refuse collection would occur from Northolt Avenue and a bin storage area is shown to the rear of the site. In order to conform to the Council's 'waste collection' maximum distance collection parameter of 10m i.e. distance from a refuse vehicle to the point of collection or bin store, arrangements should ensure that waste is positioned at a collection point within this set distance. With the arrangement as presented, the above parameter is met. There are no further observations.

#### Construction Management Plan (CMP)

The above will be a requirement given the constraints and sensitivities of the local road network in order to avoid/minimise potential detriment to the public realm. It will need to be secured under planning condition.

#### Conclusion

The application has been reviewed by the Highway Authority who are satisfied that the proposal would not discernibly exacerbate congestion or parking stress, and would not raise any measurable highway safety concerns, in accordance with Local Plan: Part 2 Development Plan Policies DMT 1, DMT 2 & DMT 6 and Policies 6.3, 6.9, and 6.13 of the London Plan (2016).

### **7.11 Urban design, access and security**

The proposals is considered to be satisfactory from an accessibility perspective. The Access officer has recommended conditions in the event of an approval.

The application seeks planning permission to erect a three-storey building comprising a dental surgery and 7 residential units (6 no. 1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to

the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery.

There have been previous pre-application discussions for this site under app: 27354/PRC/2020/181 where number of design concerns were raised. The submitted scheme has taken into consideration some of the concerns but others remain and are discussed below.

40 Station Approach is located on a prominent corner site where Northolt Avenue meets Station Approach. It is situated within a primarily residential area which is interspersed with some shops/ commercial buildings along Station Approach eastwards towards South Ruislip Station.

The buildings are primarily two to three-storeys in height on the south side of the street. Third storeys, where they exist, are contained within the roof respecting the domestic scale of the surrounding streets. The buildings on the north side of the street have a different character with larger blocks of three full storeys.

The existing building frontage is set back from the street on a similar alignment to the residential properties to the west of Northolt Avenue. There would be no objection to the proposed new front building line that corresponds to the buildings directly to the east in which the development would have a more direct visual relationship.

The retention and slight increase of the rear building line taken together with the frontage increase, however, will result in a much larger block than those within the group and the relatively unbroken mass would make the building dominate the southern side of the street in this location.

It is considered that the rear portion of the block would need to be reduced in height and depth so that there is a more comfortable transition between the new development and the bungalows / two-storey houses to the south along Northolt Avenue. It would also help the development relate better to the rear building lines of the group of buildings in which it is most closely related (nos. 40-50).

There are concerns with the roof access enclosure and lift over run which result in an intrusive and incongruous roof addition that adds visual clutter and detract from the appearance of the new building and wider streetscape. This element would seek undue attention particularly given the building's prominent corner location where it would be seen in prominent views along Station Road as well as those looking north along Northolt Avenue and the green open space of Acol Crescent

The use of brick as the primary construction material would be supported in principle and would respect the palette of materials in the area subject to an appropriate colour, tone, texture and mortar being chosen.

The scheme in its current form is not considered appropriate for the local context.

**7.12 Disabled access**

As discussed above

**7.13 Provision of affordable & special needs housing**

Not relevant to this application

**7.14 Trees, Landscaping and Ecology**

## TREES & LANDSCAPE

Policy DMHB 14 of The Local Plan: Part 2 - Development Management Policies (2020) states that:

A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.

B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.

C) Where space for ground level planting is limited, such as high rise buildings, the inclusion of living walls and roofs will be expected where feasible.

D) Planning applications for proposals that would affect existing trees will be required to provide an accurate tree survey showing the location, height, spread and species of trees. Where the tree survey identifies trees of merit, tree root protection areas and an arboricultural method statement will be required to show how the trees will be protected. Where trees are to be removed, proposals for replanting of new trees on-site must be provided or include contributions to off site provision.

The Council's Trees and Landscaping Officer has provided the following comments:

This site is occupied by a two-storey brick building located on a corner plot at the junction with Northolt Avenue. The ground-floor houses a dental surgery, with a residential unit on the first floor. An area of hard-standing to the front is currently used for parking and there is a rear service road which provides access to additional parking to the rear of the building. According to Google Streetview there are a two small trees to the rear of the building, however, there are no TPO's or Conservation Area designations affecting the site.

COMMENT This submission follows pre-application ref. 27354/PRC/2020/181. The two trees will be removed to facilitate car parking and bin / bike stores to the rear. The D&AS refers to secure and enclosed cycle storage and bin storage. It also refer to landscaping to the front of the building to enhance the public realm. Modest areas of soft landscape and green roofs are indicated on the landscape plan. There is an annotation referring to the use of native species. Where the soft landscape is part of the visual amenity for residents, it is unlikely that the use of native species will be suitable, however, there are ornamental plants which are also nectar-bearing species which will encourage biodiversity. Private outdoor amenity space is to be provided in the from of private balconies. These should be of sufficient size to be accessible and functional. The provision of soft landscape enhancement in front of the building for the benefit of the public realm is welcome, subject to detail. Its success will depend on the quality of design, implementation and future maintenance.

### **7.15 Sustainable waste management**

As discussed in section 07:17 below

### **7.16 Renewable energy / Sustainability**

Not relevant to this application

### **7.17 Flooding or Drainage Issues**

FLOOD AND WATER MANAGEMENT

The Council's Flood and Water Management Officer has provided the following comments:  
No objection based on the submitted information.

#### **7.18 Noise or Air Quality Issues**

As discussed in section 07:22 below

#### **7.19 Comments on Public Consultations**

Comments received are addressed within the body of this report

#### **7.20 Planning Obligations**

##### COMMUNITY INFRASTRUCTURE LEVY (CIL)

The Council adopted its own Community Infrastructure Levy (CIL) on August 1st 2014 and the Hillingdon CIL charge for additional floor space for residential developments is £95 per square metre and office developments of £35 per square metre. This is in addition to the Mayoral CIL charge of £60 per sq metre.

The existing floor area to be demolished is 141.6 sqm.

The proposed total additional floor area as advised in the CIL application form is 389.7 (residential) and 80.3 (non residential) equalling 470 sqm.

470 sqm total proposed - 141.6 sqm existing = 328.4 sqm

The net gain would comprise 297.4 sqm (residential), 31 sqm (non residential). = 328.4 sqm

This would equate to:

Hillingdon CIL £29,338

Mayoral CIL £19,704

Total = £49,042

#### **7.21 Expediency of enforcement action**

Not relevant to this application

#### **7.22 Other Issues**

##### AIR QUALITY

The following policy context is taken into consideration:

Paragraph 181 of the National Planning Policy Framework (February 2019) states that planning decisions should sustain and contribute towards compliance with relevant limit values or national objectives for pollutants, taking into account the presence of Air Quality Management Areas and Clean Air Zones, and the cumulative impacts from individual sites in local areas. Opportunities to improve air quality or mitigate impacts should be identified, such as through traffic and travel management, and green infrastructure provision and enhancement. Planning decisions should ensure that any new development in Air Quality Management Areas and Clean Air Zones is consistent with the local air quality action plan. Policy 7.14 of the London Plan (March 2016) further supports this.

Policy EM8 of the Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012) states that the Council will seek to safeguard and improve all land, water, air and noise quality. All development should not cause deterioration in the local air quality levels and should ensure

the protection of both existing and new sensitive receptors.

Policy DMEI 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) states:

A) Development proposals should demonstrate appropriate reductions in emissions to sustain compliance with and contribute towards meeting EU limit values and national air quality objectives for pollutants.

B) Development proposals should, as a minimum: i) be at least "air quality neutral"; ii) include sufficient mitigation to ensure there is no unacceptable risk from air pollution to sensitive receptors, both existing and new; and iii) actively contribute towards the improvement of air quality, especially within the Air Quality Management Area.

The application site is within the declared Hillingdon Air Quality Management Area and Air Quality Focus Areas: A40/ South Ruislip Focus Area. Focus Areas are where the pollution levels are already elevated and improvements are required to be made.

## NOISE

Policy D14 Noise of the London Plan (2021) states:

A In order to reduce, manage and mitigate noise to improve health and quality of life, residential and other non-aviation development proposals should manage noise by:

- 1) avoiding significant adverse noise impacts on health and quality of life
- 2) reflecting the Agent of Change principle as set out in Policy D13 Agent of Change
- 3) mitigating and minimising the existing and potential adverse impacts of noise on, from, within, as a result of, or in the vicinity of new development without placing unreasonable restrictions on existing noise-generating uses
- 4) improving and enhancing the acoustic environment and promoting appropriate soundscapes (including Quiet Areas and spaces of relative tranquillity)
- 5) separating new noise-sensitive development from major noise sources (such as road, rail, air transport and some types of industrial use) through the use of distance, screening, layout, orientation, uses and materials - in preference to sole reliance on sound insulation
- 6) where it is not possible to achieve separation of noise-sensitive development and noise sources without undue impact on other sustainable development objectives, then any potential adverse effects should be controlled and mitigated through applying good acoustic design principles
- 7) promoting new technologies and improved practices to reduce noise at source, and on the transmission path from source to receiver.

B Boroughs, and others with relevant responsibilities, should identify and nominate new Quiet Areas and protect existing Quiet Areas in line with the procedure in Defra's Noise Action Plan for Agglomerations.

Policy EM8 of the Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012) states that the Council will seek to ensure that noise sensitive development and noise generating development are only permitted if noise impacts can be adequately controlled and mitigated.

The ground floor level of the site comprises a dental surgery therefore had the application been acceptable in other respects conditions would have been imposed to address noise impacts.

## CONTAMINATED LAND

Policy DMEI 12 of The Local Plan: Part 2 - Development Management Policies (2020) states:

A) Proposals for development on potentially contaminated sites will be expected to be accompanied by at least an initial study of the likely contaminants. The Council will support planning permission for any development of land which is affected by contamination where it can be demonstrated that contamination issues have been adequately assessed and the site can be safely remediated so that the development can be made suitable for the proposed use.

B) Conditions will be imposed where planning permission is given for development on land affected by contamination to ensure all the necessary remedial works are implemented, prior to commencement of development.

C) Where initial studies reveal potentially harmful levels of contamination, either to human health or controlled waters and other environmental features, full intrusive ground investigations and remediation proposals will be expected prior to any approvals.

D) In some instances, where remedial works relate to an agreed set of measures such as the management of ongoing remedial systems, or remediation of adjoining or other affected land, a S106 planning obligation will be sought.

The Council's Contaminated Land Officer has provided the following comments:

Summary of Comments:

I have reviewed a copy of the following report:

Title: 40 Station Approach, Ruislip, HA4 6RZ, Geoenvironmental & Geotechnical Desk Study; Ref: RTCL-40SA-004 RevC; Date: 14/12/2020; Prepared by: R. Thorpe Consultancy Ltd. (RTC).

The report represents a Phase 1 study which includes a conceptual site model and preliminary risk assessment of the ground conditions (including environmental, geological, hydrological and hydrogeological conditions) at the site.

The assessed risks range from "Negligible to Moderate/Low" with regards to potential land contamination.

Details within the report are considered to be suitable and sufficient and acceptable to justify its conclusion that "intrusive investigation is not deemed necessary to further assess contamination risks."

However, I am recommending that if planning consent is granted then the following condition should be imposed:

No contaminated soils or other materials (e.g. subsoils, loose hardstanding surfacings etc) shall be imported to the site. All imported soils and/or other materials for landscaping purposes etc. shall be independently tested for chemical contamination and the results of this testing shall demonstrate the materials are clean and free of contaminants. The report/s shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the development.

REASON To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and the development can be carried out safely without unacceptable

risks to workers, neighbours and other offsite receptors in accordance with Hillingdon Local Plan: Part 2 (January 2020) Policies - DMEI 11: Protection of Ground Water Resources and DMEI 12: Development of Land Affected by Contamination.

Reason for Refusal (if objecting): N/A

Observations:

The report indicates that:

- A Moderate/Low risk to human health has been identified, which can be reduced to Low through use of PPE on site and the use of appropriate construction methods to reduce dust during groundworks and construction
- Site workers should have undertaken asbestos awareness training and have access to appropriate PPE including P3 masks, if required. If ACM is identified on site, an appropriately qualified person should attend site to inspect and advise.

## **8. Observations of the Borough Solicitor**

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning

applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

## **9. Observations of the Director of Finance**

Not applicable

## **10. CONCLUSION**

The currently proposed housing mix is considered to be contrary to the requirements of Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policy H10 of the London Plan (2021) and a higher proportion of 2-bed dwellings, as well as re-provision of the existing family unit is expected.

The 1-bed units equate to 86% of the total units provided in the scheme, which is considered to be contrary to the need to deliver a range of unit types at different price points across London and the requirement to deliver mixed and inclusive neighbourhoods. The scheme as is results in an over-dominance of 1-bed units and the loss of an existing family dwelling which is contrary to policy.

The applicant has not provided any justification or evidence to demonstrate why a housing mix more in line with policy cannot be provided as part of the proposed development. As a suggestion to the applicant, amalgamation of some of the units on the first and second floor to provide larger units seems very possible and it would allow for some of the design issues of the scheme, namely in relation to the single aspect dwellings on the first and second floor, to potentially be resolved and also provide a more policy compliant housing mix. As stated, the principle of residential development on this site is established.

The proposed development, by reason of the overall size and scale of the building would result in an incongruous and overly dominant form of development that would be detrimental to the character, appearance and visual amenities of the site, the street scene and the wider area. Furthermore the proposed roof access enclosure and lift over run by reason of its size, scale, bulk and design would result in an intrusive and incongruous roof addition that adds visual clutter and detracts from the appearance of the building and wider streetscape. As such the proposal would be contrary to Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policy D4 of the London Plan (2021) and the NPPF (2019).

Consequently the proposal is recommended for refusal

## **11. Reference Documents**

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)  
Hillingdon Local Plan: Part Two - Development Management Policies (January 2020)  
The London Plan (2021)  
Technical Housing Standards - Nationally Described Space Standard (2015)  
Hillingdon Design and Accessibility Statement: Accessible Hillingdon  
National Planning Policy Framework (2019)

**Contact Officer:** Diane Verona

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**40 STATION APPROACH  
 SOUTH RUISLIP**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**27354/APP/2020/4256**

Scale:  
**1:1,250**

Planning Committee:  
**North Page 85**

Date:  
**March 2021**



**HILLINGDON**  
 LONDON

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## Report of the Head of Planning, Transportation and Regeneration

**Address** 11 SANDY LODGE WAY NORTHWOOD

**Development:** Erection of a two storey building with habitable roofspace to create 3 x 2 -bed self-contained flats and 1 x 3-bed self-contained flat with forecourt car parking cycle parking, new vehicular access and amenity space following demolition of existing dwelling.

**LBH Ref Nos:** 16948/APP/2020/3623

**Drawing Nos:** Location Plan  
201  
202  
203  
204  
200  
Design and Access Statement

**Date Plans Received:** 04/11/2020 **Date(s) of Amendment(s):**

**Date Application Valid:** 04/11/2020

### 1. SUMMARY

The proposal is for the demolition of the existing dwelling and the erection of a two storey building with habitable accommodation within its roof space to create 3 x 2 bed self-contained flats and 1 x 3 bed self-contained flat, with associated amenity space, parking and installation of a new vehicular crossover to the front of the site.

The principle of introducing a flattened development at the site has already been established through the granting of planning permission under application number 16948/APP/2018/55. It is considered that the proposal has satisfactorily addressed the grounds for refusal attached to planning application number 16948/APP/2018/3921.

The proposal would not have a negative impact upon the visual amenity of the site or the surrounding area, nor would it result in an unacceptable loss of residential amenity to neighbouring occupiers. The proposal would provide adequate of internal and external space to future occupiers. It is considered that the provision of six off-street parking spaces is acceptable in this location, and that the proposed crossover would not prejudice pedestrian or highway safety. The details relating to tree protection measures, landscaping, cycle and refuse stores, inclusive design (i.e. disabled access), sustainable drainage and the sustainability credentials of the proposed building would be secured by condition, in the event of an approval.

The proposed associated works to the resident street parking bay are required by the recommended conditions and would be secured by a legal agreement under section 278 of the Highways Act, and have been agreed with the applicant.

It is therefore recommended that the application be approved subject to conditions.

### 2. RECOMMENDATION

**Approval, subject to the following:**

**1 RES3 Time Limit**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

**REASON**

To comply with Section 91 of the Town and Country Planning Act 1990

**2 RES4 Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, titled Location Plan and numbered 200, 201, 202, 203 and 204 and shall thereafter be retained/maintained for as long as the development remains in existence.

**REASON**

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

**3 RES7 Materials (Submission)**

No development shall take place until details of all materials and external surfaces, including details of balconies have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

**REASON**

To ensure that the development presents a satisfactory appearance in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**4 RES12 No additional windows or doors**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) England) Order 2015 (or any order revoking and reenacting that Order with or without modification), no additional windows, doors or other openings shall be constructed in the walls or roof slopes of the development hereby approved facing number 9 Sandy Lodge Way.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**5 RES13 Obscure Glazing**

Notwithstanding the approved drawings, the windows and roof lights facing number 9 Sandy Lodge Way shall be glazed with permanently obscured glass to at least level 4 on the Pilkington scale and be non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**6 RES8 Tree Protection**

No site clearance or construction work shall take place until the details have been

submitted to, and approved in writing by, the Local Planning Authority with respect to:

1. A method statement outlining the sequence of development on the site including demolition, building works and tree protection measures.
2. Detailed drawings showing the position and type of fencing to protect the entire root areas/crown spread of trees, hedges and other vegetation to be retained shall be submitted to the Local Planning Authority for approval. No site clearance works or development shall be commenced until these drawings have been approved and the fencing has been erected in accordance with the details approved. Unless otherwise agreed in writing by the Local Planning Authority. Such fencing should be a minimum height of 1.5 metres.

Thereafter, the development shall be implemented in accordance with the approved details.

The fencing shall be retained in position until development is completed.

The area within the approved protective fencing shall remain undisturbed during the course of the works and in particular in these areas:

- 2.a There shall be no changes in ground levels;
- 2.b No materials or plant shall be stored;
- 2.c No buildings or temporary buildings shall be erected or stationed.
- 2.d No materials or waste shall be burnt; and.
- 2.e No drain runs or other trenches shall be dug or otherwise created, without the prior written consent of the Local Planning Authority.

3. Where the arboricultural method statement recommends that the tree protection measures for a site will be monitored and supervised by an arboricultural consultant at key stages of the development, records of the site inspections / meetings shall be submitted to the Local Planning Authority.

#### REASON

To ensure that trees and other vegetation can and will be retained on site and not damaged during construction work and to ensure that the development conforms with policy DMHB 14 of the Hillingdon Local Plan Part 2 (2020)

#### **7 RES10 Tree to be retained**

Trees, hedges and shrubs shown to be retained on the approved plan(s) shall not be damaged, uprooted, felled, lopped or topped without the prior written consent of the Local Planning Authority. If any retained tree, hedge or shrub is removed or severely damaged during (or after) construction, or is found to be seriously diseased or dying, another tree, hedge or shrub shall be planted at the same place or, if planting in the same place would leave the new tree, hedge or shrub susceptible to disease, then the planting should be in a position to be first agreed in writing with the Local Planning Authority and shall be of a size and species to be agreed in writing by the Local Planning Authority and shall be planted in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier. Where damage is less severe, a schedule of remedial works necessary to ameliorate the effect of damage by tree surgery, feeding or groundwork shall be agreed in writing with the Local Planning Authority. New planting should comply with BS 3936 (1992) 'Nursery Stock, Part 1, Specification for Trees and Shrubs' Remedial work should be carried out to BS BS 3998:2010 'Tree work - Recommendations' and BS 4428 (1989) 'Code of Practice for General Landscape Operations (Excluding Hard Surfaces)'. The agreed work shall be completed in the first

planting season following the completion of the development or the occupation of the buildings, whichever is the earlier.

#### REASON

To ensure that the trees and other vegetation continue to make a valuable contribution to the amenity of the area in accordance with policy DMHB 14 of the Hillingdon Local Plan Part 2 (2020) and to comply with Section 197 of the Town and Country Planning Act 1990

### **8 OM19 Construction Management Plan**

Prior to development commencing, the applicant shall submit a demolition and construction management plan to the Local Planning Authority for its approval. The plan shall detail:

- (i) The phasing of development works
- (ii) The hours during which development works will occur (please refer to informative I15 for maximum permitted working hours).
- (iii) A programme to demonstrate that the most valuable or potentially contaminating materials and fittings can be removed safely and intact for later re-use or processing.
- (iv) Measures to prevent mud and dirt tracking onto footways and adjoining roads (including wheel washing facilities).
- (v) Traffic management and access arrangements (vehicular and pedestrian) and parking provisions for contractors during the development process (including measures to reduce the numbers of construction vehicles accessing the site during peak hours).
- (vi) Measures to reduce the impact of the development on local air quality and dust through minimising emissions throughout the demolition and construction process.
- (vii) The storage of demolition/construction materials on site.

The approved details shall be implemented and maintained throughout the duration of the demolition and construction process.

#### REASON

To safeguard the amenity of surrounding areas in accordance with Policies DMT 1, DMT 2 and DMT 6 of the Hillingdon Local Plan Part 2 (2020).

### **9 RES9 Landscaping (car parking & refuse/cycle storage)**

No development shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping
  - 1.a Planting plans (at not less than a scale of 1:100),
  - 1.b Written specification of planting and cultivation works to be undertaken,
  - 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate
2. Details of Hard Landscaping
  - 2.a Refuse Storage
  - 2.b Cycle Storage
  - 2.c Means of enclosure/boundary treatments
  - 2.d Car Parking Layouts (including demonstration that 1 of the car parking spaces will be an 'active' electrical charging point and for the remaining 5 spaces to be 'passive' electrical charging points)
  - 2.e Hard Surfacing Materials
  - 2.f External Lighting

2.g Other structures (such as play equipment and furniture)

3. Details of Landscape Maintenance

3.a Landscape Maintenance Schedule for a minimum period of 5 years.

3.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

4. Schedule for Implementation

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies DMHB 11, DMHB 12, DMHB 14, DMEI 1, DMT 2 and DMT 6 of the Hillingdon Local Plan Part 2 (2020).

**10 RES15 Sustainable Water Management (changed from SUDS)**

No development approved by this permission shall be commenced until a scheme for the provision of sustainable water management has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include the following details:

- i. provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
- ii. include a timetable for its implementation; and
- iii. provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iv. provide details of water collection facilities to capture excess rainwater;
- v. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure the development does not increase the risk of flooding in accordance with Policy DMEI 10 of the Hillingdon Local Plan Part 2 (2020) and Policy SI 12 and SI 13 of the London Plan (2021).

**11 NONSC Non Standard Condition**

The dwellings hereby approved shall not be occupied until they have achieved as a minimum, a water efficiency standard of not more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day for in accordance with the optional requirement defined within Approved Document G of the Building Regulations).

REASON

In the interests of water efficiency and to comply with Policy SI 2 of the London Plan (2021).

**12 NONSC Non Standard Condition**

No development approved by this permission shall be commenced until an energy statement has been submitted to and approved in writing by the local planning authority. The energy statement shall demonstrate how the development will achieve a reduction of at least 19% in carbon emissions below the normal requirement set by the Building Regulations and detail the supply of at least 10% of energy requirements from decentralised and renewable or low-carbon energy sources (provided that these requirements have not been proven to make the development unviable). The development shall be carried out in accordance with the approved energy statement.

**REASON**

In the interests of energy conservation and to comply with Policy SI 2 of the London Plan (2021).

**13 NONSC Non Standard Condition**

Prior to works commencing, details of step free access via the principal private entrance shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

**REASON**

To ensure that an appropriate standard of housing stock is achieved and maintained, in accordance with the Policy D7 of the London Plan (2021).

**14 NONSC Non Standard Condition**

The dwellings hereby approved shall be constructed to meet the standards for a Category 2 M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, and all such provisions shall remain in place for the life of the building.

**REASON**

To ensure that an appropriate standard of housing stock is achieved and maintained, in accordance with the Policy D7 of the London Plan (2021).

**15 HO7 No roof gardens**

The roof area of the single storey rear elements of the building hereby permitted shall not be used as a balcony, roof garden, terrace, balcony, patio or similar amenity area.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**16 RES22 Parking Allocation**

The residential units hereby approved shall not be occupied until a parking allocation scheme has been submitted to, and approved in writing by, the Local Planning Authority. The parking allocation scheme shall, as a minimum, include a requirement that all on-site car parking shall be allocated and dedicated for the use of each of the residential units hereby approved and shall remain allocated and dedicated in such a manner for the lifetime of the development.

**REASON**

To ensure that an appropriate level of car parking provision is provided on site in accordance with Policy DMT 6 of the Hillingdon Local Plan Part Two 2 (2020) and Policy T6 of the London Plan (2021).

**17 RES24 Secured by Design**

The dwelling(s) shall achieve 'Secured by Design' accreditation awarded by the Hillingdon Metropolitan Police Crime Prevention Design Adviser (CPDA) on behalf of the Association of Chief Police Officers (ACPO). No dwelling shall be occupied until accreditation has been achieved.

**REASON**

In pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in excising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to ensure the development provides a safe and secure environment in accordance with policy DMHB 15 of the Local Plan Part 2 (2020) and Policy D11 of the London Plan (2021).

**18 NONSC Non Standard Condition**

The development hereby approved shall not be occupied until the highway works as detailed on drawing number 202, including reinstatement of the footway, have been completed to the reasonable satisfaction of the local planning authority.

**REASON**

In the interest of highway safety and to comply with Policies DMT2 and DMT6 of the of the Hillingdon Local Plan Part Two 2 (2020).

**INFORMATIVES**

**1 172 Section 106 Agreement**

You are advised that this permission has been granted subject to a legal agreement under Section 106 of the Town and Country Planning Act 1990.

**2 173 Community Infrastructure Levy (CIL) (Granting Consent)**

Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the London Borough of Hillingdon Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the London Borough of Hillingdon CIL Charging Schedule 2014 and the Mayor of London's CIL Charging Schedule 2012. Before commencement of works the development parties must notify the London Borough of Hillingdon of the commencement date for the construction works (by submitting a Commencement Notice) and assume liability to pay CIL (by submitting an Assumption of Liability Notice) to the Council at [planning@hillingdon.gov.uk](mailto:planning@hillingdon.gov.uk). The Council will then issue a Demand Notice setting out the date and the amount of CIL that is payable. Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed.

The above forms can be found on the planning portal at: [www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil](http://www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil)

Pre-Commencement Conditions: These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of the pre-commencement conditions have been discharged/complied with.

**3 115 Control of Environmental Nuisance from Construction Work**

Nuisance from demolition and construction works is subject to control under The Control

of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance' The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit ([www.hillingdon.gov.uk/noise](http://www.hillingdon.gov.uk/noise) Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

#### **4 147 Damage to Verge - For Council Roads:**

The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

For Private Roads: Care should be taken during the building works hereby approved to ensure no damage occurs to the verge of footpaths on private roads during construction. Vehicles delivering materials to this development shall not override or cause damage to a private road and where possible alternative routes should be taken to avoid private roads. The applicant may be required to make good any damage caused.

#### **5 159 Councils Local Plan : Part 1 - Strategic Policies**

On this decision notice policies from the Councils Local Plan: Part 1 - Strategic Policies appear first, then relevant Local Plan Part 2 (2020), then London Plan Policies (2021). Hillingdon's Full Council adopted the Hillingdon Local Plan: Part 1 - Strategic Policies on 8 November 2012 and the Hillingdon Local Plan Part 2 on 16 January 2020.

#### **6 170 LBH worked applicant in a positive & proactive (Granting)**

In dealing with the application the Council has implemented the requirement in the National Planning Policy Framework to work with the applicant in a positive and proactive way. We have made available detailed advice in the form of our statutory policies from Local Plan Part 1, Local Plan Part 2, Supplementary Planning Documents, Planning Briefs and other

informal written guidance, as well as offering a full pre-application advice service, in order to ensure that the applicant has been given every opportunity to submit an application which is likely to be considered favourably.

### **3. CONSIDERATIONS**

#### **3.1 Site and Locality**

The application site is a generously sized corner plot, located on the Western side of Sandy Lodge Way South of its junction with Grove Road. It currently comprises a detached chalet bungalow with an attached garage to the side. The front garden is mainly landscaped with a driveway to one side leading to the garage. The site also benefits from a reasonably sized rear garden.

The street scene is residential in character with two storey properties to the southern side and to the rear, as well as on the opposite corner of the junction. The properties on the opposite side of Sandy Lodge Way are more modern 3 storey flatted developments.

The application site does not contain any Listed Buildings, nor does it lie within a designated Conservation Area or an Area of Special Local Character. However, the site is located within an area covered by Tree Preservation Order number 746.

Based on the Transport for London's WebCAT planning tool, the application site has a Public Transport Accessibility Level (PTAL) rating of 1b and 2 (very poor/ poor). The highway on Sandy Lodge Way is designated within a Controlled Parking Zone.

The site is in Flood Zone 1.

#### **3.2 Proposed Scheme**

The proposal is for the demolition of the existing dwelling and the erection of a two storey building with habitable accommodation within its roof space to create 3 x 2 bed self-contained flats and 1 x 3 bed self-contained flat, with associated amenity space, parking and installation of a new vehicular crossover to the front of the site.

#### **3.3 Relevant Planning History**

16948/APP/2015/4658 11 Sandy Lodge Way Northwood

Erection of two x two storey, 4-bed, detached dwellings to include habitable roofspace and basement with associated parking and amenity space and installation of 1 vehicular crossover

**Decision:** 03-08-2016 Approved

16948/APP/2018/3921 11 Sandy Lodge Way Northwood

Erection of a two storey building with habitable basement and roof space to create 1 x 3-bed and 3 x 2-bed self-contained flats with associated parking and installation of vehicular crossover to front, involving demolition of existing dwelling house

**Decision:** 04-01-2019 Refused

16948/APP/2018/55 11 Sandy Lodge Way Northwood

Erection of a two storey building with habitable basement and roof space to create 1 x 3-bed and 3 x 2-bed self-contained flats with associated parking and installation of vehicular crossover to front, involving demolition of existing dwelling house.

**Decision:** 23-05-2018 Approved

16948/APP/2020/3151 11 Sandy Lodge Way Northwood

Details pursuant to discharge condition Nos. 3 (Materials) 6 (Site Clearance) 8 (Landscape Scheme) and 12 (Parking Allocation Scheme) of planning permission ref: 16948/APP/2018/55 dated 29/05/2018 (Erection of a two storey building with habitable basement and roof space to create 1 x 3-bed and 3 x 2-bed self-contained flats with associated parking and installation of vehicular crossover to front, involving demolition of existing dwelling house)

**Decision:**

16948/APP/2020/3645 11 Sandy Lodge Way Northwood

Details pursuant to condition 9 (Ground investigation) of planning permission Ref: 16948/APP/2018/55 dated 29/05/2018 (Erection of a two storey building with habitable basement and roof space to create 1 x 3-bed and 3 x 2-bed self-contained flats with associated parking and installation of vehicular crossover to front, involving demolition of existing dwelling houses)

**Decision:** 15-01-2021 Approved

16948/PRC/2015/135 11 Sandy Lodge Way Northwood

Demolition of existing dwelling and redevelopment of site to provide two x 4 bedroom dwellings

**Decision:** 03-03-2016 OBJ

16948/PRC/2017/105 11 Sandy Lodge Way Northwood

Demolition of existing dwelling and erection of 4 flats in a two and half storey building with partial basement and front and rear parking

**Decision:** 09-08-2017 OBJ

16948/PRC/2017/32 11 Sandy Lodge Way Northwood

Demolition of the existing dwelling and the erection of 4 flats in a two and a half storey building

**Decision:** 31-03-2017 OBJ

#### **Comment on Relevant Planning History**

The most relevant planning history records relates to application numbers 16948/APP/2018/55 and 16948/APP/2018/3921, which both relate to schemes involving the erection of four flats at the site (see full details below).

Ref: 16948/APP/2018/55

Proposal: Erection of a two storey building with habitable basement and roof space to

create 1 x 3-bed and 3 x 2-bed self-contained flats with associated parking and installation of vehicular crossover to front, involving demolition of existing dwelling house.  
Status: Approved on 23.05.2018

Ref: 16948/APP/2018/3921

Proposal: Erection of a two storey building with habitable basement and roof space to create 1 x 3-bed and 3 x 2-bed self-contained flats with associated parking and installation of vehicular crossover to front, involving demolition of existing dwelling house.

The officer report comments on the changes from the approved scheme as follows:

'This proposal has increased the height of the roof in between the two side projecting hips and raising the eaves height where the building faces Grove Road, in comparison to the approved scheme. This results in an overly large crown roof detail, which appears disproportionate to the overall character of the building and would be a bulky and incongruous addition within this prominent corner position. The proposal also includes the provision of 2m high screens in the centre of the rear first floor terrace and along the southern boundary. Although not fully shown on all of the submitted elevations these would also be conspicuous additions, which would detract from the character and appearance of the proposed building. As such in terms of design the proposal is considered to be out of keeping with the character and appearance of the surrounding area.'

Status: Refused on 04.01.2019

Grounds for refusal:

1. The proposal, by reason of its size, scale, bulk and design, would represent a visually unsympathetic form of development that would detract from the character, appearance and visual amenity of the street scene and the wider area. The proposal would therefore be contrary to Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (November 2012), Policies BE13, BE15 and BE19 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012), Policies 3.5 and 7.4 of the London Plan (2016), the adopted Supplementary Planning Document HDAS: Residential Layouts and the NPPF.

2. The proposed development by reason of the rear first floor terrace and its proximity to the neighbouring properties would be detrimental to the amenities of the adjoining occupiers by reason of overlooking and loss of privacy. Therefore the proposal would be contrary to Policies BE19, BE21 and BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and the Council's adopted Supplementary Planning Documents HDAS: Residential Extensions and HDAS: Residential Layouts.

#### **4. Planning Policies and Standards**

Development Plan

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)

The Local Plan: Part 2 - Development Management Policies (2020)

The Local Plan: Part 2 - Site Allocations and Designations (2020)

The West London Waste Plan (2015)

The London Plan (2021)

## Material Considerations

The National Planning Policy Framework (NPPF) (2019) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

### **UDP / LDF Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

#### Part 1 Policies:

- PT1.BE1 (2012) Built Environment
- PT1.H1 (2012) Housing Growth
- PT1.EM6 (2012) Flood Risk Management

#### Part 2 Policies:

- DMEI 10 Water Management, Efficiency and Quality
- DMEI 9 Management of Flood Risk
- DMH 2 Housing Mix
- DMH 7 Provision of Affordable Housing
- DMHB 11 Design of New Development
- DMHB 12 Streets and Public Realm
- DMHB 14 Trees and Landscaping
- DMHB 15 Planning for Safer Places
- DMHB 16 Housing Standards
- DMHB 17 Residential Density
- DMHB 18 Private Outdoor Amenity Space
- DMT 2 Highways Impacts
- DMT 6 Vehicle Parking
- LLP D1 (2021) London's form character and capacity for growth
- LPP D4 (2021) Delivering good design
- LPP D5 (2021) Inclusive design
- LPP D6 (2021) Housing quality and standards
- LPP D7 (2021) Accessible housing
- LPP D8 (2021) Public realm
- LPP D11 (2021) Safety, security and resilience to emergency
- LPP D12 (2021) Fire safety
- LPP DF1 (2021) Delivery of the Plan and Planning Obligations
- LPP SI12 (2021) Flood risk management
- LPP SI13 (2021) Sustainable drainage
- LPP T5 (2021) Cycling
- LPP T6 (2021) Car parking
- NPPF- 11 NPPF-11 2018 - Making effective use of land

NPPF- 12	NPPF-12 2018 - Achieving well-designed places
NPPF- 2	NPPF-2 2018 - Achieving sustainable development
NPPF- 5	NPPF-5 2018 - Delivering a sufficient supply of homes

## **5. Advertisement and Site Notice**

**5.1** Advertisement Expiry Date:- Not applicable

**5.2** Site Notice Expiry Date:- Not applicable

## **6. Consultations**

### **External Consultees**

Following an amendment to the wording of the proposal description, 39 neighbouring properties were consulted on 26th November 2020. The consultation period expired on 17th December 2020. 25 objections from separate addresses were received and their comments are summarised as follows:

- The proposal includes Juliet balconies and terraces, despite these having previously been rejected (16948/APP/2018/3921);
- Eight windows would be overlooking towards No.9 Sandy Lodge Way. If approved, then all of these need to remain glazed with permanently obscured glass and be non opening to prevent overlooking to adjoining properties;
- The previous application decision prohibited the roof area being used as a balcony, roof garden or similar amenity area to prevent overlooking to adjoining properties;
- The new application includes additional roof windows which will create significant overlooking into the nursery bedroom at No.9 Sandy Lodge, and straight into this neighbouring property's main family bathroom;
- It should be noted that whilst the property at No.9 Sandy Lodge has one Juliet balcony, this is in a bedroom which is used to sleep in. This is substantially different to the overlooking opportunities presented when having a Juliet balcony in a kitchen/dining area which are used constantly throughout the day.
- The proposed change from the previously approved semi-detached properties to flats is a completely different situation;
- The proposal has second floor terraces looking directly into the living area of No.16 Grove Road from an increased elevation;
- We fully understand the need for more housing, but does it have to look directly into other people's living rooms?;
- We already suffer from the huge oak tree in the garden of no. 11 (TPO,) creating serious subsidence problems to our garage;
- Concerns about the ground water drainage and the flow of water running across the public pathway of Grove Road;
- The building will be visually overbearing. It is an inappropriate design for this quiet corner of Northwood. Such a building would be totally out of keeping with the neighbouring properties, which are detached homes on a quiet residential street;
- The height of the proposed building is to be raised from 7720 to 8290 - an increase of 570cm.
- The additional height and especially terraces are of great concern to our privacy as new residents can look down into our bedrooms which is only a few meters away. This loss of privacy will have a significant detrimental impact on the peaceful enjoyment of our home and the homes of our neighbours on Grove Road;
- The dustbin store appears to be located at the back of the flats, and I am extremely concerned that this arrangement will lead to an increase of vermin in the area.;

- The application states that there is capacity for three cars to be parked on the current site - however it is apparent that there is only enough space for two cars at a maximum;
- Visually, this will not be in keeping with other properties on Grove Road. As the property sits on the corner of Grove Road and Sandy Lodge Way, the impact to Grove Road needs to be considered. There are no properties of this size on Grove Road, so this development is not in keeping with the existing character of the area. ;
- Additional traffic on what is an already busy road. Furthermore, with a single entrance/exit, it is likely that this will create congestion and potential hazardous situations.
- There appears to be a terrace from the loft, which would overlook properties in Grove road and result in a loss of privacy;
- In reality this is a 3-storey property with the loft being used as living areas. Difficult to understand how this planning application is submitted on the basis of a 2-storey property, with a negative visual impact from Grove Road properties in close proximity;
- Allowing this development, will set a precedence for developing multiple dwelling properties on existing gardens and this will completely ruin the character of the area;
- On-site parking is less than required for four flats for residents and visitors, and so the scheme should be reduced to three flats and more space left at the front of the property for enough parking and landscaping. The parking management scheme, voted for by the majority of residents, prevents the space on the road, and may well lead to some additional parking outside the parking management scheme area;
- There is a large balcony for the flats in the loft which will cause adverse overlooking of the property to the rear. It should be a window only with a Juliet balcony. It is unclear if the property breaches the 45 degree rule;
- Sandy Lodge Way is already heavily populated by multiple apartment blocks and road traffic is already at an all-time high. The removal of a single occupancy dwelling and its replacement by a multiple occupancy building will further increase the level of road traffic, placing additional pressure on the NHS GP surgery service, as well as education provision;
- Sandy Lodge Way, Dene Road, and Grove Road can't handle any more congestion or wait while another lovely single-dwelling home is demolished to build flats! There are enough flats and enough traffic on this road;
- Concerned that this proposed development will greatly impact traffic flow in an area that is already busy given its proximity to the high street and St Martins school - particularly during the proposed construction phase;
- All the fantastic architecture of old Northwood is gradually being depleted and destroyed by people who just want to make money without due respect to the residents and those who love Northwood as a green village with well spaced houses.
- Where are the structural plans to ensure no collapse ? There are no calculations or methodology for earthwork support;
- The proposal, by reason of its size, scale, bulk and design, would represent a visually unsympathetic form of development. It would detract from the character, appearance and visual amenity of Grove Road's street scene and encourage more to attempt to repeat the process;
- The bin store area would lead to an increase in vermin in the area which has already proven to be an issue;

Officer's response: The concerns raised in regards to design, character and appearance, neighbouring residential amenity, sustainable drainage, parking and highway safety will be addressed in the section below titled 'Main Planning Issues'.

### **Internal Consultees**

Council's Flood and Water Management Team: A basement is no longer proposed. Planning officers should apply the Government's standing advice and ensure that all hard surfaces are permeable and that surface water run-off is managed on-site.

Council's Highways Officer: The application has been reviewed by the Highway Authority who are

satisfied that the proposal would not discernibly exacerbate congestion or parking stress, and would not raise any measurable highway safety concerns, in accordance with Local Plan: Part 2 Development Plan Policies DMT 1, DMT 2 & DMT 6.

Council's Access Officer: There are not deemed to be any accessibility issues raised by the proposal. However, the following conditions should be attached to any grant of planning permission.

Prior to works commencing, details of step free access via the principal private entrance shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

Reason: To ensure that an appropriate standard of housing stock, in accordance with the Policy D7 of the London Plan (2021), is achieved and maintained.

The dwellings hereby approved shall be constructed to meet the standards for a Category 2 M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, and all such provisions shall remain in place for the life of the building.

Reason: To ensure that an appropriate standard of housing stock, in accordance with Policy D7 of the London Plan (2021), is achieved and maintained.

Council's Environmental Health Officer: No comments received.

Council's Trees and Landscape Officer: This re-development was first approved under application reference 16948/APP/2018/55. The application was supported by a tree report by Merwood and the layout included a soft landscape buffer along the front boundary which would have partly screened the car park, which dominates the front of the property. Although the tree report is technically out of date, in this case an updated tree protection plan and arboricultural method statement could be submitted through a pre-commencement condition (RES8). The layout has been amended in the current scheme and the hedge / planting to the south side of the entrance has been removed. This planted strip needs to be re-instated.

Officer's Response: The Trees and Landscape Officer's comments about the reinstatement of planting will be addressed through the imposition of a landscaping scheme condition, in the event of a recommendation for approval. Similarly, the requested updated tree report would be secured via a pre-commencement condition.

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

Paragraph 118 of the NPPF (2019) states that planning policies and decisions should "promote and support the development of under-utilised land and buildings, especially if this would help to meet identified needs for housing where land supply is constrained and available sites could be used more effectively".

Policy H1 of the London Plan (2021) emphasises that the Mayor recognises the pressing need for more homes in London, and will work with relevant partners to ensure that housing need is met.

Policy DMH 4 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that residential conversions and the redevelopment of dwellings into new blocks of flats will only be permitted where:

- i) it is on a residential street where the proposal will not result in more than 10% of properties being redeveloped into flats;

- ii) On residential streets longer than 1km the proposed redevelopment site should be taken as the midpoint of a 1km length of road for assessment purposes;
- iii) the internal floor area of the original building to be converted is at least 120 sqm; and
- iv) units are limited to one unit per floor for residential conversions.

Notwithstanding the above policy context, the planning history attached to the application site is a material consideration and is considered to carry significant weight. In this case, planning permission has already been approved for a flatted development at the site under application number 16948/APP/2018/55. The Committee Report relating to this approved permission raised no objection, in principle, to the intensification of the residential use of the site.

The most recent scheme considered by the Council at this site under application number 16948/APP/2018/3921 was refused. However, it is important to note the Committee Report relating to this refused application raised no objection to the principle of the flatted development itself, nor did this form part of the grounds for refusal (refer to the planning history section of this report for further details). In reaching this position, the Committee Report relating to application number 16948/APP/2018/3921 stated that: "It is acknowledged that there have been a number of properties and sites redeveloped along Sandy Lodge Way. This includes flats and in-fill, although having regard to recent developments within 1 km of the application site, there is insufficient information to show that the 10% guideline has been exceeded. However, the design and scale of the proposal is not dissimilar to other large, detached properties within the street scene and having regard to the Inspectors comments [38-40 Ducks Hill Road], it may be unreasonable to refuse on this basis."

Within the Appeal Decision relating to 38-40 Ducks Hill Road, the Inspector advised that "it appears that HDAS was originally prepared by the Council in 2006 as an interim document in anticipation of more specific policies within the Core Strategy. Since the 10% rule was not subsequently included within the development plan. I conclude this is guidance only. Furthermore there is no evidence of clear justification for the 10% figure, which seems to be arbitrary rather than based on clear evidence. Consequently the 10% should be a material consideration but one which should not be used on its own as a means of determination as whether a proposal is acceptable." (Appeal Decision reference APP/R5510/W/17/3177014)

Since the Appeal Decision at 38-40 Ducks Hill Road, the Hillingdon Design and Accessibility Statement Supplementary Planning Document: Residential Layouts (2006) has been superseded by the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). However, the 10% threshold referred to within the SPD has been incorporated into Policy DMH 4 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), which states that flatted development will only be permitted where proposals will not result in more than 10% of properties being redeveloped into flats. However, given that no grounds for refusal were raised about the potential exceedance of the 10% threshold under the previous two schemes at this site, and noting the weight the previous Committee Report gave to the Appeal decision at 38-40 Ducks Hill Road (refer above), it is considered that a refusal on this basis would be very difficult to sustain in the event of an Appeal.

Having regard to the planning history of the site, which is considered to carry a significant degree of weight, and noting the residential character of the surrounding area, it is considered, on balance, that the principle of providing additional residential accommodation

at the site is acceptable, in principle. However, this is subject to the other planning considerations and details, being in accordance with the all the relevant planning policies and guidance.

## **7.02 Density of the proposed development**

Policy DMHB 17 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all new residential development should take account of the Residential Density Matrix contained in Table 5.2. Developments will be expected to meet habitable rooms standards.

The site has a Public Transport Accessibility Level (PTAL) of 1b and 2 (very poor/ poor). The proposed density level is considered to be acceptable given the suburban context of the site. It should also be noted that numerical density levels are considered to be more appropriate to larger sites, and are not typically used in the assessment of schemes of less than 10 units such as that proposed with this application. In such cases, it is often more appropriate to consider how the development harmonises with its surroundings and its impact on adjoining occupiers.

## **7.07 Impact on the character & appearance of the area**

Policy D4 of the London Plan (2021) requires that new development is of high quality design.

Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012) states that all new developments should achieve a high quality of design in all new buildings and the public realm contributes to community cohesion and a sense of place.

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all development will be required to be designed to the highest standards and incorporate principles of good design. It should take into account aspects including the scale of the development considering the height, mass and bulk of adjacent structures; building plot sizes and established street patterns; building lines and streetscape rhythm and landscaping.

Policy DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) advises that development should be well integrated with the surrounding area. It should ensure public realm design takes account of the established townscape character and quality of the surrounding area.

There are a diverse range of styles, designs and materials in the makeup of the existing properties within the street scene. This comprises two storey detached dwellings on the western side of Sandy Lodge Way and Grove Road and three storey modern flatted properties opposite. The existing dwelling spans virtually the whole width of the plot, with the garage extending to the side boundary with No. 9 Sandy Lodge Way, and is set-back approximately 1.75 metres from the boundary with Grove Road.

The proposed building measures approximately 16.7 metres in width and 13.8 metres in depth, with an additional single storey element to the rear of 3.6 metres in depth (from the rear gables). It would have a main roof ridge height of approximately 8.3 metres (compared against the ridge height on the neighbouring property at No. 9 Sandy Lodge Way at approximately 8.05 metres). The proposed building has been designed to reflect the character of the adjacent properties at Nos. 7 and 9 Sandy Lodge Way, and incorporates detailed front and rear gable features, with the southernmost front gable at a height of 8.9 metres (approximately).

Since the previous refusal under application number 16948/APP/2018/3921, changes have been made to the scheme in order to address the grounds for refusal. The ridge line of the crown roof along the Grove Road frontage has been reduced in height to approximately 8.04 metres. This element of the crown roof would now sit below the ridge height of the two side projecting hipped roof forms, similar to the scheme approved by the Council in 2018. A section of the eaves line of the proposed building along the Grove Road frontage has been lowered, as per the roof design approved under the approved 2018 scheme. Additionally, the previously proposed first floor rear terrace, which was deemed inappropriate under the refused application, has been removed.

The amendments that have been made to the current scheme has satisfactorily address the grounds for refusal attached to application number 16948/APP/2018/3921. It is therefore considered that the proposal would not cause harm to the character, appearance and visual amenity of the area, in accordance with the NPPF (2019), Policy D4 of the London Plan (2021), Policy BE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) and Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.08 Impact on neighbours**

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that: B) Development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

Paragraph 5.38 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that: "The Council will aim to ensure that there is sufficient privacy for residents and it will resist proposals where there is an unreasonable level of overlooking between habitable rooms of adjacent residential properties, schools or onto private open spaces. A minimum of 21 metres separation distance between windows of habitable rooms will be required to maintain levels of privacy and to prevent the possibility of overlooking. In some locations where there is a significant difference in ground levels between dwellings, a greater separation distance may be necessary."

Paragraph 5.40 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states: "For the purposes of this policy, outlook is defined as the visual amenity enjoyed by occupants when looking out of their windows or from their garden. The Council will expect new development proposals to carefully consider layout and massing in order to ensure development does not result in an increased sense of enclosure and loss of outlook."

The proposed building would maintain a front building line with the adjacent property at No. 9 Sandy Lodge Way. It would be set-away from the mutual boundary shared with No.9 by approximately 1.8 metres, and approximately 3.4 metres from the nearest flank wall at No.9. The rear building line of the proposed building would project approximately 1 metre (excluding the rear gable face features) beyond the rear elevation of the neighbouring property at No.9 at first floor level, with an additional 4.5 metres (approximately) projection at ground floor level.

It is acknowledged that the ground floor rear building line of the proposed building would exceed the prescribed depth for single storey rear extensions set out in Policy DHMHD 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). However, it is worth highlighting that the separation distances, depth and ridge height of the proposed building in relation to the neighbouring property at No.9 Sandy Lodge Way is very similar to that previously deemed acceptable by the Council under the previously approved planning

application (ref: 16948/APP/2018/55). The Committee Report for the previously approved scheme included the following assessment in relation to the impact upon the residential amenities of the neighbouring occupiers at No.9:

"However, given the degree of separation, and given that the proposal is situated to the North of the neighbouring property [at No.9] and that it would not compromise a 45 degree line of sight from the rear first floor windows, it is not considered the proposed rear projection would result in over dominance or loss of outlook to the neighbouring property [at No.9]. It is noted that there are windows on the side elevation of no. 9 facing the application site and concern has been raised regarding the loss of light as a result of the proposal. The proposal would bring the main bulk of the dwelling slightly closer to the neighbouring property and be higher than the existing dwelling. However it would also remove the existing single storey element currently situated adjacent to the boundary. It is also noted that the ground floor window serves the kitchen area of an open plan living space with additional windows on the rear elevation.

The neighbour [at No.9 Sandy Lodge Way] has advised that the first floor window is currently serving a nursery room. This was originally approved as a secondary window serving a dressing area to a bedroom, with a primary window facing the rear elevation. In consideration of the application for that side extension to no. 9 a side facing window serving a habitable room would have been considered unacceptable. There is also no indication that this room has been subdivided from the main bedroom with the original floor plans for the extension identifying the main entrance to that room was through the dressing room. It is also noted that there is an extant permission [attached to the application site] for the [erection of ] semi detached properties, which were approved prior to the change of use of that room. Therefore it would be considered unreasonable to refuse this proposal on that basis. The second floor window serves the loft space. Therefore, having regard to the North facing aspect, it is not considered that the proposal would result in the loss of sunlight or a significant loss of light or amenity to the adjacent property [at No.9]."

The current scheme includes windows and roof lights fitted in the side (south) elevation of the proposed building. These proposed windows and roof lights would either serve non habitable rooms such as a bathroom and utility room, or as secondary windows to an open plan kitchen or bedrooms (with the principal windows serving these habitable rooms being fitted in the front or rear elevations). As such, a condition would be secured requiring all the windows and roof lights facing No.9 Sandy Lodge Way to be obscure glazed and non-opening up to 1.8 metres of the finished floor level, in the event of an approval.

There would be Juliet balconies fitted in the rear elevation of the proposed building. However, these balconies would not contain a platform and therefore no external access would be available to the green roof on the single storey rear element of the building. The outlook provided by the proposed Juliet balconies would not be dissimilar to that of first floor windows. It is also noted the previous schemes under application numbers 16948/APP/2018/55 & 16948/APP/2018/3921 included Juliet balconies, and that no objections were raised by the Council in respect to these features. Taking these factors into account, it is considered that the Juliet balconies being considered as part of the current application would not result in an unacceptable loss of privacy or overlooking, over and beyond that deemed permissible under approved application number 16948/APP/2018/55.

Although it is noted that the recessed roof terraces have been retained as part of the current proposal, no concerns were raised by the Council, in regards to this aspect of the

proposal, under application number 16948/APP/2018/3921. The delegated report states the "loft floor terraces are small and set within the roof slope and are not considered to afford a significantly greater view of the rear of the neighbouring properties than the previously approved gable window." Taking into account the Council's previous stance in regards to the proposed recessed roof terraces, it is considered that a refusal on the ground of loss of privacy or overlooking would be unreasonable, and difficult to sustain in the event of an Appeal.

The previously proposed first floor terrace has been removed from the current proposal, since the most recent refusal under application number 16948/APP/2018/3921. It is therefore considered that the current scheme has addressed the second ground for refusal attached to application number 16948/APP/2018/3921 (refer to planning history section above). If planning permission were to be granted, a condition would be secured to prevent the green roof on the single storey rear element of the proposed building from being used as a roof garden, terrace, balcony, patio or other similar amenity area.

There would be a separation distance of approximately 24 metres between the proposed building and the mutual boundary shared with No. 16 Grove Road and the front elevation of No.35 Grove Road. Given these separation distances, coupled with the scale and height of the proposed building, it is considered that the proposal would not adversely impact the living conditions of the occupiers at Nos. 16 and 35 Grove Road, in terms of loss of light, privacy, outlook and overbearing impact.

In light of the above, it is considered that the proposed development would not cause undue harm to the residential amenities of neighbouring occupiers, in accordance with the NPPF (2019), Policy BE1 of the Hillingdon Local Plan: Part One Strategic Policies (2012) and Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

## **7.09 Living conditions for future occupiers**

### **INTERNAL AMENITY SPACE:**

Policy DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all housing development should have an adequate provision of internal space in order to provide an appropriate living environment. To achieve this all residential development or conversions should: i) meet or exceed the most up to date internal space standards, as set out in Table 5.1; and ii) in the case of major developments, provide at least 10% of new housing to be accessible or easily adaptable for wheelchair users.

Policy D6 of the London Plan (2021) sets out the minimum internal floor space standards required for residential developments in order to ensure that there is an adequate level of amenity for existing and future occupants. Table 3.1 requires:

- A one-storey dwelling with 2 bed spaces, 4 person occupancy, to have a gross internal area of at least 70 square metres;
- A one-storey dwelling 3 bed spaces, 6 person occupancy, to have a gross internal area of at least 95 square metres.

The DCLG's Technical housing standards (2015) requires that double bedrooms measure at least 11.5 square metres in floor space, and for single bedrooms to measure at least 7.5 square metres and be at least 2.15 metres wide.

The submitted drawings show that the proposed dwellings would have the following gross internal floors areas:

Flat 1 (2 bed, 4 person) is 151 square metres, Flat 2 (2 bed, 4 person) is 106 square metres, Flat 3 (3 bed, 6 person) is 151 square metres and Flat 4 (2 bed, 4 person) is 130 square metres. All of the proposed flats would exceed the relevant minimum internal space standards set out in Table 3.1 and Policy D6 of the London Plan (2021). The proposed flats would also be compliant with the Technical housing standards - nationally described space standard, in regards to bedroom sizes.

All of the proposed flats would be dual aspect and would therefore afford future occupiers with an adequate level of natural light and outlook. It is acknowledged that the proposed bedrooms serving the ground floor flats would face towards the car parking area serving the proposed development. However, the soft landscaping to the front of these windows would provide defensible space which would help to minimise the impact of the residential car parking area.

In the event of an approval, a condition would be secured to ensure that the side windows and roof lights facing No.9 Sandy Lodge Way are obscure glazed and non-opening up to 1.8 metres in order to prevent any issues with privacy or overlooking. These side windows and roof lights would either serve non-habitable rooms or as a secondary windows to habitable rooms (with the primary windows fitted in the front or rear elevations of the building). It is therefore considered that the overall quality of internal amenity provision for future occupiers would not be unacceptably prejudice by the imposition of this condition.

Having regard to the above, it is considered that the proposed development would provide future occupiers with a high standard of internal living accommodation. The proposal would therefore accord with Policy D6 of the London Plan (2021) and Policy DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), in this respect.

#### EXTERNAL AMENITY PROVISION:

Policy DMHB 18 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that all new residential development and conversions will be required to provide good quality and useable private outdoor amenity space. Amenity space should be provided in accordance with the standards set out in Table 5.3. It also states that balconies should have a depth of not less than 1.5 metres and a width of not less than 2 metres.

Paragraph 5.70 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that the Council is keen to improve the quality of housing in the Borough and therefore communal provision of private outdoor space is generally not supported unless there are strong planning reasons and the proposed scheme is of high quality with clear planning merits.

In this case, the majority of external amenity space is being provided in the form of a communal garden. However, there are material considerations relating to the site which are of importance and should be given significant weight. The previous scheme approved under application number 16948/APP/2018/55 contained a similar site layout where a communal rear garden was provided. It is therefore considered that it would be unreasonable to refuse this current application solely on the basis that external amenity space is being provided in the form a communal garden.

A flatted development of this size and occupancy would need to provide at least 105 square metres of private amenity space in order to comply with amenity space standard

set out in Table 5.3 of the the Hillingdon Local Plan: Part Two - Development Management Policies (2020). With a total external amenity space provision of over 500 square metres, the proposed scheme would far exceed this requirement. The proposed ground floor flats would each benefit from a dedicated patio area measuring approximately 26 square metres. The two proposed upper floor flats would contain a recessed terrace within the roof space measuring between 1.6 to 3.8 square metres (approximately). The remaining external amenity space provision is provided by the communal garden measuring approximately 500 square metres.

Having regard to the above considerations, it is considered that the future occupiers would be afforded with a reasonable standard of external amenity space provision.

#### **7.10 Traffic impact, car/cycle parking, pedestrian safety**

Policy DMT 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals will be required to meet the transport needs of the development and address its transport impacts in a sustainable manner.

Policy DMT 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals must ensure that:

- i) safe and efficient vehicular access to the highway network is provided to the Council's standards;
- ii) they do not contribute to the deterioration of air quality, noise or local amenity or safety of all road users and residents;
- iii) safe, secure and convenient access and facilities for cyclists and pedestrian are satisfactorily accommodated in the design of highway and traffic management schemes;
- iv) impacts on local amenity and congestion are minimised by routing through traffic by the most direct means to the strategic road network, avoiding local distributor and access roads; and
- v) there are suitable mitigation measures to address any traffic impacts in terms of capacity and functions of existing and committed roads, including along roads or through junctions which are at capacity.

Policy DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals must comply with the parking standards outlined in Appendix C Table 1 in order to facilitate sustainable development and address issues relating to congestion and amenity.

Based on the Transport for London's WebCAT planning tool, the application site has a Public Transport Accessibility Level (PTAL) rating of 1b and 2 (Very poor/ Poor). The roads surrounding the site are covered by a Controlled Parking Zone, which operates for one hour of the day (1-2pm) Mondays to Fridays.

The car parking standards set out in Appendix C Table 1 requires between 1-1.5 spaces to be provided on-site for each of the flatted units up to a scale of 2 bedrooms with 2 spaces for 3 bedroom units. This would total a requirement of 5-6 spaces. The submitted drawings shows that six off-street car parking spaces (including 1 disabled compliant space) would be provided to the front of the proposed building, thus according with the Council's Parking Standards. In the event of an approval, a condition would be secured requiring a Parking Allocation Plan to be submitted to the Council for consideration. Also, a condition would be secured requiring 20% of the car parking spaces to be 'active' electric vehicle charging points with all the remaining spaces being designated as 'passive'. In this case, this means that 1 'active' and 5 'passive' spaces would need be provided, in order to comply with the draft New London Plan Intend to Publish (2020).

Similar to the previous schemes, the current proposal seeks to install a new vehicular crossover access to the site. The existing vehicular crossover would be reinstated to a raised kerb, which would allow for the existing resident street parking bay to be re-positioned. The proposed associated works to the resident street parking bay are required by recommended Condition 18 and would be secured by a legal agreement under section 278 of the Highways Act, and has been agreed with the applicant. On that basis, the Council's Highways Officer has raised no objection, on the grounds of highway safety, to the proposed vehicular crossover works.

It is observed that the properties situated on the western side of the road are generally larger detached dwellings with up to 4 car parking spaces to their respective fronts. It is therefore considered that the proposed intensification of use of the front garden for parking provision is not significantly out of keeping with other properties within the area, and that a refusal on this basis would be unjustified.

In light of the above, it is considered that the proposed development is unlikely to significantly increase the demand for street parking spaces or prejudice highway safety over and beyond the scheme which has been approved under application number 16948/APP/2018/55. The proposal would therefore comply with Policies DMT1, DMT2 and DMT6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), in this respect.

#### **BICYCLE PROVISION:**

Policy DMT 5 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires development proposals to ensure that safe, direct and inclusive access for pedestrians and cyclists is provided on the site connecting it to the wider network, including: i) the retention and, where appropriate, enhancement of any existing pedestrian and cycle routes; ii) the provision of a high quality and safe public realm or interface with the public realm, which facilitates convenient and direct access to the site for pedestrian and cyclists; iii) the provision of well signposted, attractive pedestrian and cycle routes separated from vehicular traffic where possible; and iv) the provision of cycle parking and changing facilities in accordance with Appendix C, Table 1 or, in agreement with Council.

There should be a provision of at least 1 secure and accessible bicycle space for each of the 2 bedroom flatted units and 2 bicycle spaces for the 3 bedroom unit (totalling 5 spaces), in order to accord with the minimum standards set out in Appendix C, Table 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). The submitted drawings show that bicycle storage provision would be provided within a secure and accessible compound which would be located within the communal garden area of the proposed development. Whilst there is no objection to the location or number of bicycle parking spaces being provided, full details of the design and dimensions of the bicycle compound would be secured by condition, in the event of an approval.

#### **7.11 Urban design, access and security**

Secured by Design is now covered by Part Q of the Building Regulations.

#### **7.12 Disabled access**

The proposed development would contain a lift which would allow wheelchair users to access the first floor level of the building. If recommended for approval, conditions would be secured requiring the submission of details to demonstrate that the proposed dwellings would comply with Category 2 M4 (2) of the Approved Document M to the Building Regulations (2010) 2015. Subject to such conditions, the proposal would accord with Policy D7 of the London Plan (2021).

The Council's Access Officer has raised no objection to the application, subject to the above conditions.

#### **7.13 Provision of affordable & special needs housing**

Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states that housing provision is expected to include a range of housing to meet the needs of all types of households and the Council will seek to maximise the delivery of affordable housing from all sites over the period of the Local Plan. For sites with a capacity of 10 or more units the Council will seek to ensure that the affordable housing mix reflects housing needs in the borough, particularly the need for larger family units. This is supported by Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

The proposal is for less than 10 residential units and does not meet the threshold in order to require affordable housing provision. As such, the proposal is not contrary to Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) and Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.14 Trees, Landscaping and Ecology**

DMHB14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that:

A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.

B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.

C) Where space for ground level planting is limited, such as high rise buildings, the inclusion of living walls and roofs will be expected where feasible.

The site is located in area covered by Tree Preservation Order number 746. There is a mature protected Oak tree at the rear of the site, in close proximity to the mutual boundary shared with number 16 Grove Road. The protected Oak is a Category B1 and A2 tree. It is readily visible from public domain and is considered to make a significant contribution to the visual amenity of the area.

The previously approved scheme under application number 16948/APP/2018/55 was accompanied by an Arboricultural survey and Impact Assessment, which included protection measures to mitigate any potential harm to the protected Oak tree at the site. This current application is not supported by a tree report and the one that was submitted as part of application number 16948/APP/2018/55 is technically out of date. However, the Council's Trees and Landscape Officer has recommended that an updated Tree Protection Plan and Arboricultural Method Statement to be secured through the imposition of a pre-commencement condition, in the event of an approval.

The Council's Tree and Landscape Officer has requested for the reinstatement of the soft landscape buffer along the front boundary of the site, similar to that previously approved under application number 16948/APP/2018/55. This matter would be resolved by a condition requiring for a landscape scheme to be submitted to the Council for consideration, in the event of a recommendation for approval.

Subject to the recommended conditions, it is considered that the proposed development would respect the verdant character of the area, and would not cause harm to the protected tree at the site. It is therefore considered that the proposal would accord with DMHB14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.15 Sustainable waste management**

The submitted drawings show that refuse and recycling facilities would be provided within the communal garden of the site, adjacent to the side gate on Grove Road. The location of the proposed refuse and recycling facilities would provide convenient access for both the future occupiers of the proposed development, and for the Council's refuse collection vehicles which would service the site from the highway on Grove Road on collection days. Full details of the refuse and recycling store (including its design and dimensions) would be secured by condition, in the event of an approval.

Subject to this condition, it is considered that the proposed development would provide a satisfactory provision of refuse and recycling facilities, in accordance with Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.16 Renewable energy / Sustainability**

Policy DMEI 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires all developments to make the fullest contribution to minimising carbon dioxide emissions in accordance with London Plan targets.

Policy DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that applications for all new build developments (not conversions, change of use, or refurbishment) are required to include a drainage assessment demonstrating that appropriate sustainable drainage systems (SuDS) have been incorporated in accordance with the London Plan Hierarchy (Policy 5.13: Sustainable drainage).

No specific details about the sustainability credential of the proposed development were accompanied with the application. If planning permission were to be granted, a condition would be secured requiring the proposed development to achieve an energy efficiency standard of 19% CO2 improvement over Building Regulations requirements Part L 2013 (TER Baseline).

Also, a condition would be secured requiring the proposed flats to achieve as a minimum, a water efficiency standard of no more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day for in accordance with the option requirement defined within Approved Document G of the Building Regulations).

Subject to the above conditions, the proposal would be compliant with Policies DMEI 2 and DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.17 Flooding or Drainage Issues**

Policy SI 12 of the London Plan (2021) requires flood risk from all sources to be managed in a sustainable and cost effective way.

Policy SI 13 of the London Plan (2021) requires development proposals to utilise sustainable urban drainage systems (SUDS) unless there are practical reasons for not doing so, and should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible.

Policy EM6 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states that applicants must demonstrate that Flood Risk can be suitably mitigated. Policy DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development within areas identified at risk from surface water flooding which fail to make adequate provision for the control and reduction of surface water runoff rates will be refused.

The application site is in Flood Zone 1 where the risk of flooding from rivers or seas is low. As such, all forms of development, including 'more vulnerable' uses, are acceptable in terms of fluvial flood risk in this location.

It should be noted that the basement previously proposed as part of the schemes under application numbers 16948/APP/2018/55 and 16948/APP/2018/3921 has been omitted from this current application.

In the event of an approval, a sustainable water management scheme would be secured by condition in order to ensure compliance with Policy EM6 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012), Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policies SI 12 and SI 13 of the London Plan (2021).

## **7.20 Planning Obligations**

Policy DMCI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states:

A) To ensure development is sustainable, planning permission will only be granted for development that clearly demonstrates there will be sufficient infrastructure of all types to support it. Infrastructure requirements will be predominantly addressed through the Council's Community Infrastructure Levy (CIL).

B) Planning obligations will be sought on a scheme-by-scheme basis:

i) to secure the provision of affordable housing in relation to residential development schemes;

ii) where a development has infrastructure needs that are not addressed through CIL; and

iii) to ensure that development proposals provide or fund improvements to mitigate site specific impacts made necessary by the proposal.

C) Applications that fail to secure an appropriate Planning Obligation to make the proposal acceptable will be refused.

The proposed works to the resident street parking bay are required by recommended Condition 18 and would be secured by a legal agreement under section 278 of the Highways Act, and has been agreed with the applicant.

The scheme would be liable for payments under the Community Infrastructure Levy.

## **8. Observations of the Borough Solicitor**

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

### Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

### Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

### Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

## **9. Observations of the Director of Finance**

Not applicable.

## **10. CONCLUSION**

The principle of introducing a flatted development at the site has already been established through the granting of planning permission under application number 16948/APP/2018/55. It is considered that the proposal has satisfactorily addressed the grounds for refusal attached to planning application number 16948/APP/2018/3921.

The proposal would not have a negative impact upon the visual amenity of the site or the surrounding area, nor would it result in an unacceptable loss of residential amenity to neighbouring occupiers. The proposal would provide an adequate provision of internal and external space to future occupiers. It is considered that the provision of six off-street parking spaces is acceptable in this location, and that the proposed crossover would not

prejudice pedestrian or highway safety. The details relating to tree protection measures, landscaping, cycle and refuse stores, inclusive design (i.e. disabled access), sustainable drainage and the sustainability credentials of the proposed building would be secured by condition, in the event of an approval.

The proposed associated works to the resident street parking bay are required by the recommended conditions and would be secured by a legal agreement under section 278 of the Highways Act, and have been agreed with the applicant.

#### **11. Reference Documents**

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)

Hillingdon Local Plan: Part Two - Development Management Policies (January 2020)

The London Plan (2021)

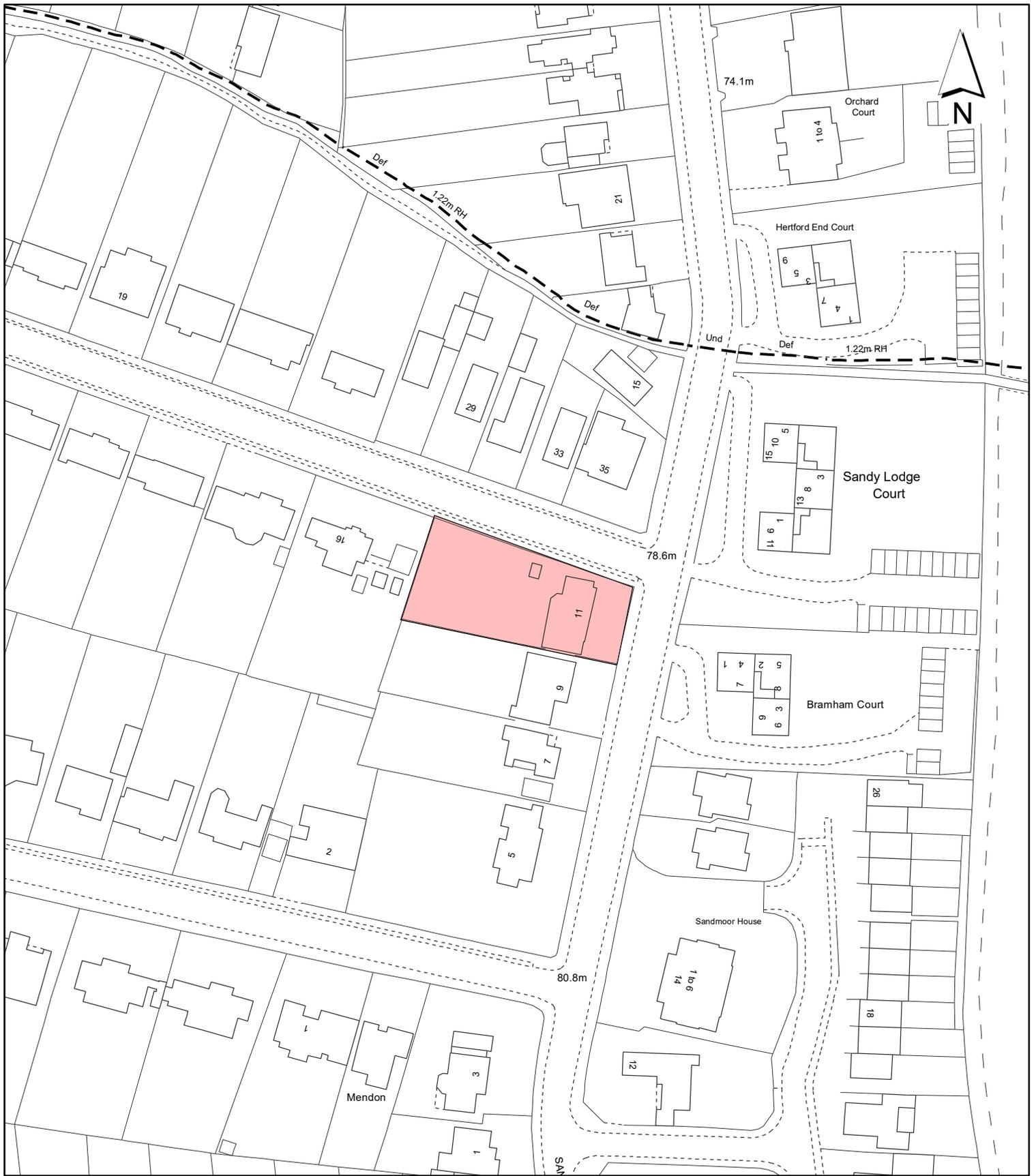
Technical Housing Standards - Nationally Described Space Standard (March 2015)

Hillingdon Local Plan Accessible Hillingdon Supplementary Planning Document (September 2017)

National Planning Policy Framework (February 2019)

**Contact Officer:** Nesha Burnham

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**11 SANDY LODGE WAY  
 NORTHWOOD**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**16948/APP/2020/3623**

Scale:

**1:1,250**

Planning Committee:

**North Page 115**

Date:

**March 2021**



**HILLINGDON**  
 LONDON

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STRICTLY NOT FOR PUBLICATION

Exempt information by virtue of paragraph(s) 1, 2, 7 of Part 1 of Schedule 12A  
of the Local Government Act 1972 (as amended).

# Agenda Item 11

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Exempt information by virtue of paragraph(s) 1, 2, 7 of Part 1 of Schedule 12A  
of the Local Government Act 1972 (as amended).

# Agenda Item 12

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# Plans for North Applications Planning Committee

Thursday 11th March 2021



**HILLINGDON**  
LONDON

[www.hillingdon.gov.uk](http://www.hillingdon.gov.uk)

## **Report of the Head of Planning, Transportation and Regeneration**

**Address** 50 NEWNHAM AVENUE RUISLIP MIDDLESEX

**Development:** Single storey side/rear extension, canopy structure above existing side entrance door and external alterations to the property, following demolition of existing outbuilding (Revised description)

**LBH Ref Nos:** 70191/APP/2021/174

**Drawing Nos:**

# 50 Newnham Avenue, HA4 9RN



Plan Produced for: Resi  
Date Produced: 16 Nov 2020  
Plan Reference Number: TORQM20321123140249  
Scale: 1:1250 @ A4

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**NOTES**



**Revision**

Rev	Notes	Date
A	Planning Issue	05.01.2021
B	Planning Amendment	19.02.2021

**KEY**

Neighboring context Boundary line

**JOB TITLE**

Proposed ground floor side / rear extension, front porch door removal, outbuilding demolition, floor plan redesign and all associated works at 50 Newnham Avenue

**STATUS**

Planning

**DRAWING TITLE**

Block Plans

**CLIENT**

Matt Ware

**SCALE**

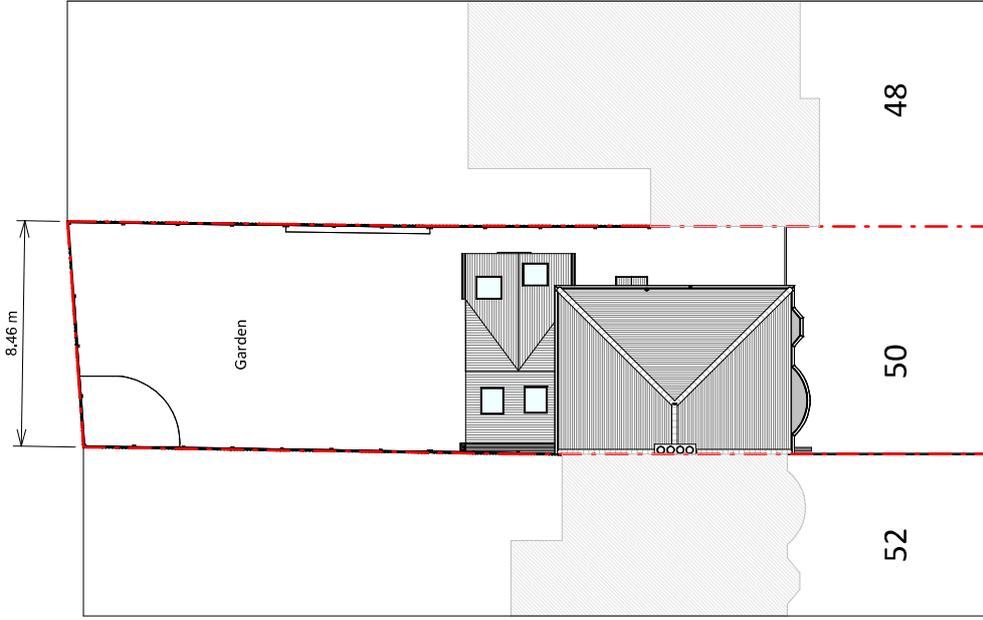
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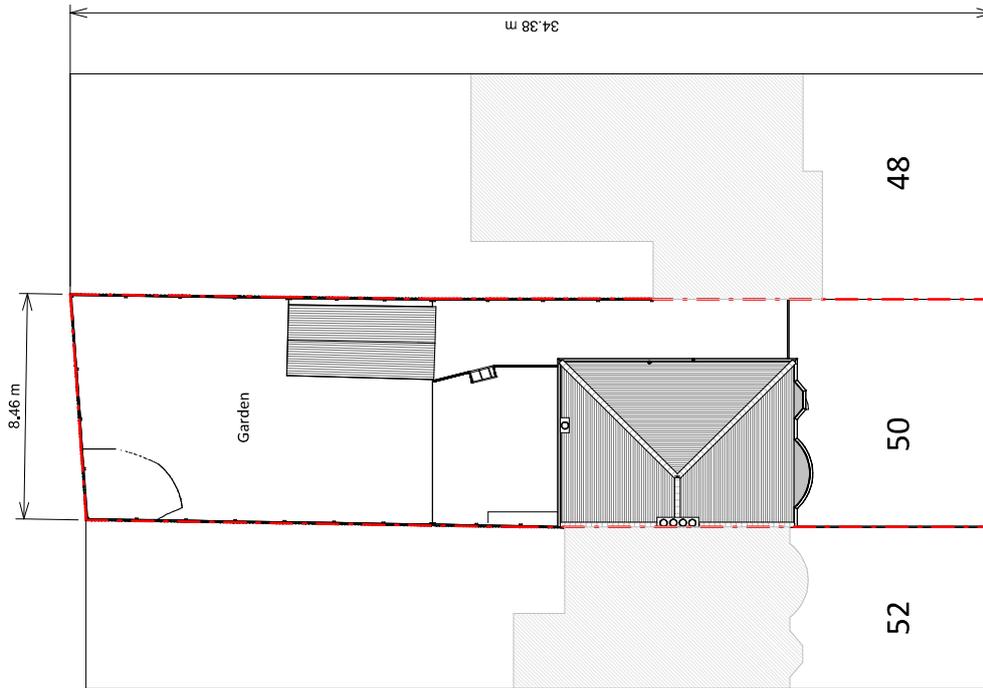
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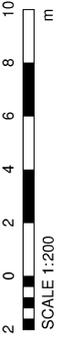
69112 - 1500 B



Proposed Block Plan



Existing Block Plan



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**NOTES**



**Revision**

Rev	Notes	Date
A	Planning Issue	05.01.2021

**KEY**

- Proposed walls
- Existing walls
- Existing removed
- Proposed rooflight
- Boundary line
- Proposed beam
- Proposed drainage
- 1.8m Head height
- 1.5m Head height

**JOB TITLE**

Proposed ground floor side / rear extension, front porch door removal, outbuilding demolition, floor plan redesign and all associated works at 50 Newnham Avenue

**STATUS**

Planning

**DRAWING TITLE**

Existing Floor Plans

**CLIENT**

Matt Ware

**SCALE**

1 : 100 at A3

**DATE**

Jan 2021

**DRAWN**

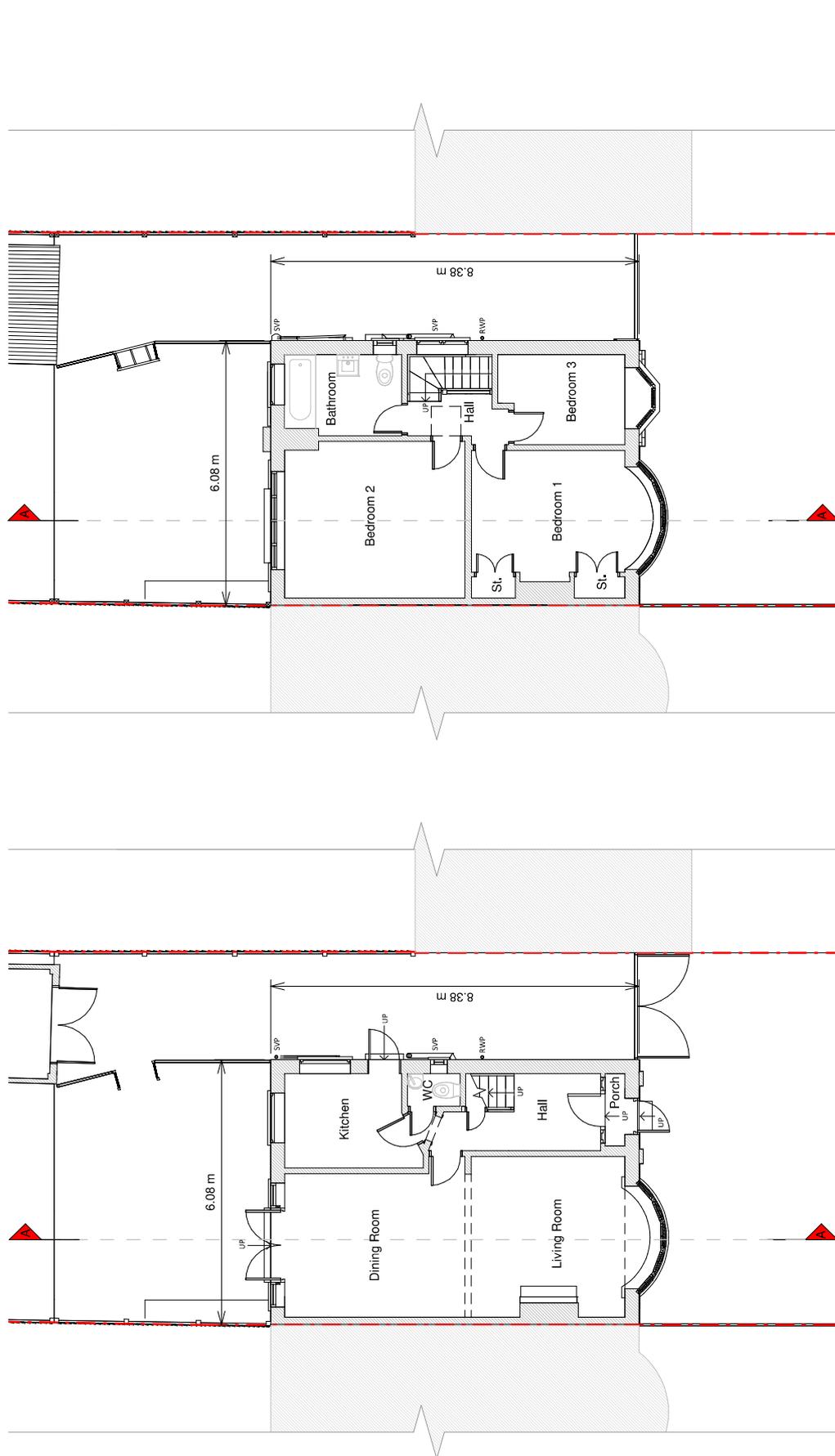
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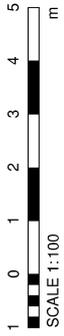
**DRAWING NO.**

69112 - 1100 A



First Floor Plan

Ground Floor Plan





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**NOTES**

- Exterior Materials:**
- Brickwork / Walls - White painted pebbledash and red bricks
  - Pitched roof - Clay tiles
  - Windows - White uPVC framed windows
  - Doors - White uPVC framed doors and double doors
  - RWP / Gutter / Fascia - Black uPVC / Black painted cast iron downpipes, guttering, and white painted timber fascias

Revision		
Rev	Notes	Date
A	Planning Issue	05.01.2021

**KEY**

- Boundary line
- - - Level line
- ▭ Neighboring context

**JOB TITLE**

Proposed ground floor side / rear extension, front porch door removal, outbuilding demolition, floor plan redesign and all associated works at 50 Newnham Avenue

**STATUS**

Planning

**DRAWING TITLE**

Existing Elevations

**CLIENT**

Matt Ware

**SCALE**

1 : 100 at A3

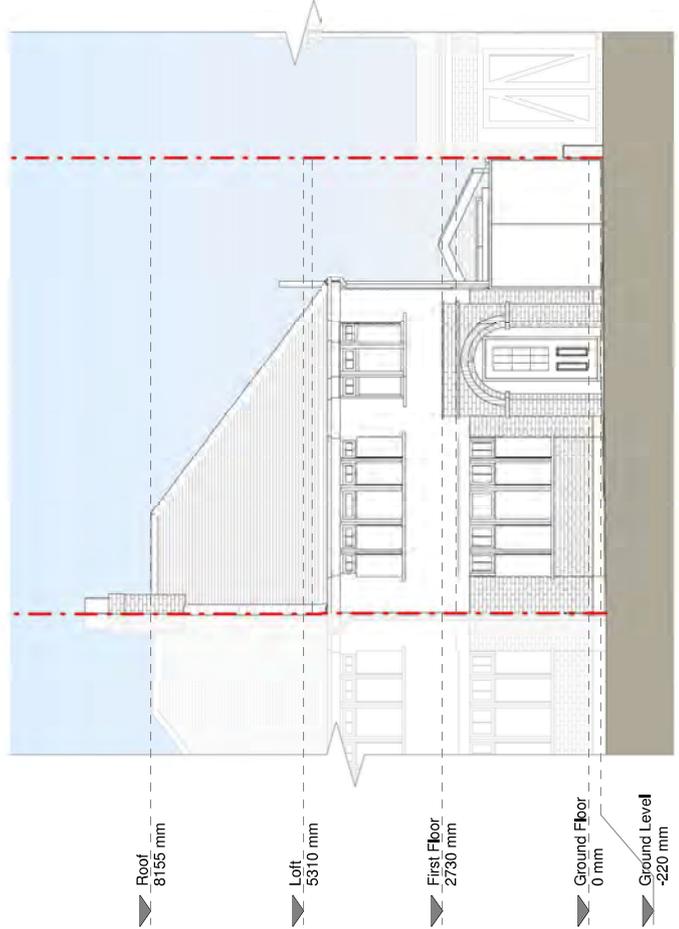
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Jan 2021

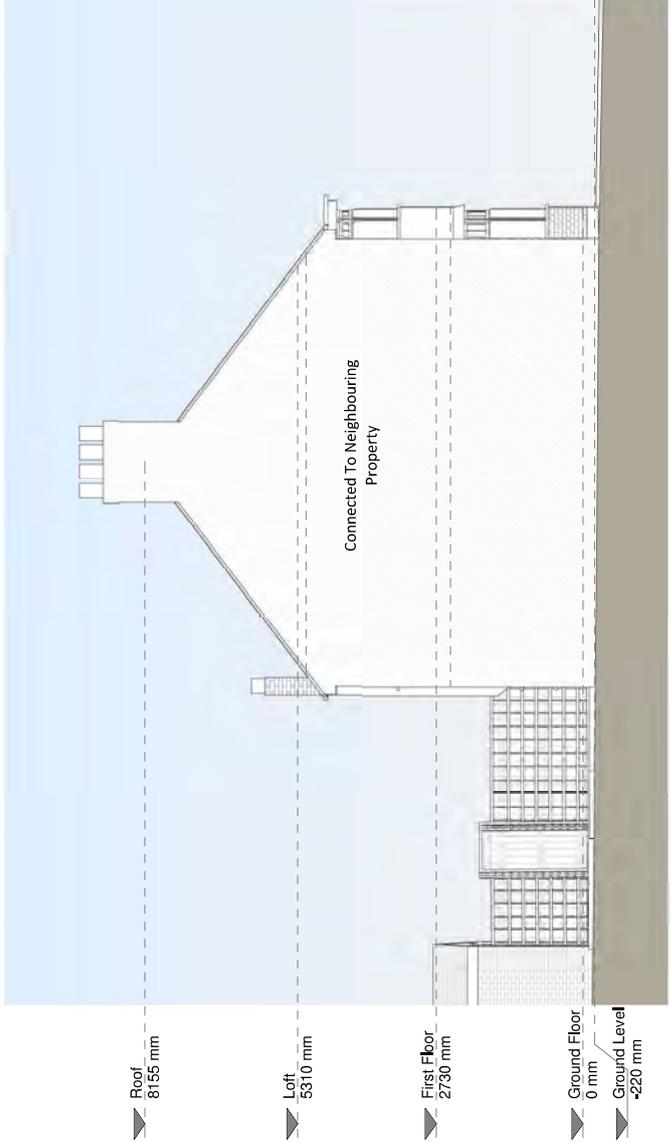
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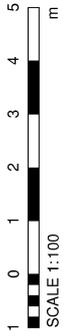
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Front Elevation



Left Side Elevation



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**NOTES**

- Existing Materials:**  
Brickwork / Walls - White painted pebbledash and red brick.  
Pitched roof - Clay tiles  
Windows - White uPVC framed windows  
Doors - White uPVC framed doors and double doors  
RWP / Gutter / Fascia - Black uPVC / black painted cast iron downpipes, guttering, and white painted timber fascias

**Revision**

Rev	Notes	Date
A	Planning Issue	05.01.2021

**KEY**

- Boundary line
- - - Level line
- ▭ Neighboring context

**JOB TITLE**

Proposed ground floor side / rear extension, front porch door removal, outbuilding demolition, floor plan redesign and all associated works at 50 Newnham Avenue

**STATUS**

Planning

**DRAWING TITLE**

Existing Elevations

**CLIENT**

Matt Ware

**SCALE**

1 : 100 at A3

**DATE**

Jan 2021

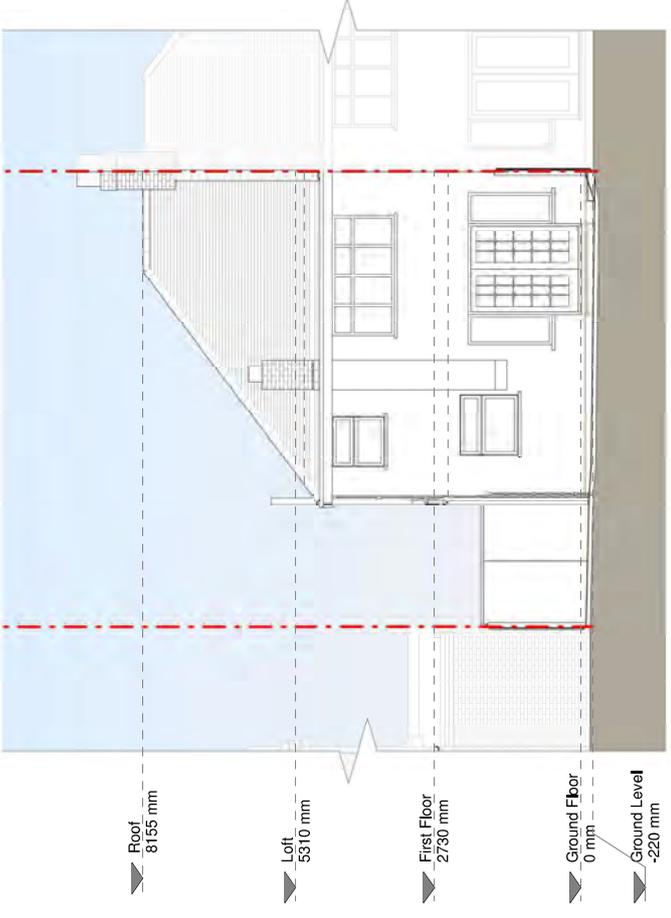
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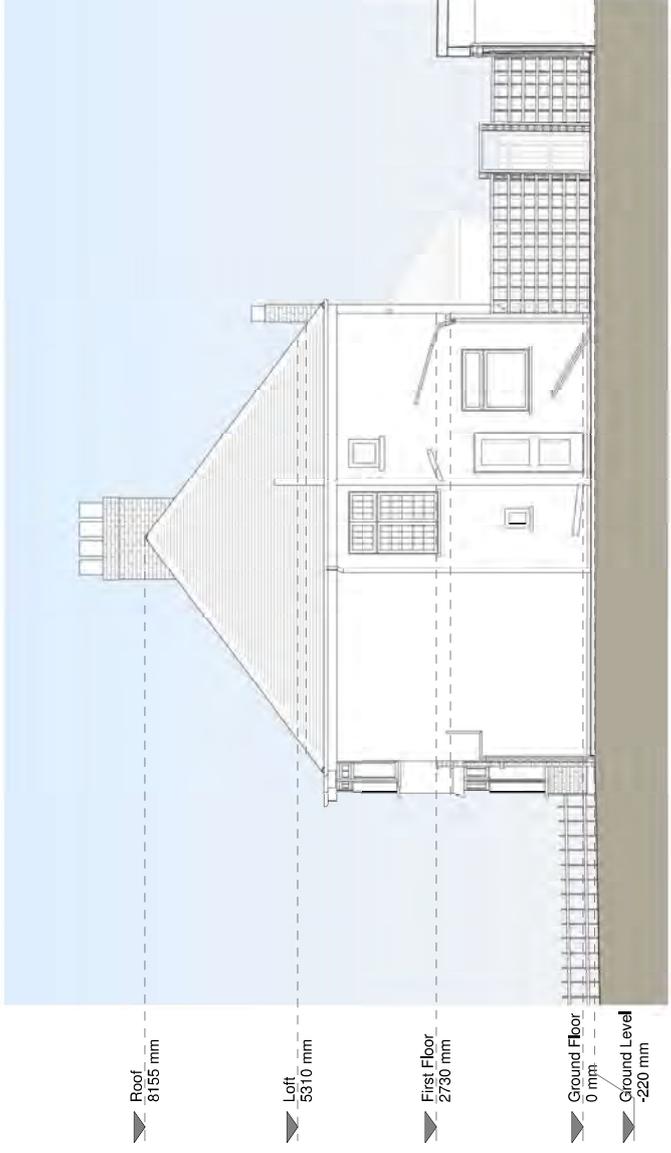
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DH

SCALE 1:100



Rear Elevation



Right Side Elevation



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**NOTES**



**Revision**

Rev	Notes	Date
A	Planning Issue	05.01.2021
B	Planning Amendments	17.02.2021

**KEY**

- Proposed walls
- Boundary line
- Existing walls
- Proposed beam
- Existing removed
- Proposed drainage
- Existing rooflight
- Proposed rooflight
- 1.8m Head height
- 1.5m Head height

**JOB TITLE**

Proposed ground floor side / rear extension, front porch door removal, outbuilding demolition, floor plan redesign and all associated works at 50 Newnham Avenue

**STATUS**

Planning

**DRAWING TITLE**

Proposed Floor Plans

**CLIENT**

Matt Ware

**SCALE**

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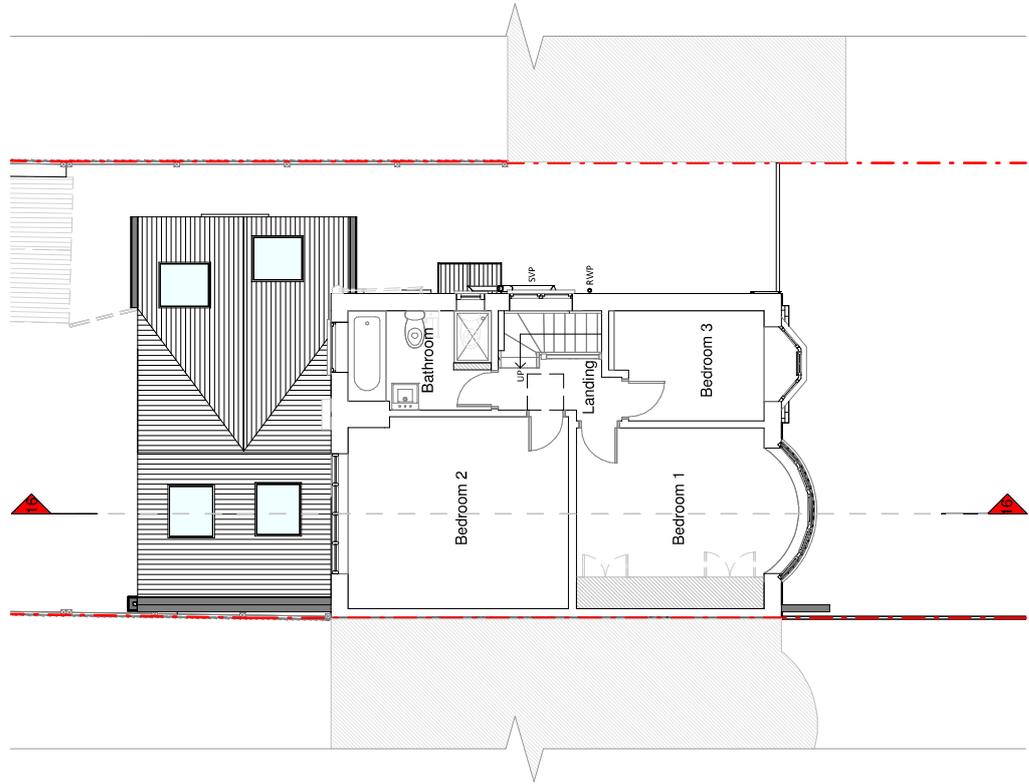
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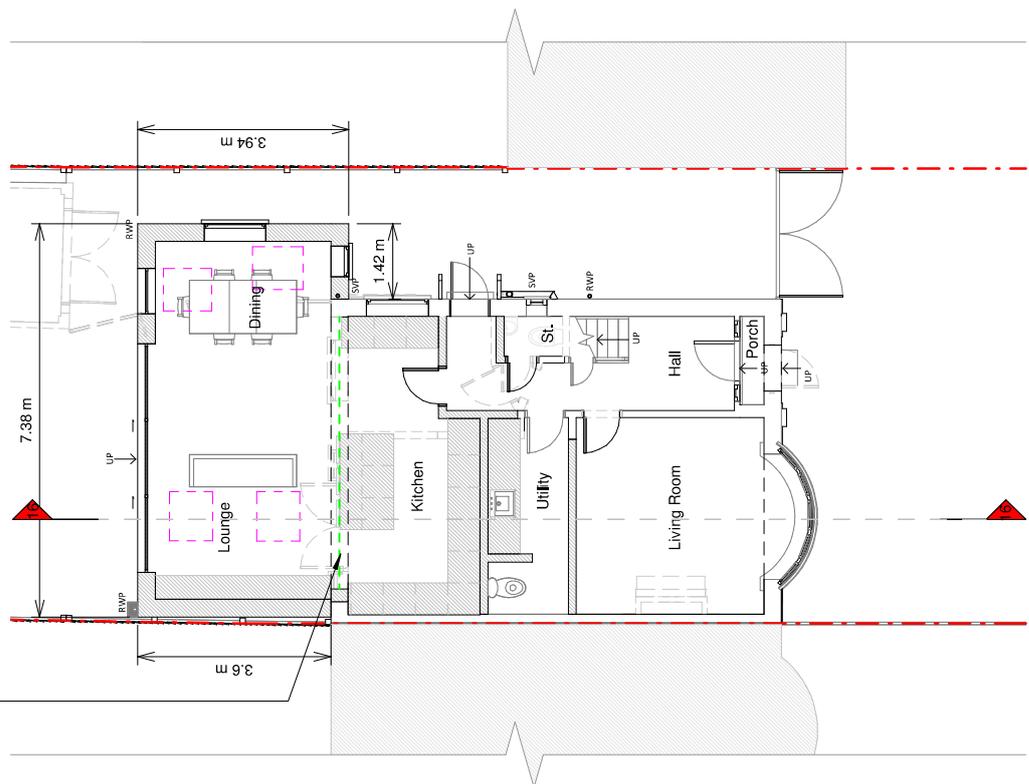
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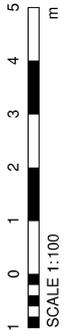


First Floor Plan



Structural Engineer to advise on structural elements

Ground Floor Plan







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**NOTES**

Proposed Materials:

- Brickwork / Walls - White painted render
- Pitched roof - Slate / clay tiles
- Windows - Aluminium framed / UPVC framed windows and Velux rooflights
- Doors - Aluminium framed sliding doors
- Roof / Gutter / Fascia - Aluminium box gutter, hopper and downpipe



**Revision**

Rev	Notes	Date
A	Planning Issue	05.01.2021
B	Planning Amendments	17.02.2021

**KEY**

- Boundary line
- Level line
- Neighboring context

**JOB TITLE**

Proposed ground floor side / rear extension, front porch door removal, outbuilding demolition, floor plan redesign and all associated works at 50 Newnham Avenue

**STATUS**

Planning

**DRAWING TITLE**

Proposed Elevations

**CLIENT**

Matt Ware

**SCALE**

1 : 100 at A3

**DATE**

Jan 2021

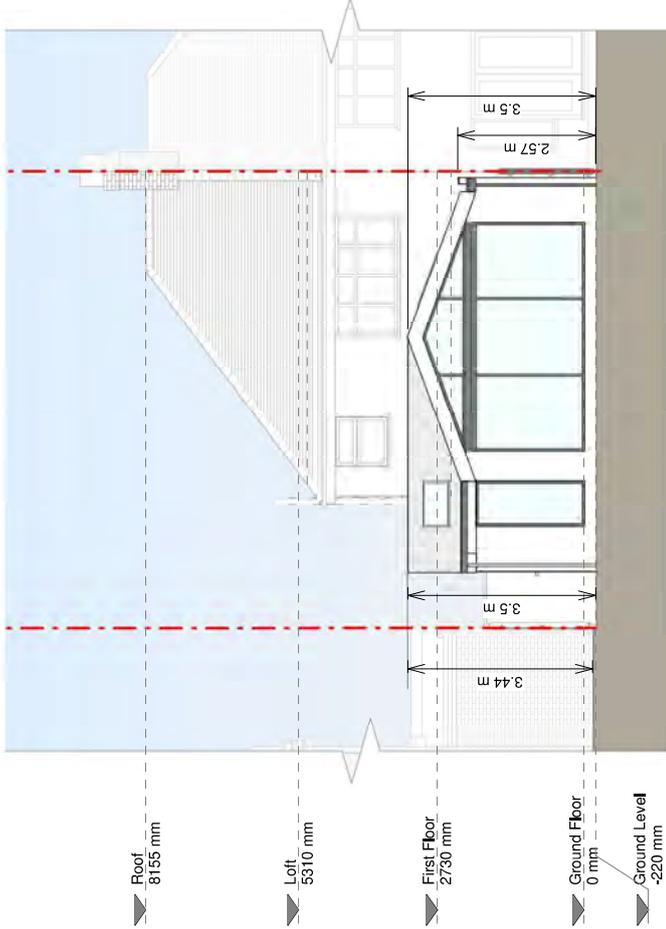
**DRAWN**

TM

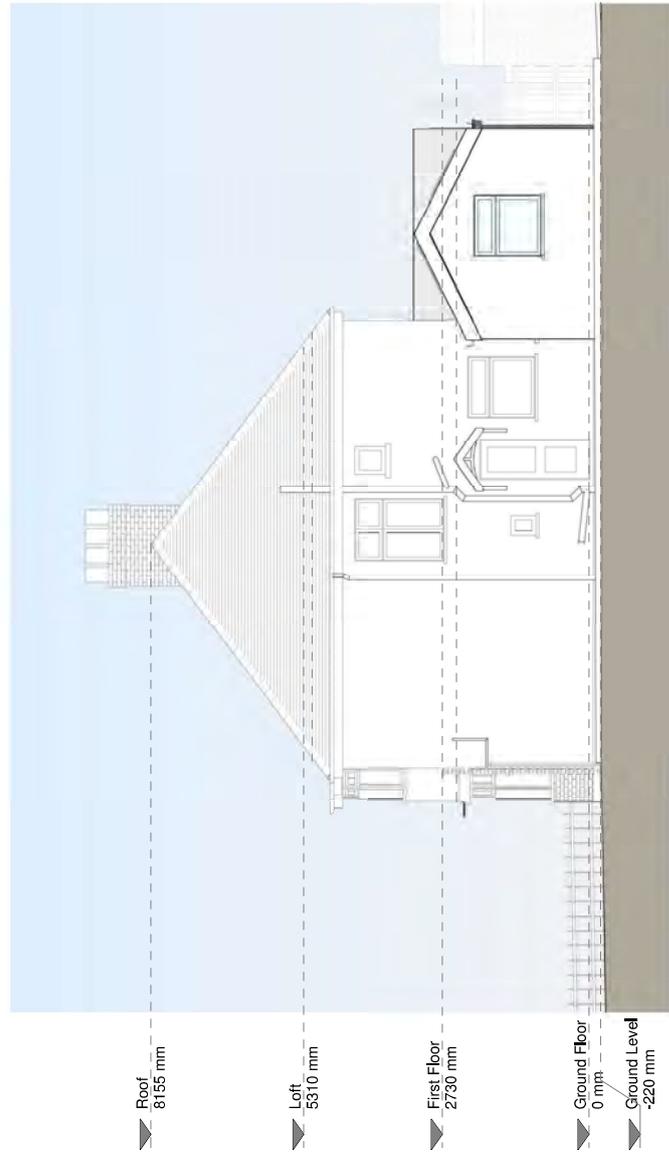
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OC

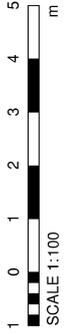
DRAWING NO. 691142 - 3201 B



Rear Elevation



Right Side Elevation



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Not to be used for construction. Do not scale from this drawing. All dimensions are in millimetres. All dimensions are to be cross-checked on-site prior to manufacture and installation. Any discrepancies to be reported to RESI immediately.

**NOTES**



**Revision**

Rev	Notes	Date
A	Planning Issue	05.01.2021
B	Planning Amendment	19.02.2021

**KEY**

- Proposed walls
- Existing walls
- Existing removed
- Proposed rooflight
- Boundary line
- Proposed beam
- Proposed drainage
- 1.8m Head Height
- 1.5m Head Height

**JOB TITLE**

Proposed ground floor side / rear extension, front porch door removal, outbuilding demolition, floor plan redesign and all associated works at 50 Newnham Avenue

**STATUS**

Planning

**DRAWING TITLE**

Proposed Section A - A

**CLIENT**

Matt Ware

**SCALE**

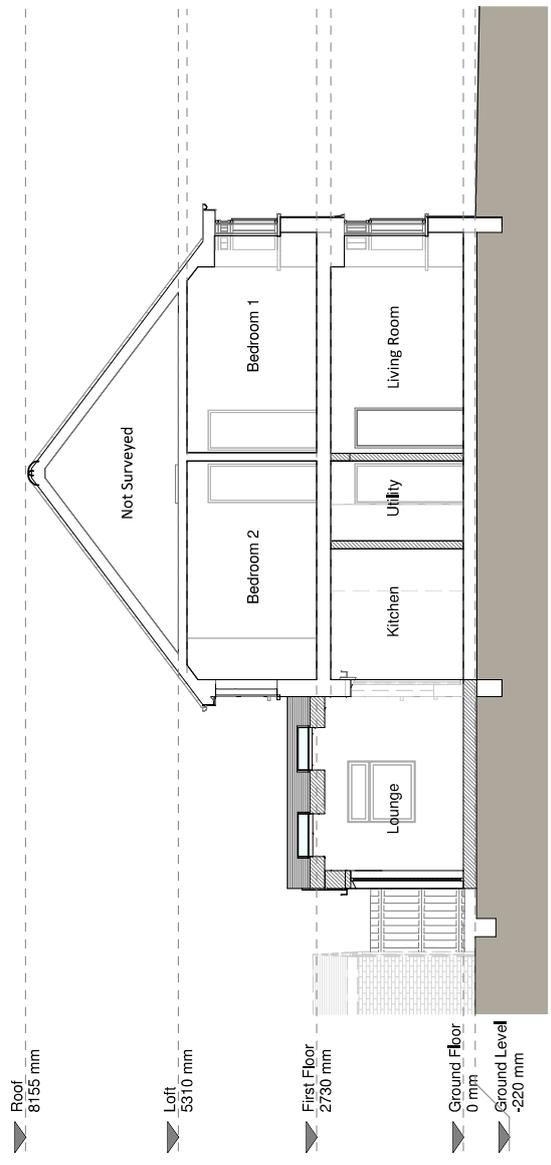
1 : 100 at A3

**DATE**

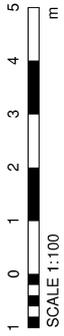
Jan 2021

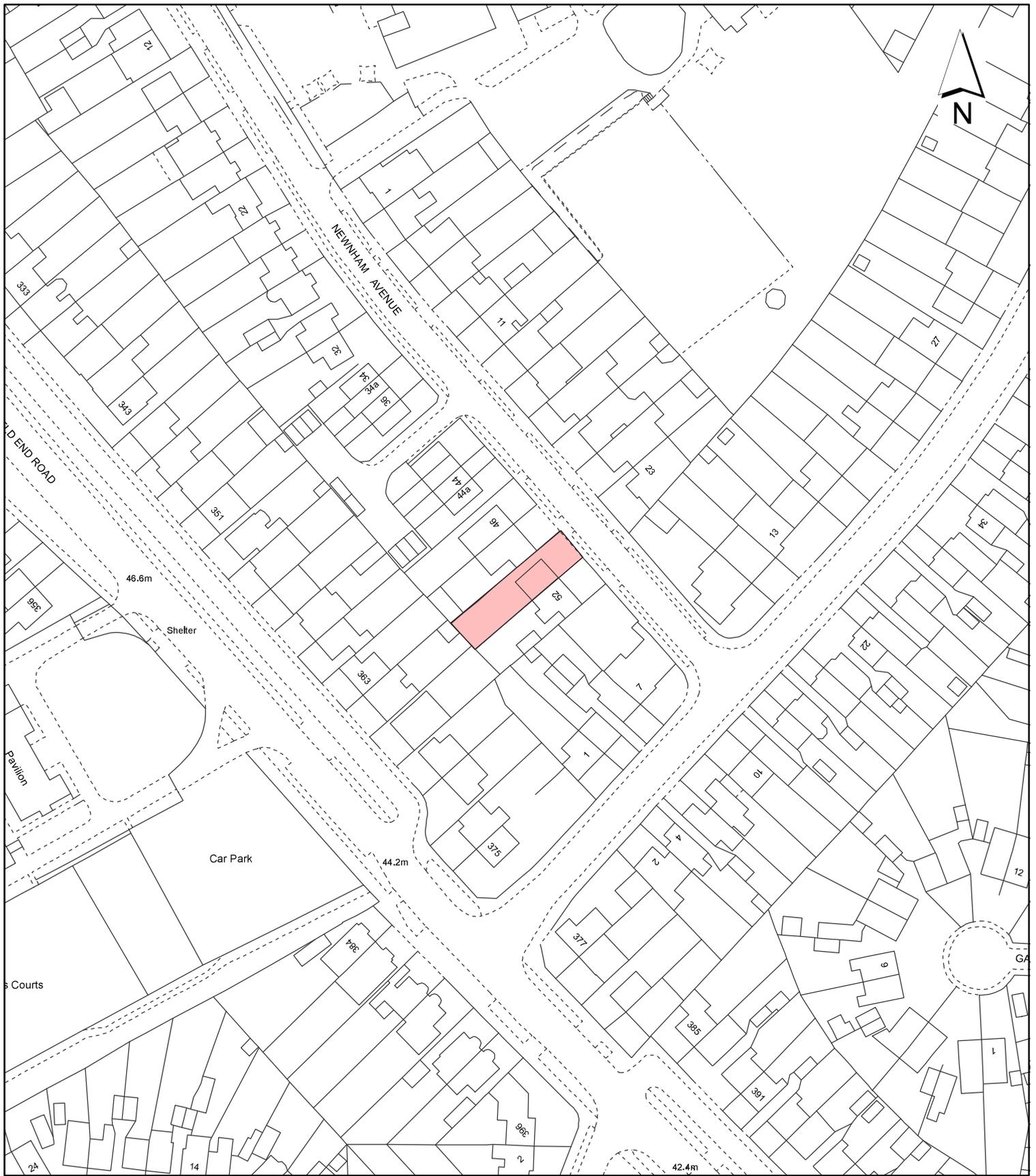
**DRAWING NO.**

69112 - 3300 B



Section A - A





**Notes:**

 Site boundary

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Site Address:

**50 Newnham Avenue**

**LONDON BOROUGH OF HILLINGDON**  
**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**70191/APP/2021/174**

Scale:  
**1:1,250**

Planning Committee:  
**North Page 151**

Date:  
**March 2021**



**Report of the Head of Planning, Transportation and Regeneration**

**Address** 169 JOEL STREET EASTCOTE PINNER

**Development:** Demolition of the existing dwelling and erection of a part one, part two and part three-storey building comprising 9 flats (3 x 1-bed, 5 x 2-bed and 1 x 3-bed) with 4 parking spaces.

**LBH Ref Nos:** 22642/APP/2020/3181

**Drawing Nos:**

Site area: 0.11 Ha



169 Joel Street, Pinner  
LONDON HA5 2PD

LOCATION PLAN

Scale 1:1250 at A3

Drawing No. JSP / P20 / 01





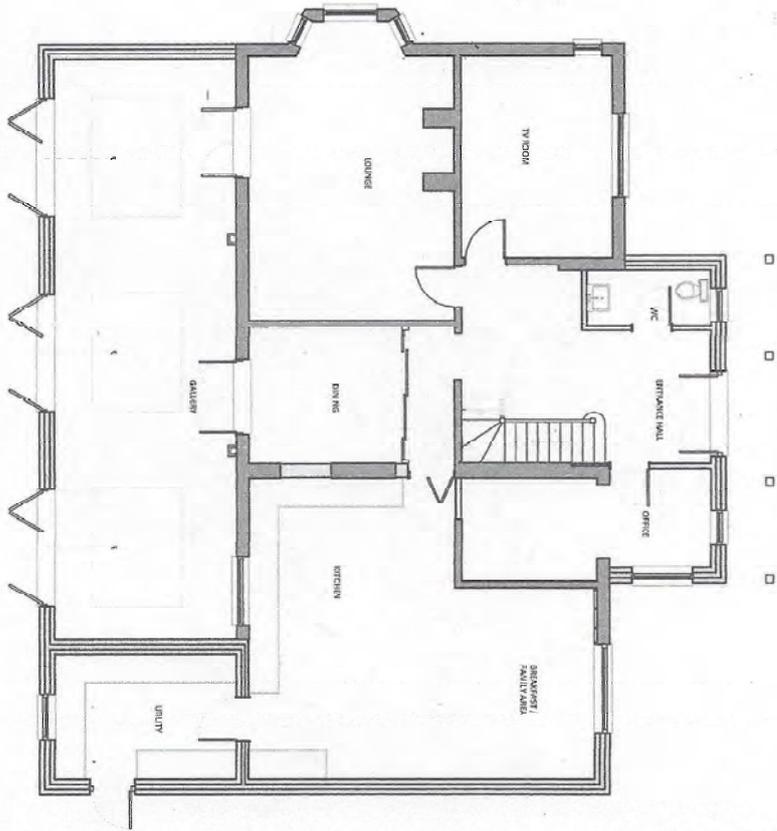
169 Joel Street, Pinner  
LONDON HA5 2PD

EXISTING BLOCK PLAN

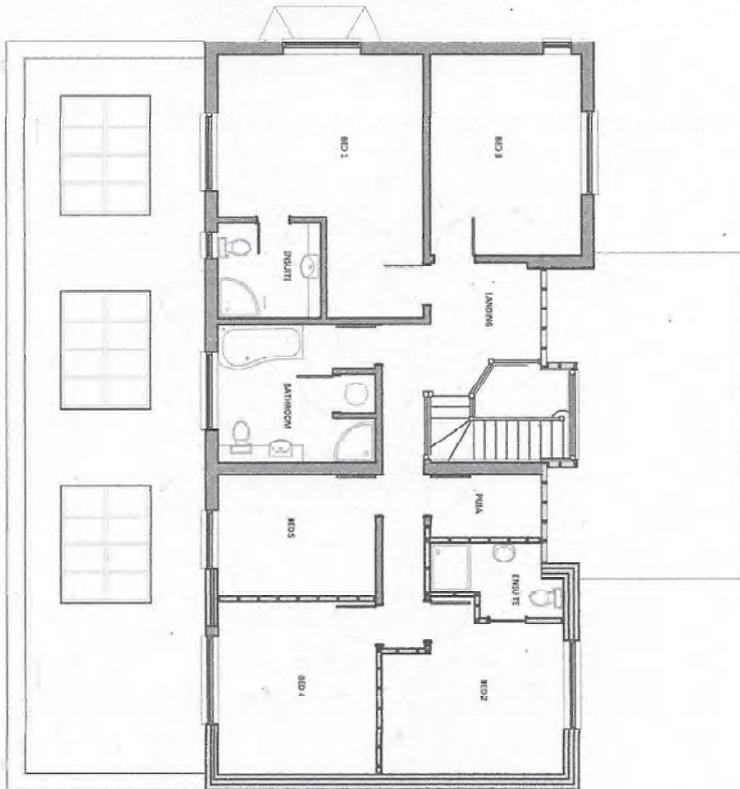
Scale 1:500 at A3

Drawing No. JSP / P20 / 03





FIRST



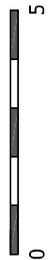
GROUND

169 Joel Street, Pinner  
LONDON HA5 2PD

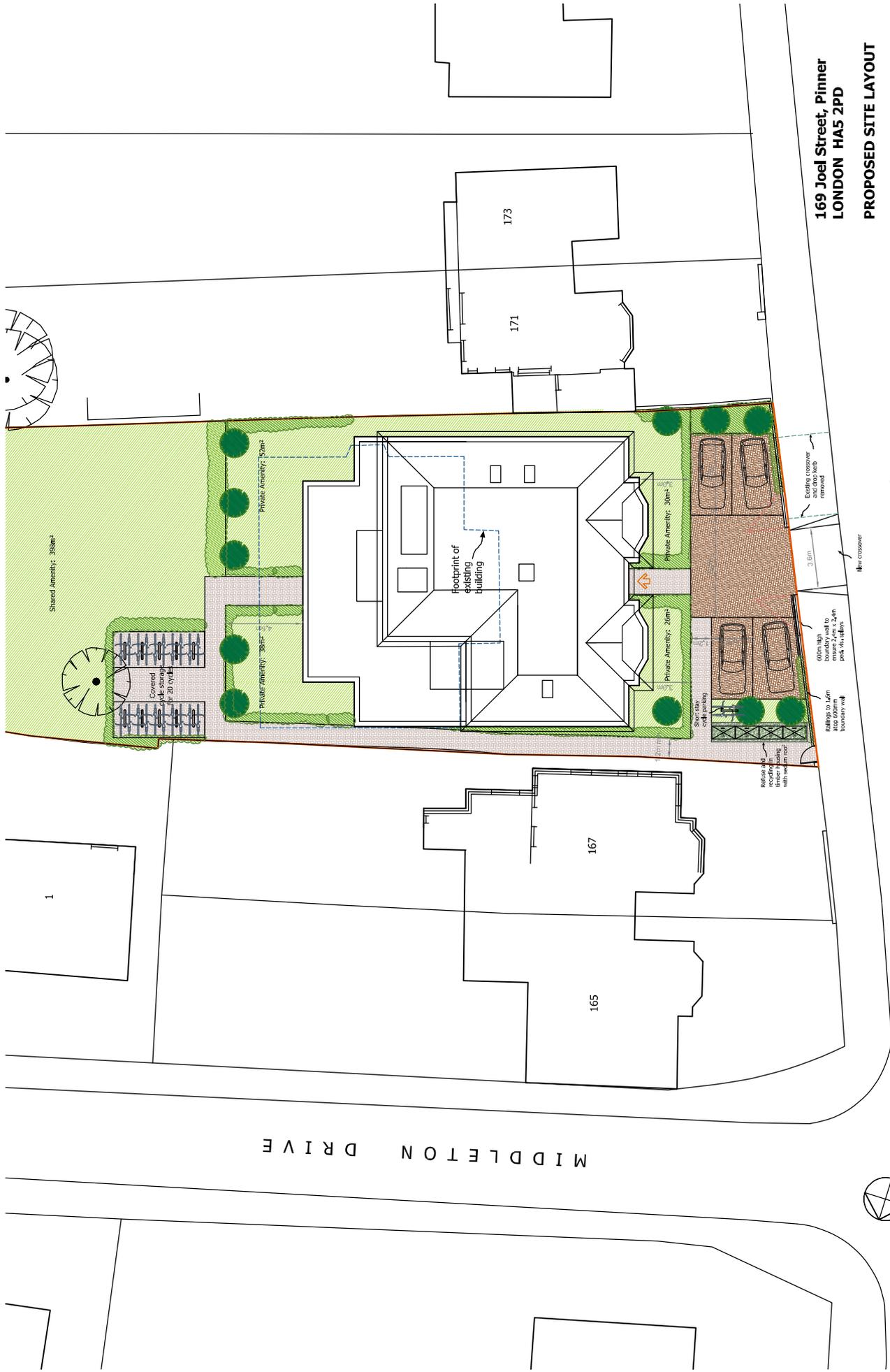
EXISTING FLOOR PLANS

Scale 1:100 at A3

Drawing No. JSP / P20 / 20







**169 Joel Street, Pinner  
LONDON HA5 2PD**

**PROPOSED SITE LAYOUT**

Scale 1:200 at A3

Drawing No. JSP / P20 / 05

JOEL STREET

MIDDLETON DRIVE

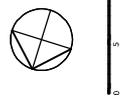


169 Joel Street, Pinner  
LONDON HA5 2PD

**PROPOSED BLOCK PLAN**

Scale 1:500 at A3

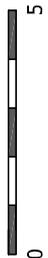
Drawing No. JSP / P20 / 04

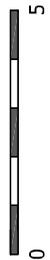
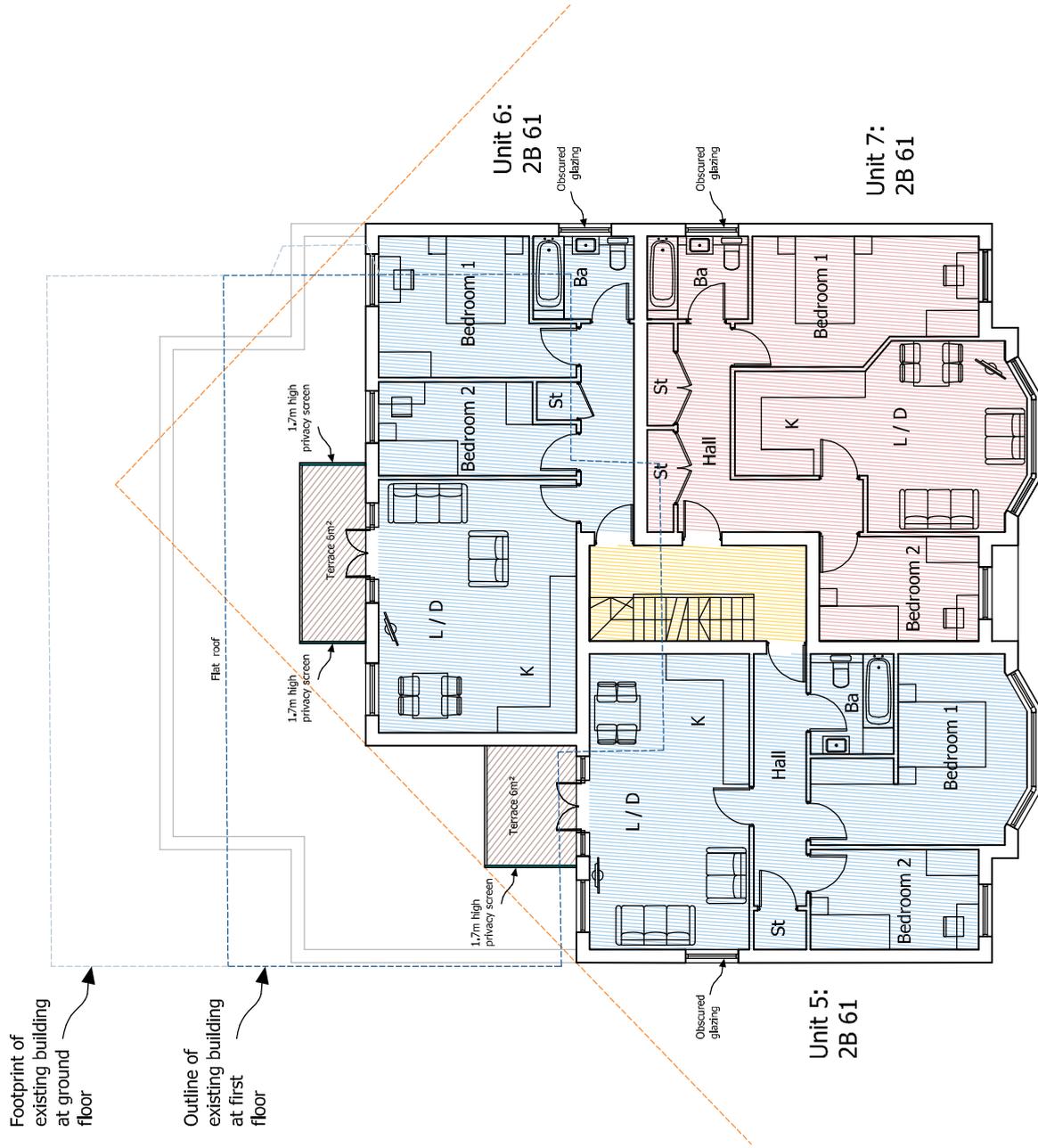


169 Joel Street, Pinner  
LONDON HA5 2PD

PROPOSED GROUND  
FLOOR PLAN  
Scale 1:100 at A3

Drawing No. JSP / P20 / 06





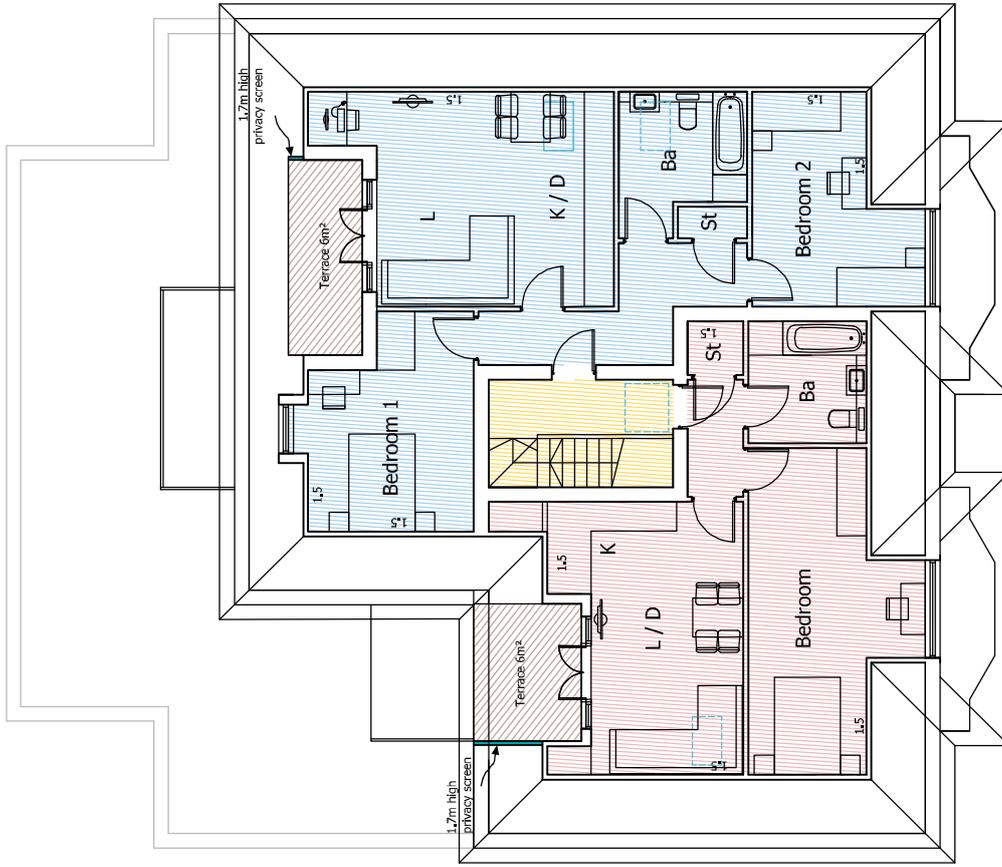
169 Joel Street, Pinner  
LONDON HA5 2PD

PROPOSED SECOND  
FLOOR PLAN

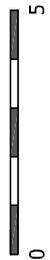
Scale 1:100 at A3

Drawing No. JSP / P20 / 08

Unit 9:  
2B 67



Unit 8:  
1B 50

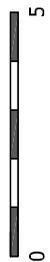
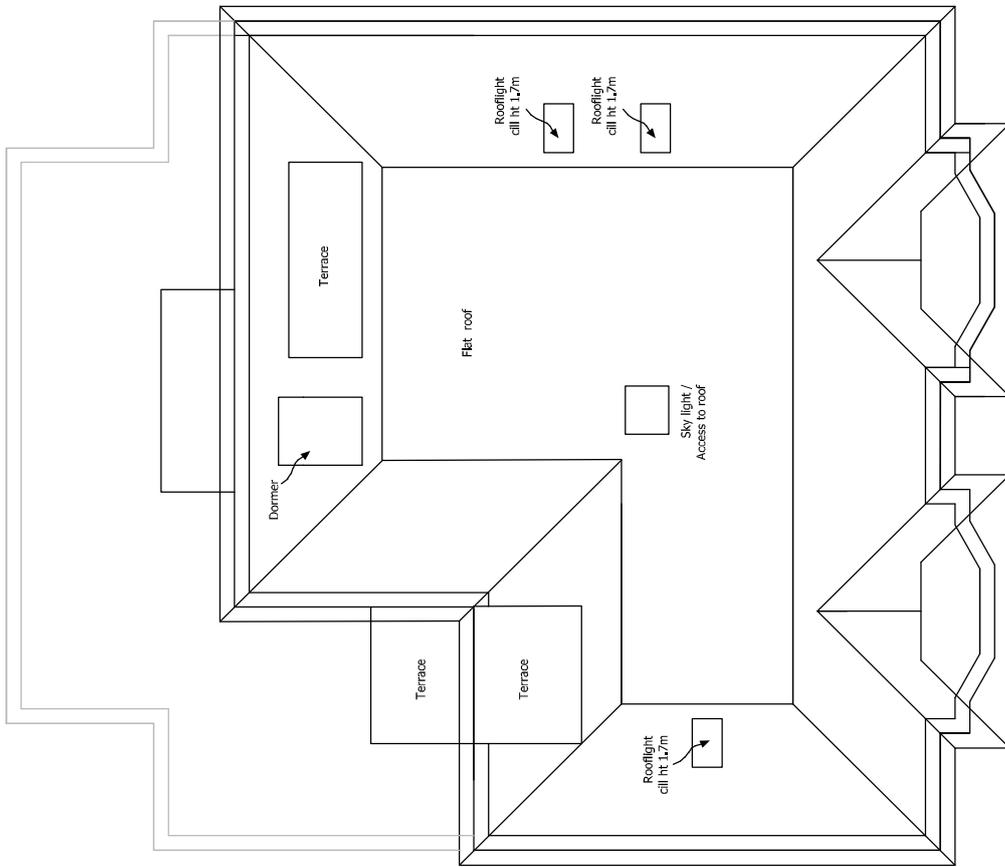


169 Joel Street, Pinner  
LONDON HA5 2PD

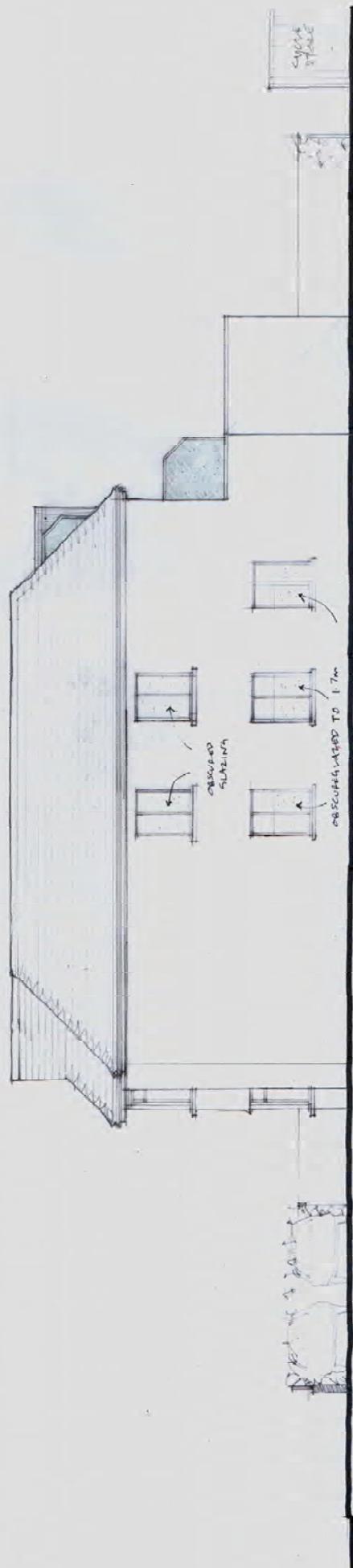
**PROPOSED ROOF PLAN**

Scale 1:100 at A3

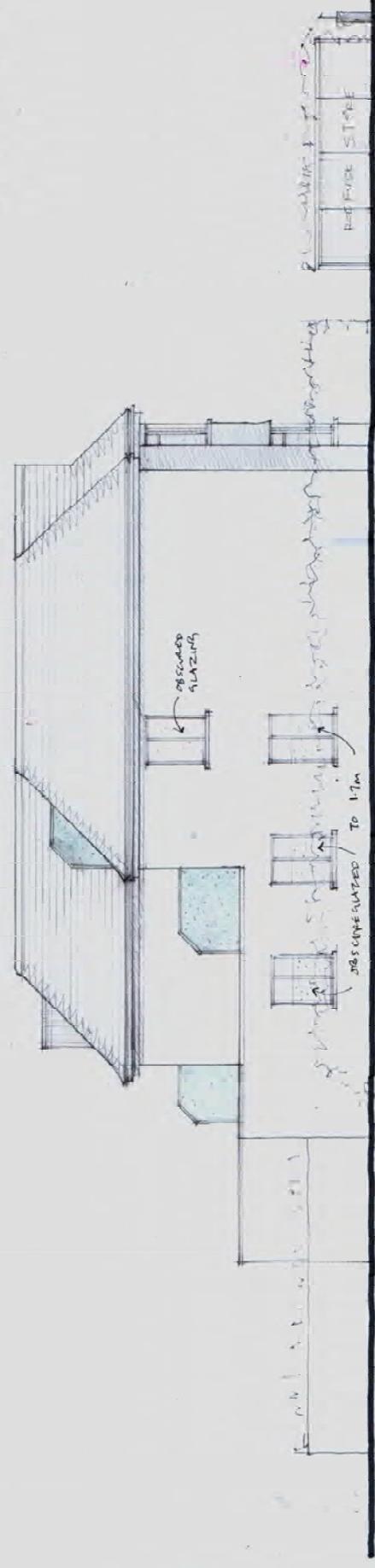
Drawing No. JSP / P20 / 09



169 Joel Street, Plinner  
 LONDON HA5 2PD  
**PROPOSED SIDE ELEVATIONS**  
 Scale 1:100 at A3  
 Drawing No. JSP / P20 / 12

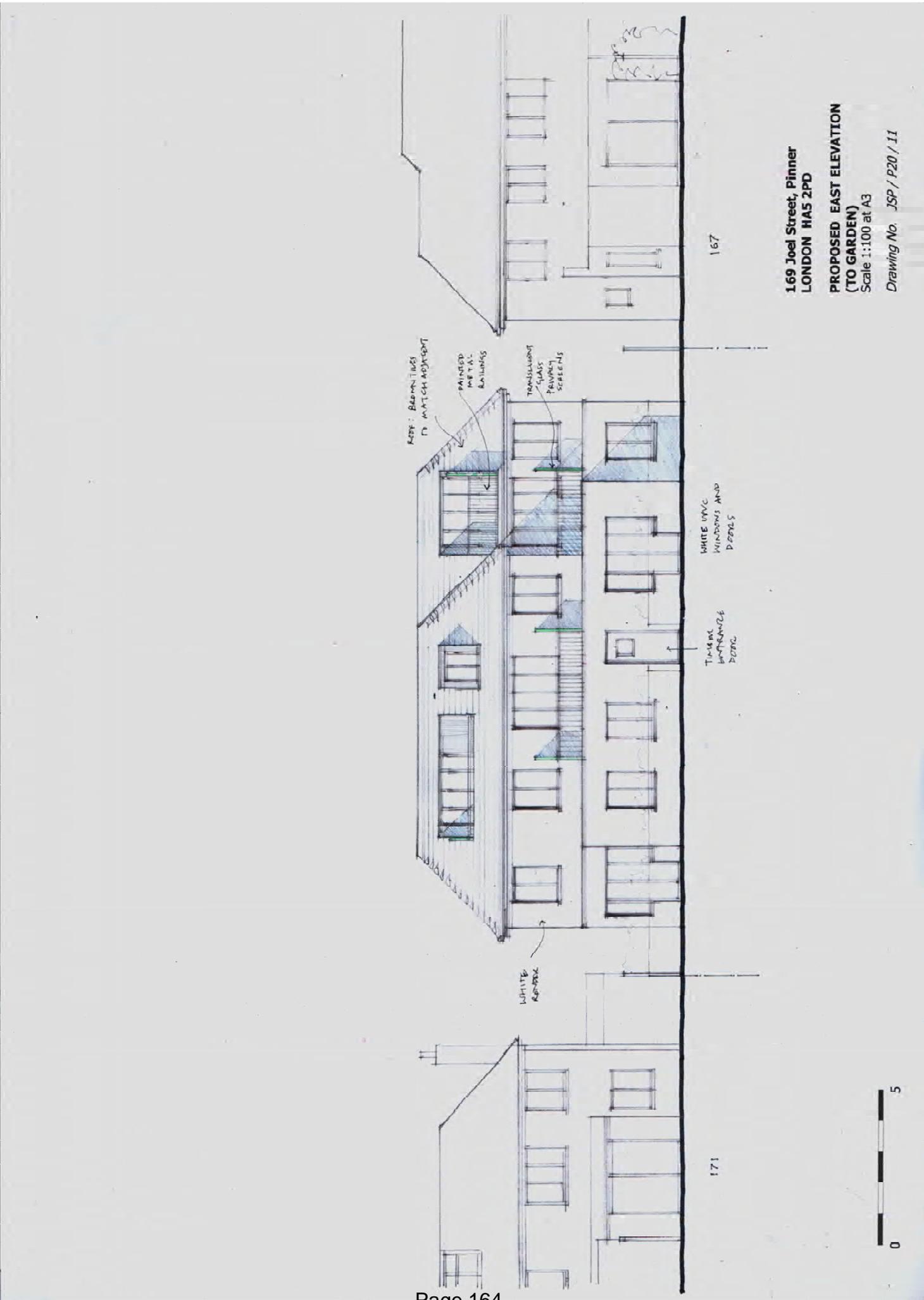


S O U T H



N O R T H





169 Joel Street, Pinner  
LONDON HA5 2PD

PROPOSED EAST ELEVATION  
(TO GARDEN)  
Scale 1:100 at A3

Drawing No. JSP / P20 / 11

167

REF: BEHIND THIS IS MATCH ADJACENT

PAINTED METAL BALUNES

TRANSPARENT GLASS PRIVACY SCREENS

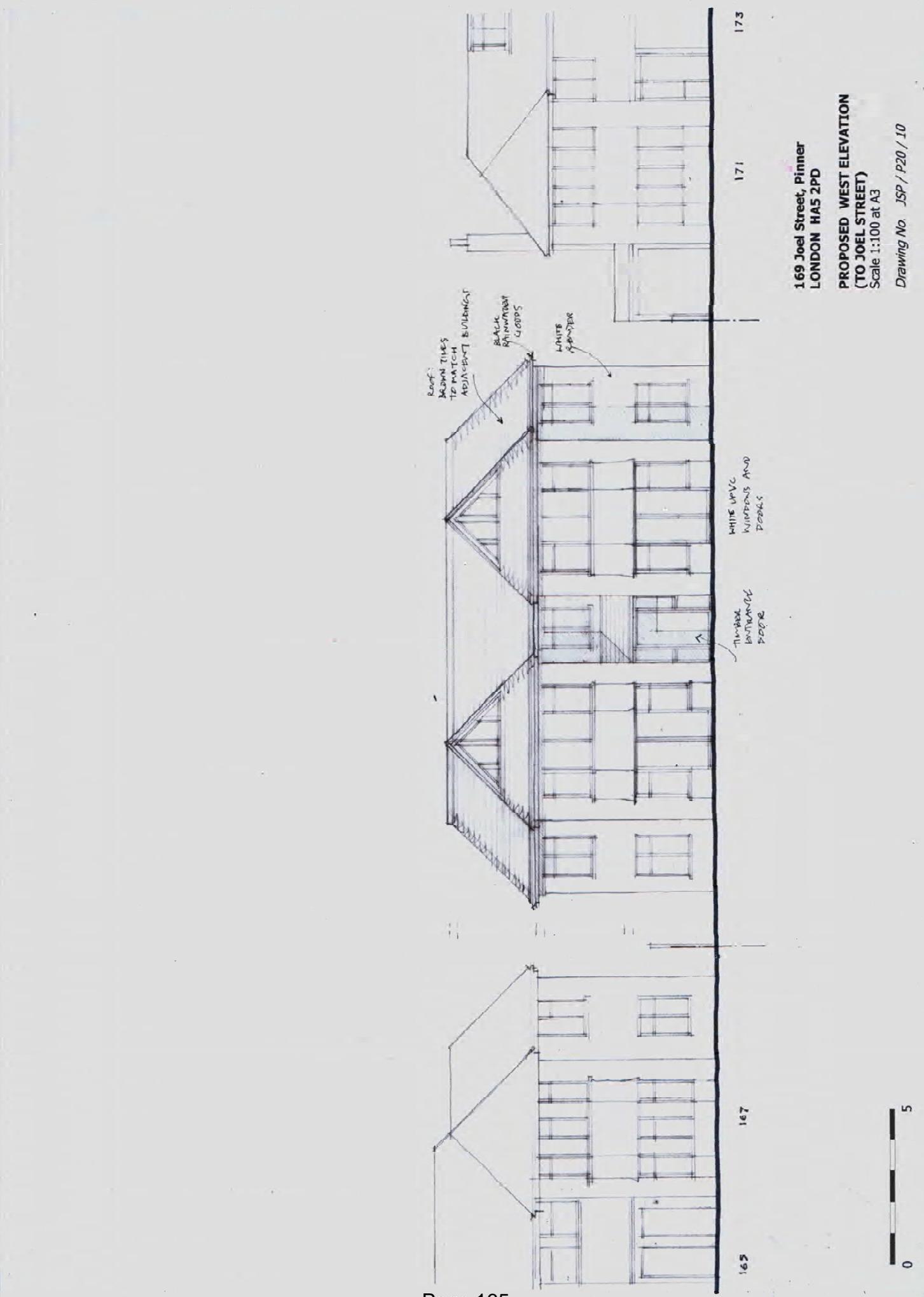
WHITE UPVC WINDOWS AND DOORS

TIMBER ENTRANCE DOOR

WHITE RENDER

171





169 Joel Street, Pinner  
LONDON HA5 2PD  
**PROPOSED WEST ELEVATION  
(TO JOEL STREET)**  
Scale 1:100 at A3  
Drawing No. JSP / P20 / 10



169 Joel Street, Pinner  
LONDON HA5 2PD

PROPOSED STREETSCENE ELEVATION  
(TO JOEL STREET)  
Scale 1:200 at A3

Drawing No. JSP/P20/13

163

165

167

APPLICATION SITE

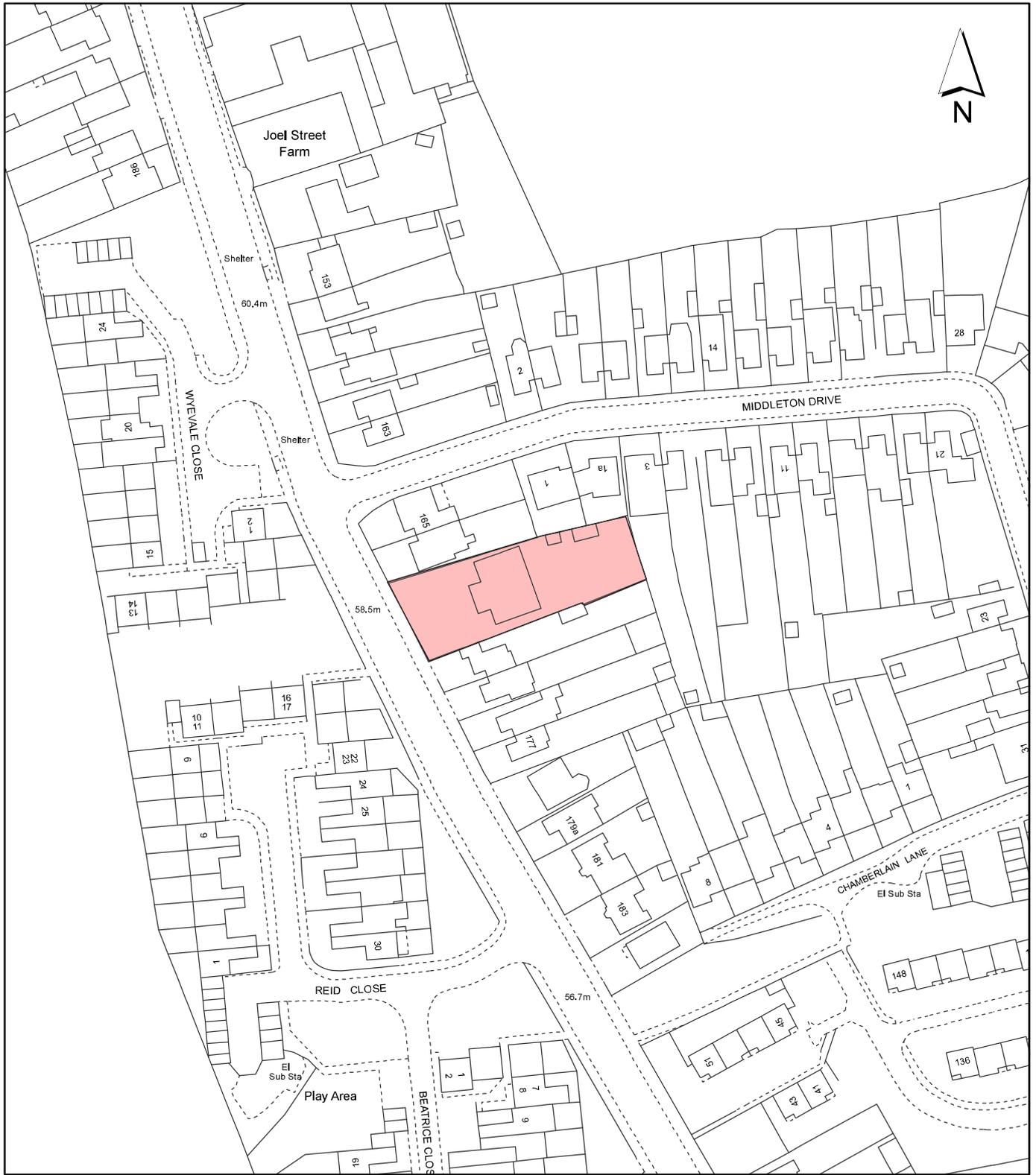
171

173

175







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 Site boundary

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Site Address: **169 JOEL STREET  
 EASTCOTE  
 PINNER**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**22642/APP/2020/3181**

Scale:  
**1:1,250**

Planning Committee:  
**North**

Date:  
**March 2021**



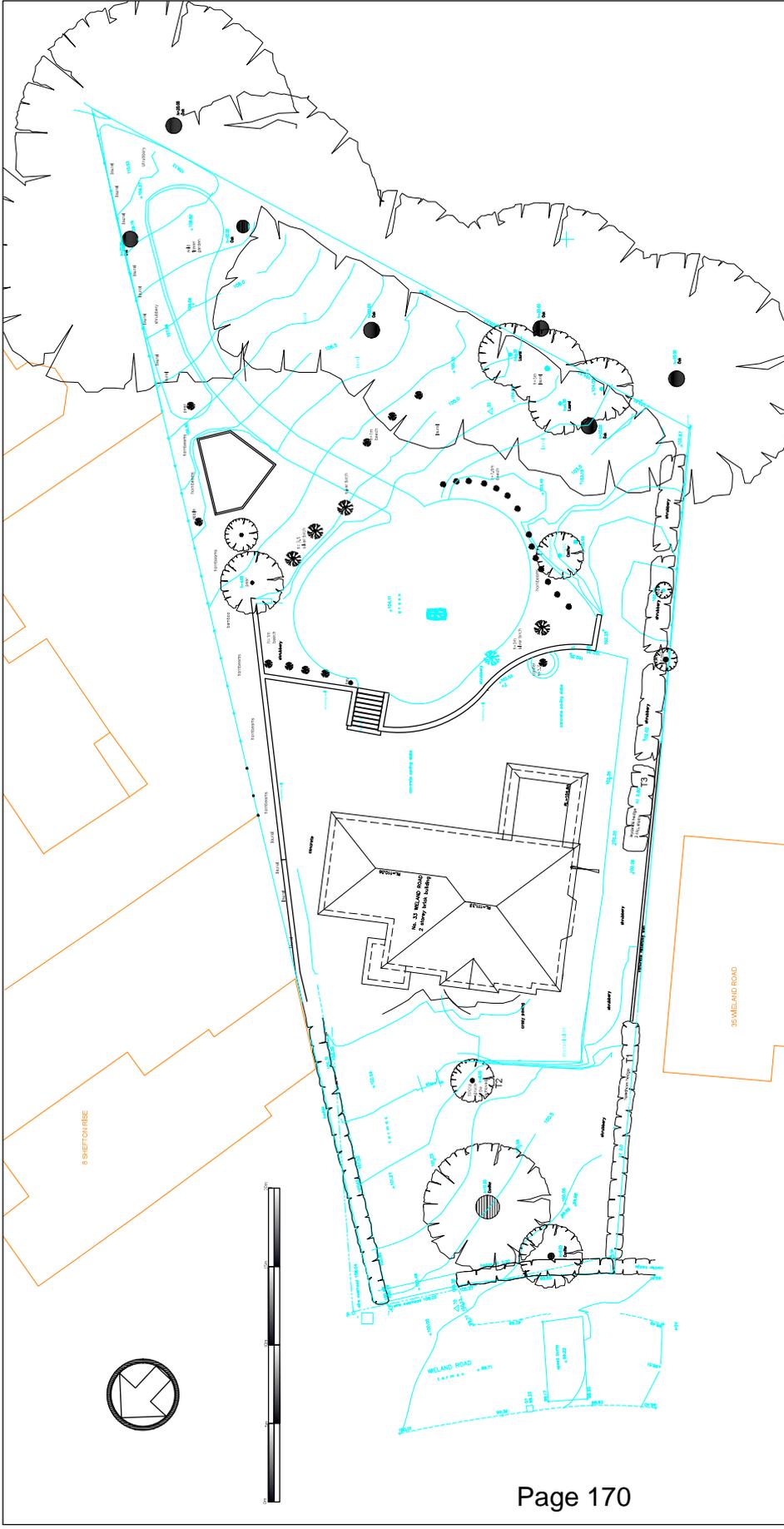
**Report of the Head of Planning, Transportation and Regeneration**

**Address** 33 WIELAND ROAD NORTHWOOD

**Development:** Part two, part three storey rear extension, subterranean garage/services and storage room, structural underpinning, external remodelling including new roof construction

**LBH Ref Nos:** 14787/APP/2020/3531

**Drawing Nos:**

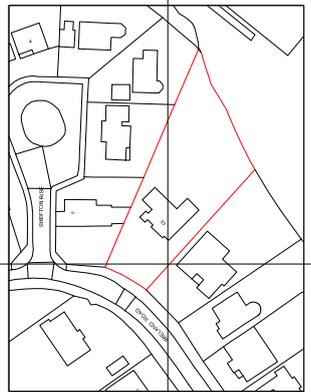


1:200 Existing Site Plan & Tree Survey

Rev d: 14.10.20 Updated with additional planting  
 Rev C: 12.04.19 Amended with updated landscaping  
 Rev B: 12.04.19 Amended with updated survey data  
 Rev A: 12.03.19 rear garden relandscaping indicated.

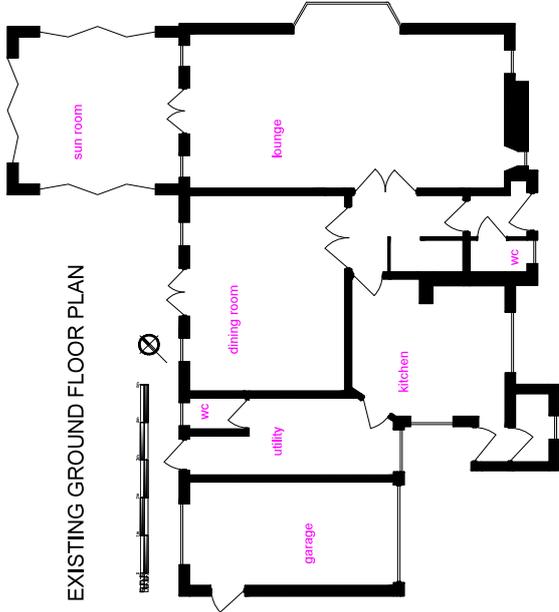
	
<b>Apollo Architectural Services</b> Tel: 01873 841 937    Mob: 07825647050 52 Garsell Road, Northwood, Middlesex, HA6 3QP	
<b>Title</b> LOCATION PLAN, SITE PLAN & TREE SURVEY 33 WELAND ROAD, Northwood, HA6 3QX	
<b>Date</b> 04.06.18	<b>Scale</b> 1:1250 & 1:200
<b>drawn by</b> Christopher J Harris	<b>Dip-Arch, B.A</b>
<b>Drawng. No</b> 401.109-04	<b>Rev. D</b> © Copyright

Original paper size A2

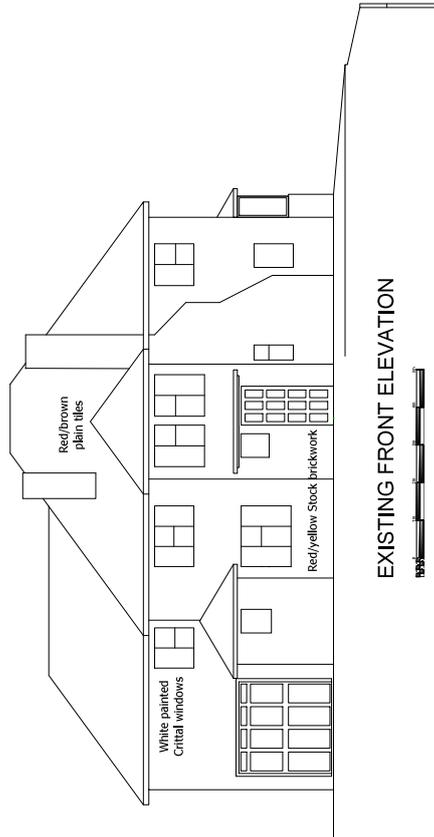
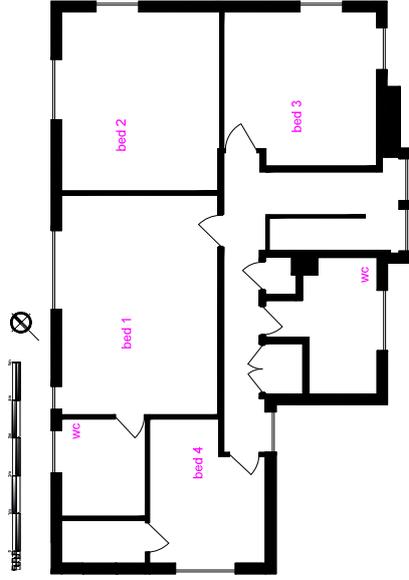


1:1250 Location Plan

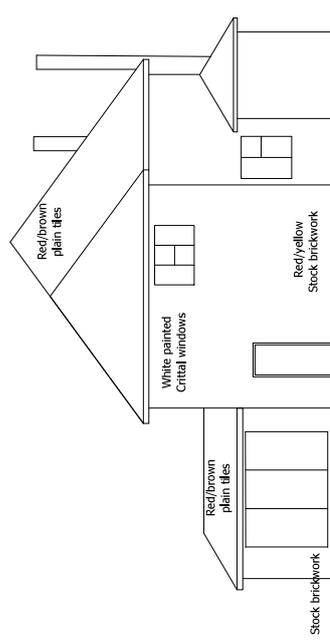
EXISTING GROUND FLOOR PLAN



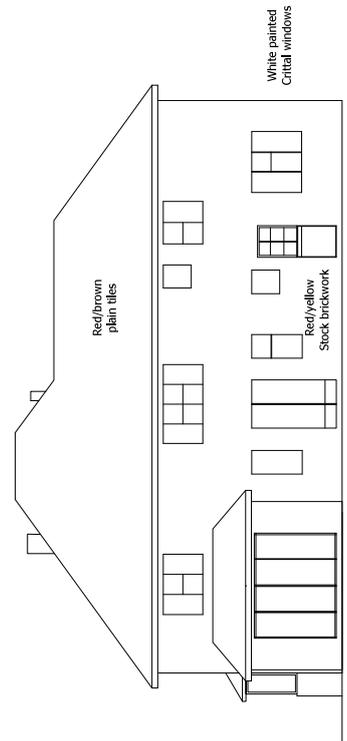
EXISTING FIRST FLOOR PLAN



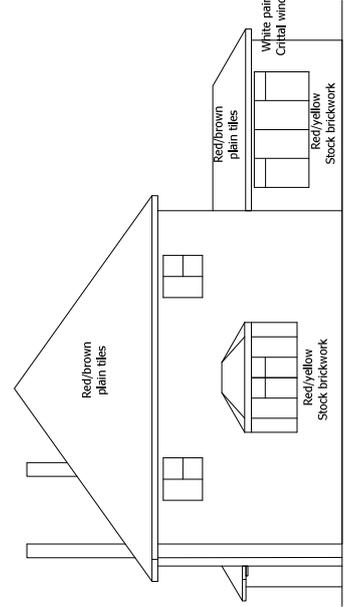
EXISTING FRONT ELEVATION



EXISTING SIDE ELEVATION



EXISTING REAR ELEVATION

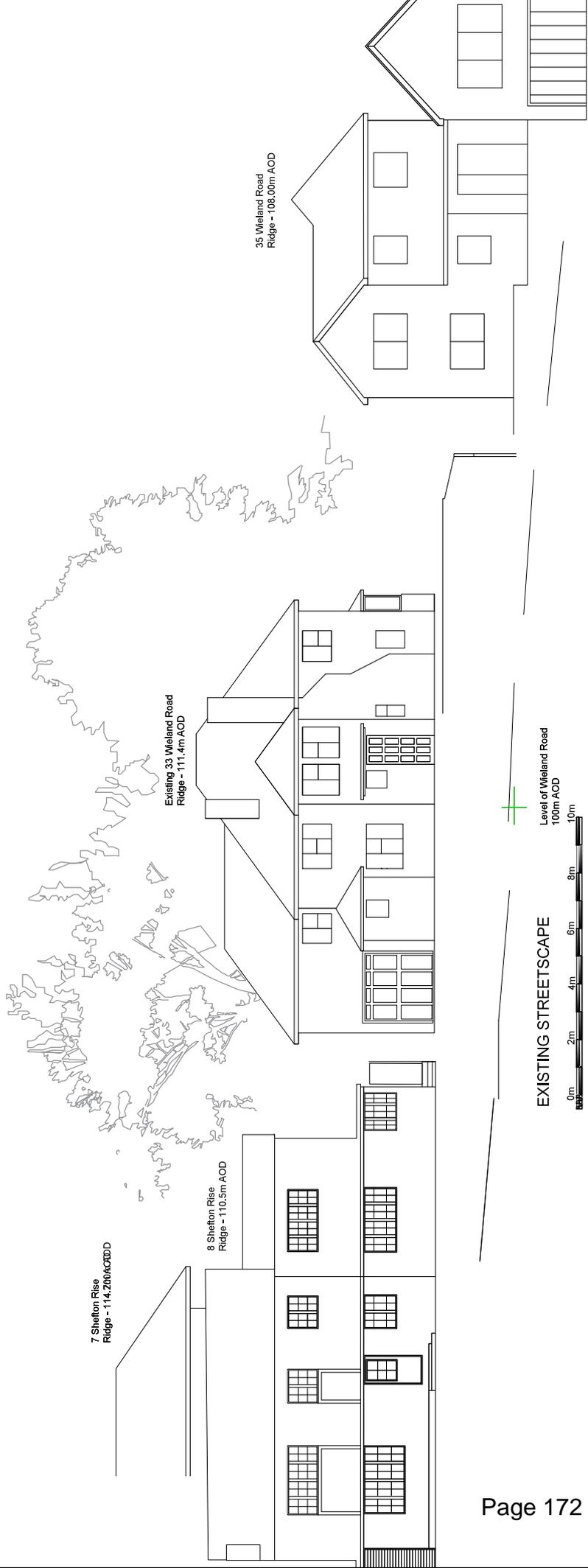


EXISTING SIDE ELEVATION

Original paper size A2

19.10.20 Rev B: Updated as per client email of same date  
 23.05.18 Rev A: Setting out of elevations updated to reflect client datum info.

	
<b>Apollo Architectural Services</b> Tel: 01873 841 937 Mob: 07825647050 52 Galeshill Road, Northwood, Middlesbrough, Cleveland, HA6 3QP	
Title	EXISTING PLANS & ELEVATIONS 33 WIELAND ROAD, NORTHWOOD HA6 3QX
Date	05.02.18
Scale	1:100
drawn by	Christopher J Harris Dip-Arch, B.A
Drawn. No.	401.109-01
	Rev. B © Copyright



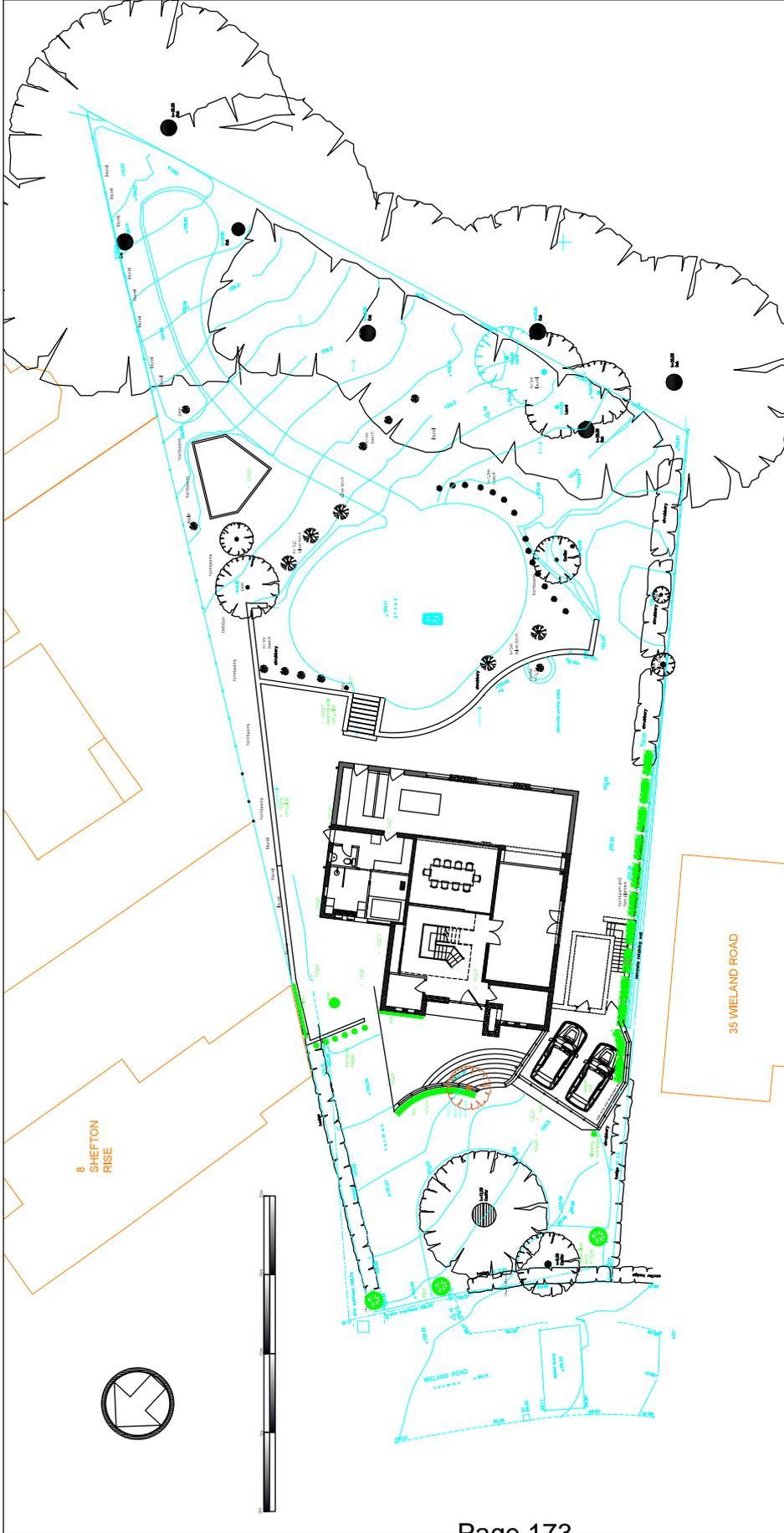
EXISTING STREETSCAPE

Level of Wieland Road  
100m AOD

0m 2m 4m 6m 8m 10m

Rev. B - 05.10.20 Changes as per client email of 24/10  
 Rev. A - 09.10.20 Updated as per client email of same date

	
<b>Apollo Architectural Services</b> Tel: 01873 841 937    Mob: 07825647050 52 Greenhill Road, Northwood, Middlesex HA6 3QP	
Title	EXISTING STREETSCAPE 33 Wieland Road, Northwood, HA6 3QX
Date	04.06.18
Scale	1:100
drawn by	Christopher J Harris    Dip-Arch, B.A
Drawng. No.	401.109-05
Rev. B	© Copyright

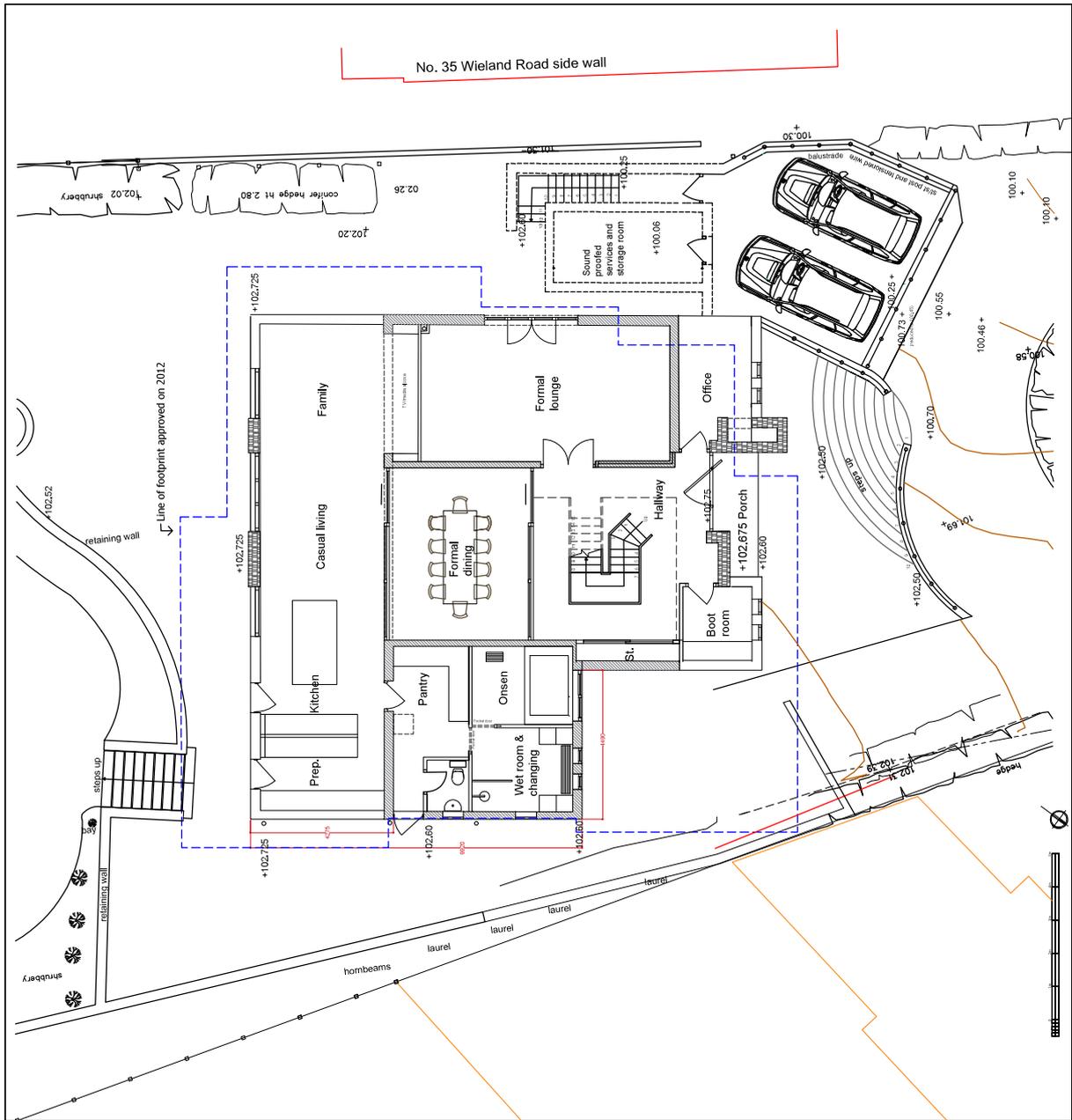


# 1:200 Proposed Site Plan & Tree Survey

Rev E: 19.10.20 Updated as per client email of same date  
 Rev D: 21.09.20 Amended with updated landscaping indicated  
 Rev C: 12.04.19 Amended with updated survey data  
 Rev B: 12.04.19 Amended with updated survey data  
 Rev A: 29.03.19 Site plan elements, external stair amended  
 Rev A: 29.03.19 Site plan elements, external stair amended  
 Rev A: 29.03.19 Site plan elements, external stair amended  
 Rev A: 29.03.19 Site plan elements, external stair amended

	
<b>Apollo Architectural Services</b> Tel: 01873 841 937    Mob: 07826547050 52 Gablehill Road, Northwood, Middlesex HA63DP PROPOSED SITE PLAN & TREE SURVEY 33 Wieland Road, Northwood, HA6 3QX	
Title	
Date	12.03.19
Scale	1:200
drawn by	Christopher J Harris Dip-Arch, B.A
Dwng. No.	401.109-15
Rev. F	© Copyright

Original paper size A2

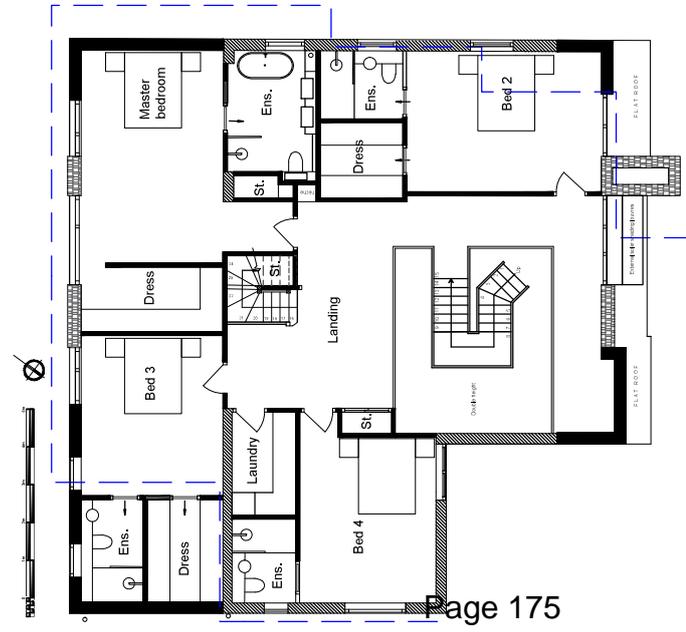


Rev I: 26.10.20 Layout changes as per client email of 24/10  
 Rev H: 19.10.20 Width of front door reduced  
 Rev G: 14.10.20 Updated to match client email of 25/09  
 Rev F: 20.08.20 Front door recessed further, stairs amended to suit  
 Rev E: 04.10.19 Datums amended based on spot heights  
 Rev D: 12.04.19 External steps amended  
 Rev C: 29.03.19 External garage layout indicated  
 Rev B: 29.11.18 Porch and formal lounge doors amended  
 Rev A: 16.04.18 Layout changes as per meeting of 12.04.18

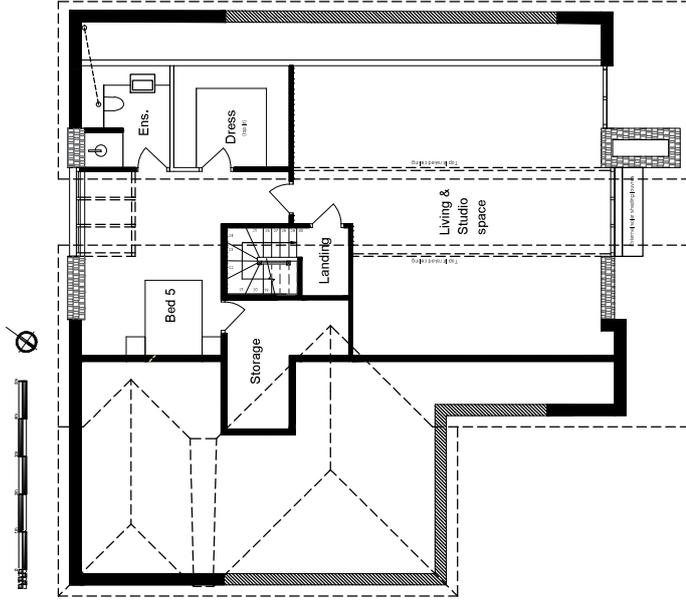
 <b>Apollo Architectural Services</b> Tel: 01923 841 937    Mob: 07825647050 52 Gaskell Road, Northwood, Middlesex HA6 3QP	
Title	PROPOSED GROUND FLOOR PLAN 33 WIELAND ROAD, NORTHWOOD HA6 3QX
Date	21.03.18
Scale	1:100
drawn by	Christopher J Harris    Dip.Arch, B.A
Driving, Inc.	401.109-11
Rev. I	© Copyright

**PROPOSED GROUND FLOOR PLAN**  
 GIA= 186m<sup>2</sup> or 232m<sup>2</sup> inc. garage  
 Dotted blue line represents footprint of building approved in 2012

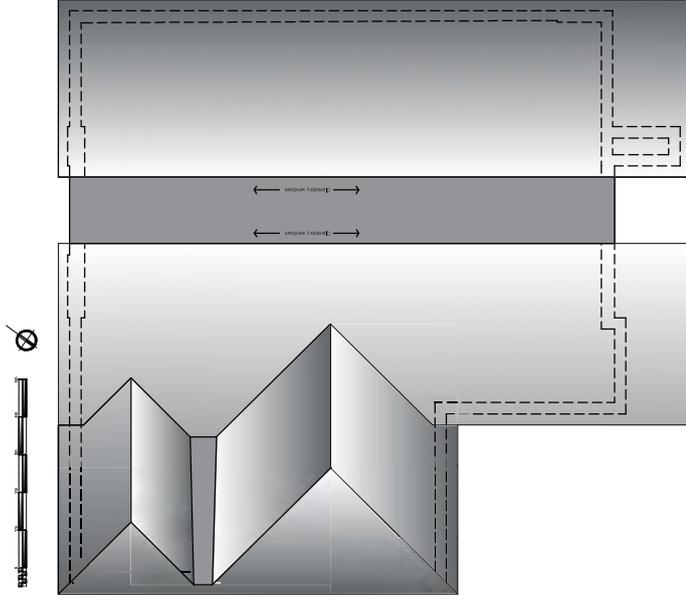
Original paper size A2



PROPOSED FIRST FLOOR PLAN GIA = 182m<sup>2</sup>



PROPOSED SECOND FLOOR - GIA = 109m<sup>2</sup>



PROPOSED ROOF PLAN

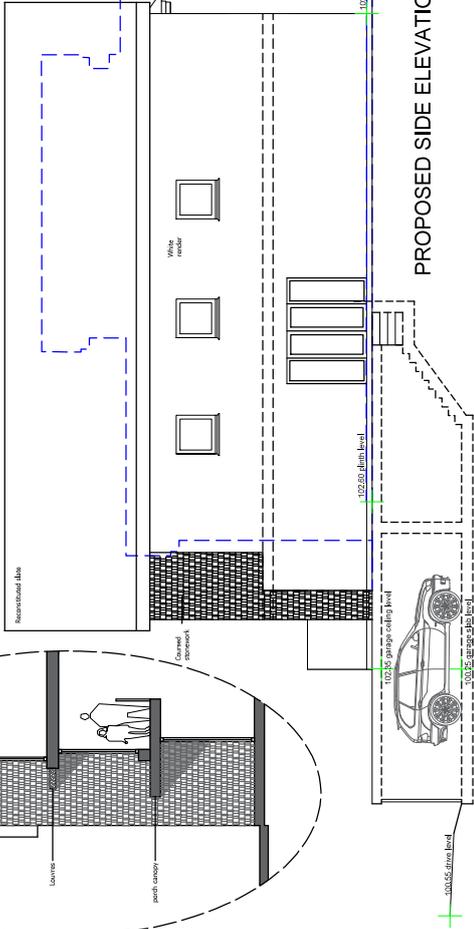
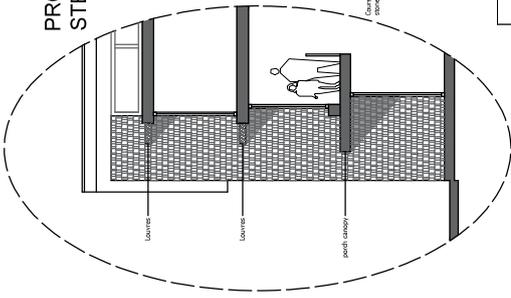
 <b>Apollo Architectural Services</b> Tel: 01873 841 937 Mob: 07825647050 52 Ganehill Road, Northwood, Middlesex, HA63QP	
Title	PROPOSED FIRST & SECOND FLOOR PLANS, ROOF PLAN 33 WIELAND ROAD, NORTHWOOD HA6 3QX
Date	21.11.18
Scale	1:100
drawn by	Christopher J Harris Dip-Arch, B.A
Drawn. No.	401.109-12
	Rev. E © Copyright

21.09.20 Rev. F: Front floorplate tiled to increase horizontality of front facade  
 07.09.20 Rev. E: Layout revisions as per client email of same date  
 01.09.20 Rev. D: Roof revisions as per client email dated 31.08.20 m/bed & bed 3 switched  
 20.08.20 Rev. C: Crown roof eliminated, m/bed & bed 3 switched; front glazing cantilevered at 2nd floor level to create staggered glazing profile in section  
 28.03.18 Rev. B: First, second & roof plan aggregated onto one drawing at 1:100  
 29.11.18 Rev. A: Bed 2 unretiled

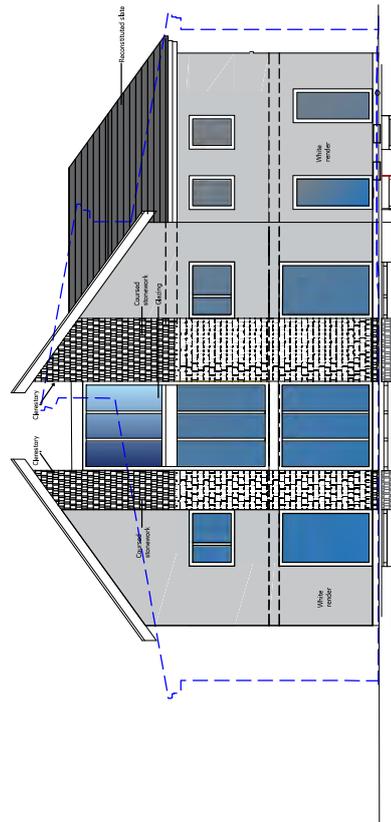


PROPOSED FRONT ELEVATION

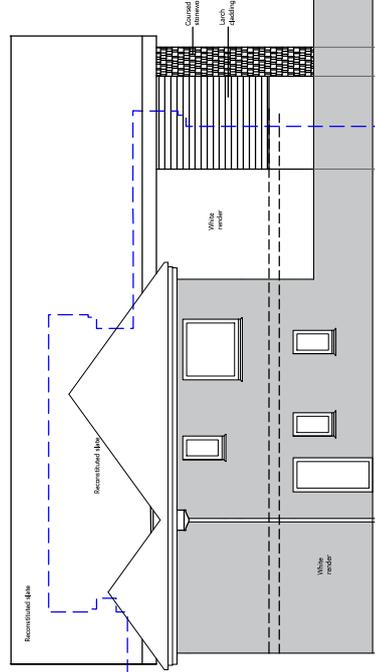
PROPOSED SECTION THROUGH STEPPED FACADE



PROPOSED SIDE ELEVATION



PROPOSED REAR ELEVATION

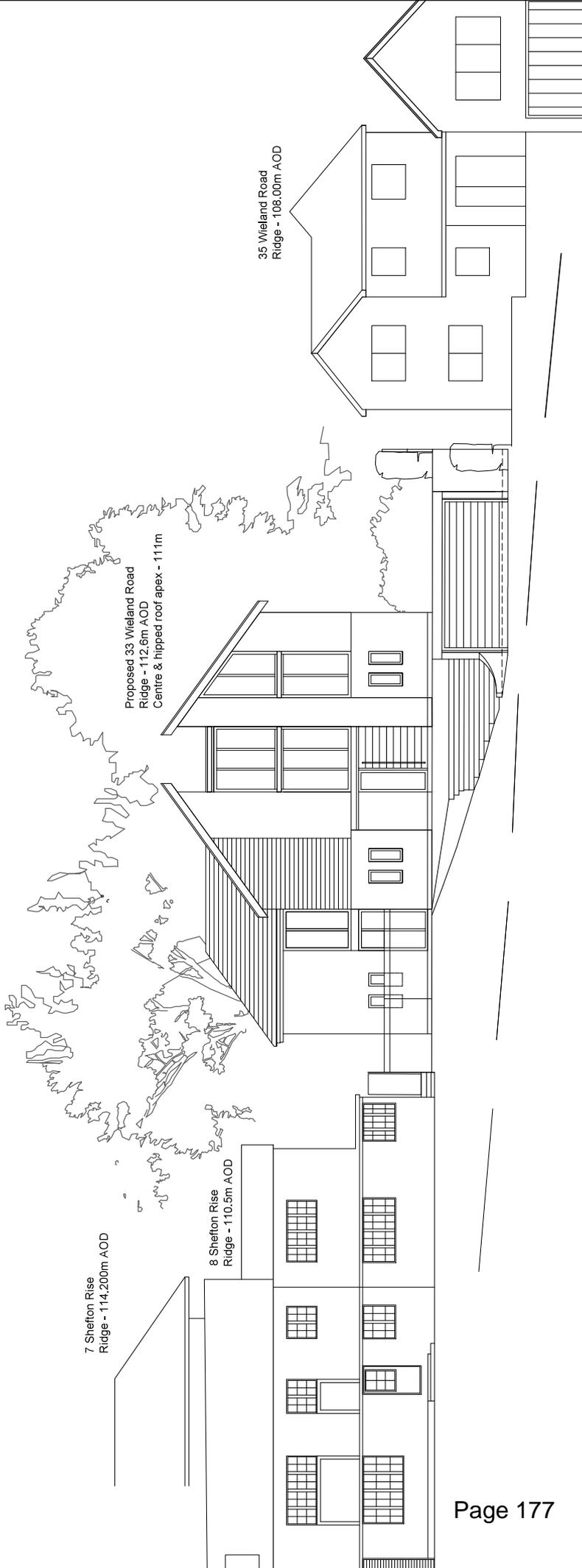


PROPOSED SIDE ELEVATION  
with position of grd flr door  
Hopper/downpipe dashes

- Rev J: 21.09.20 Section Added: glazing of front facade stepped to increase horizontality
- Rev I: 01.09.20 Roof revisions as per client email dated 31.08
- Rev H: 20.08.20 Elimination of crown roof
- Rev G: 04.10.19 Datum amended based on spot heights
- Rev F: 28.03.19 Garage amended to slanted angle
- Rev E: 28.03.19 Garage amended to slanted angle
- Rev D: 19.03.19 Additional external door added to rear elevation
- Rev C: 29.11.18 New scheme indicated
- Rev B: 23.05.18 Profiles of existing house & approved 2012 scheme indicd
- Rev A: 17.04.18 External revisions as per meeting of 12.04.18

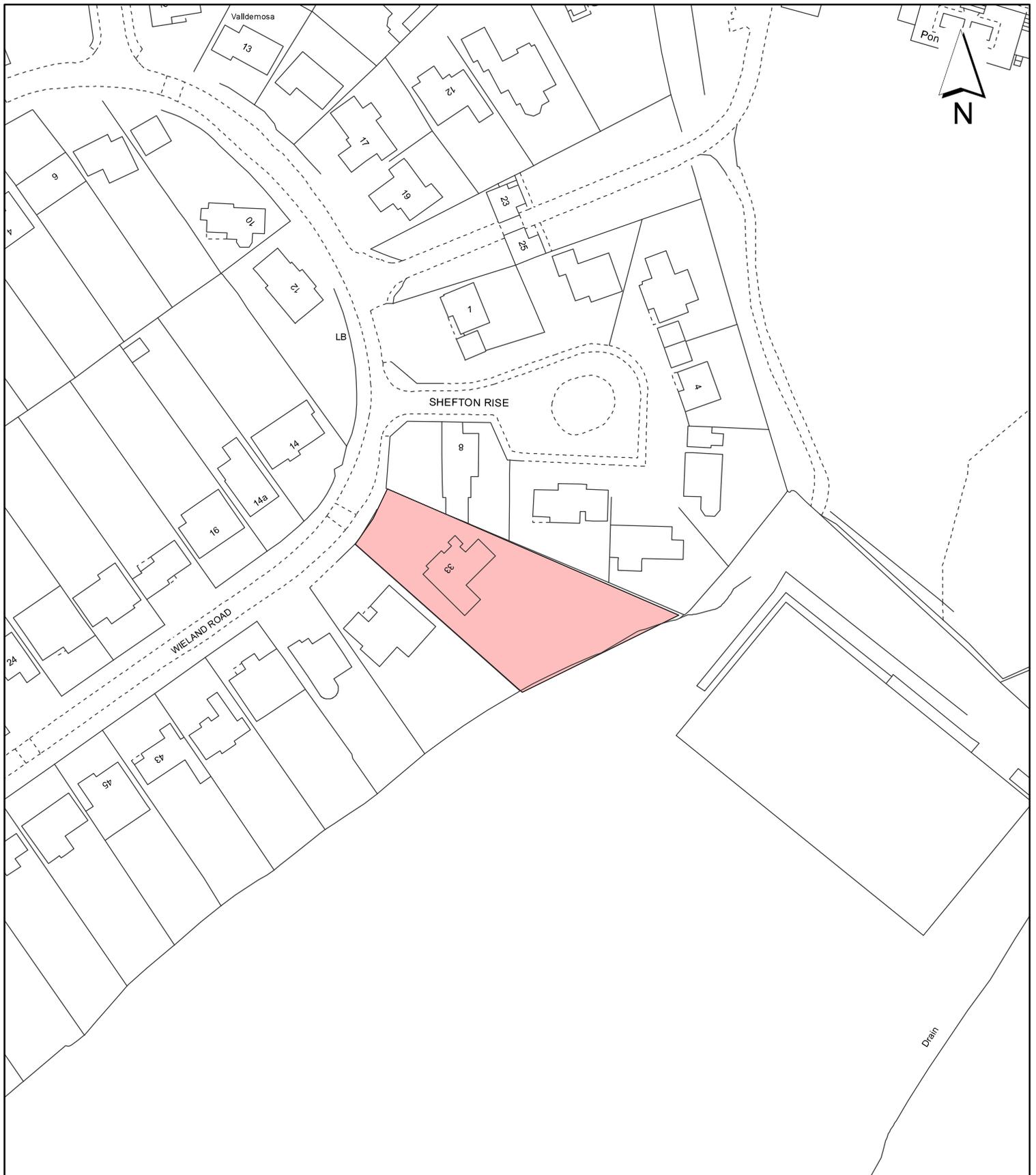
Drwg. No.	401.109-14
Scale	1:100
Date	22.11.18
Drawn by	Christopher J Harris Dip.Arch, B.A
Title	PROPOSED ELEVATIONS 52 Gatehill Road, Northwood, Middlesex HA63P 33 Wileland Road, Northwood, HA6 3QX
	Tel: 01923 841 937 Mob: 07825647050
	<b>Apollo Architectural Services</b>
Rev. J	© Copyright





Rev F: 26.10.20 Changes as per client email of 24/10  
 Rev E: 19.10.20 Updated as per client email of same date  
 Rev D: 14.10.20 Updated to match client email of 25/09  
 Rev C: 04.10.19 Datums amended based on spot heights  
 Rev B: 12.04.19 External steps amended; front door widened  
 Rev A: 28.03.19 Garage amended to display angle; wing No. amended to 16

	
<b>Apollo Architectural Services</b>	
Tel: 01873 841 937    Mob: 07826547050 52 Garshill Road, Northwood, Middlesex HA6 3QP	
Title	PROPOSED STREETSCAPE 33 Wieland Road, Northwood, HA6 3QX
Date	04.06.18
Scale	1:100
drawn by	Christopher J Harris Dip.Arch, B.A
Dwg. No.	401.109-16
Rev. F © Copyright	



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Site Address:

**33 WIELAND ROAD  
 NORTHWOOD**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**14787/APP/2020/3531**

Scale:

**1:1,250**

Planning Committee:

**North Page 178**

Date:

**March 2021**



## **Report of the Head of Planning, Transportation and Regeneration**

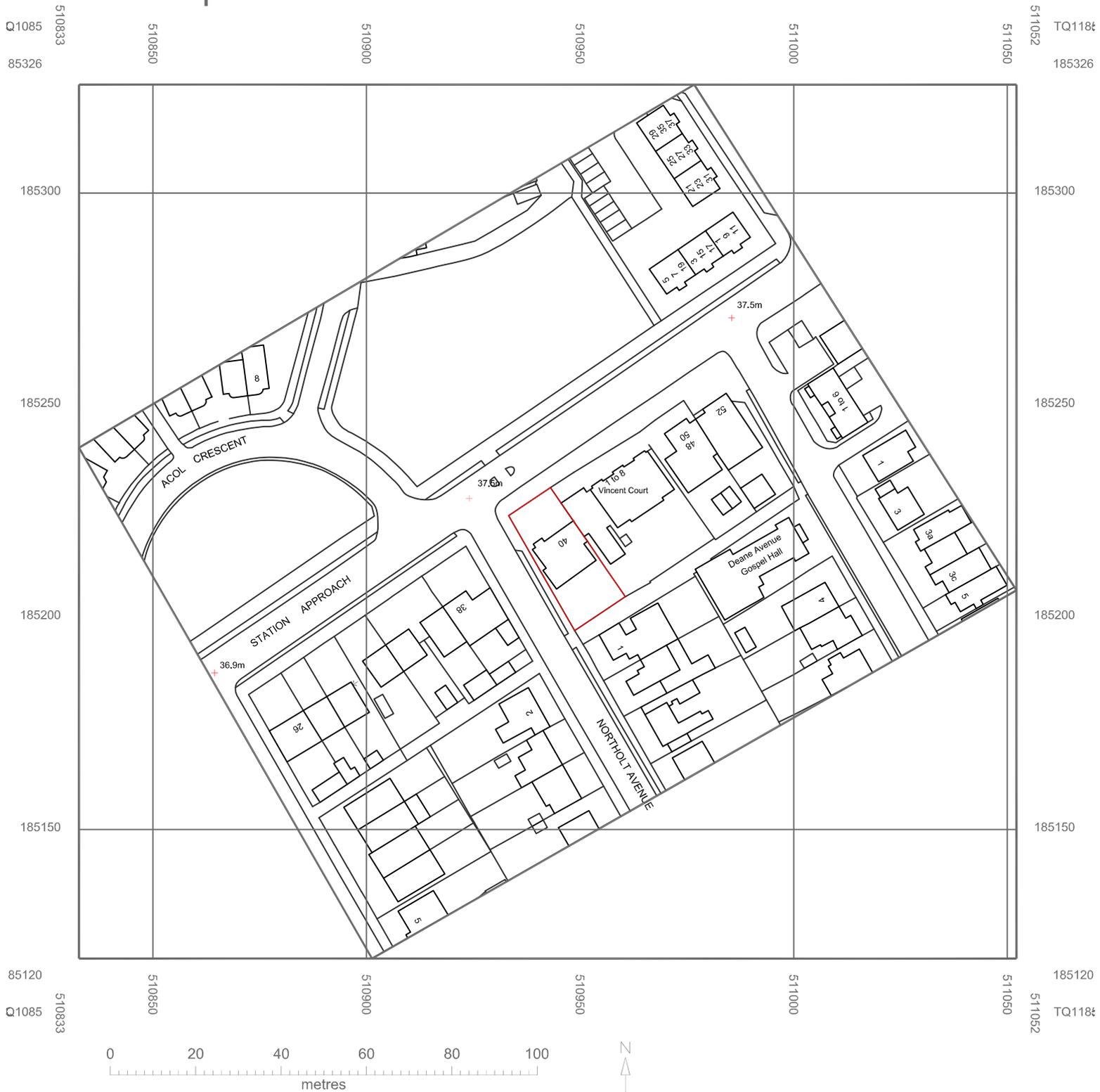
**Address** 40 STATION APPROACH SOUTH RUISLIP

**Development:** Erection of a three storey building comprising a dental surgery and 7 residential units (6 no. 1-bed units and 1 no. 2-bed unit), car and cycle parking and associated works, temporary permission for the siting of a modular building to the rear of the site for use as a dental surgery during the demolition of the existing dental surgery and construction of the proposed dental surgery.

**LBH Ref Nos:** 27354/APP/2020/4256

**Drawing Nos:**

# MapServe



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40 Station Approach  
HA4 6RZ  
1:1250

001\_0006  
Rev 00

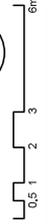
42 Epsom Road, London, N4 1EQ  
info@studiohallettike.co.uk  
www.studiohallettike.co.uk

All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work and contractor is to advise on all details and ensure stability and strength of construction. The contractor to provide setting out for the approved construction. All structures to be constructed in accordance with the Building Regulations and to Engineers' details. All structures to be constructed in accordance with the Building Regulations and Fire Regulations.

Contractor to check if the details in the latest revision apply to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.  
Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
Job No. 001  
Client Albon Housing Ltd



Status DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title EXISTING SITE PLAN

Scale	Date	Drawn
1:150@A3	28.07.2020	MI

Drawing No.	Rev
001_0005	01

NORTHOLT AVENUE

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Contractor to check if the design is the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.  
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Project Name 40 Station Approach  
Job No. 001  
Client Albon Housing Ltd



Status DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title EXISTING SITE PLAN

Scale	Date	Drawn
1:150@A3	28.07.2020	MI
Drawing No.	Rev	Rev
001_0005		01

NORTHOLT AVENUE



42 Erdymion Road, LONDON, N4 1EQ  
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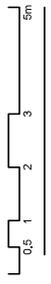
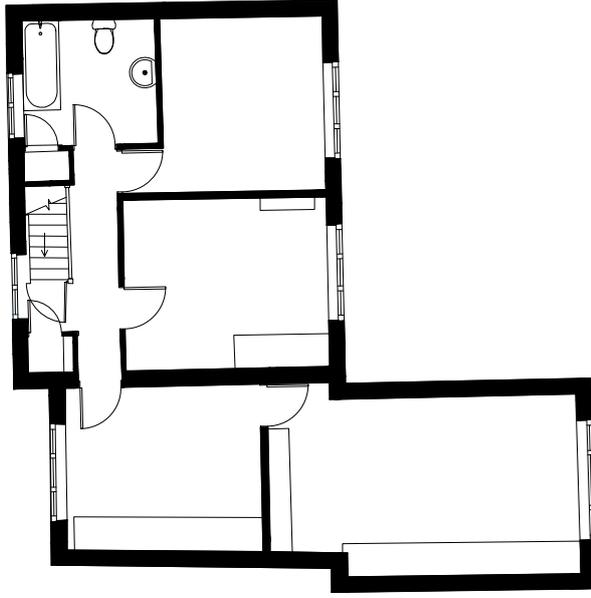
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Contractor to check if the drawings in the latest revision refer to construction and to remove all previous revisions from circulation.

To be read in conjunction with consultant drawings.  
 Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd

Schedule of Accommodation				
LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E(0)	Dental Surgery	49.3	531
	C3	Dwelling	23.3	251
		<b>Sub Total</b>	<b>72.6</b>	<b>781</b>
L01	C3	Dwelling	68.0	743
		<b>Sub Total</b>	<b>68.0</b>	<b>743</b>
		<b>Total</b>	<b>141.6</b>	<b>1524</b>



Status  
**DESIGN**

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title  
 EXISTING PLAN L01

Scale 1:100@A3  
 Date Drawn 28.07.2020  
 Rev MI

Drawing No. 001\_0001  
 Rev 01

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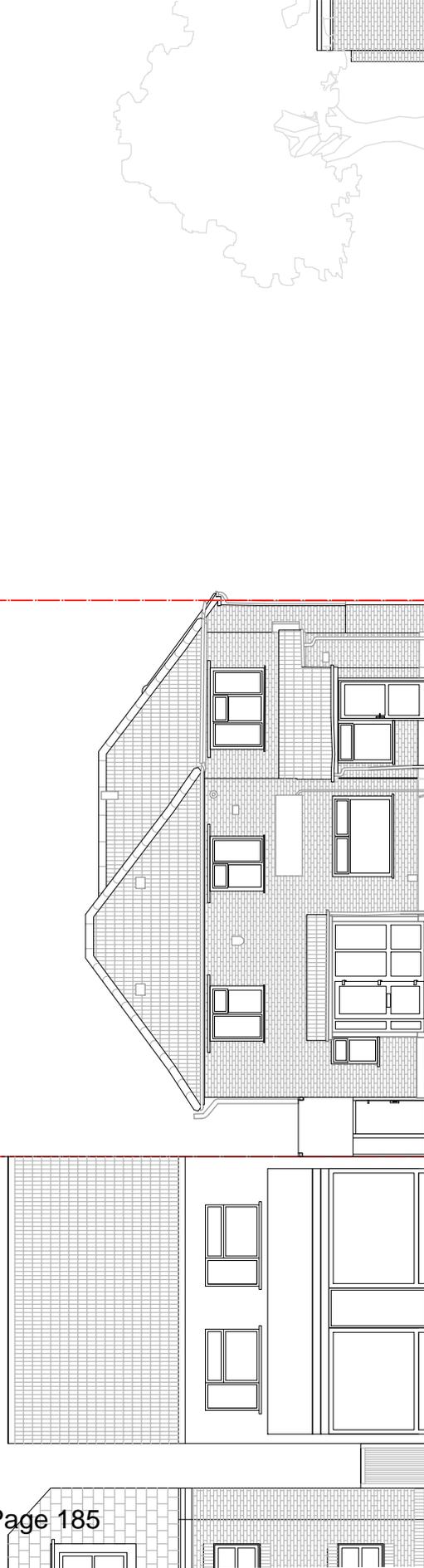
All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are in millimetres. The contractor is to advise on all details and ensure stability and strength of construction. The contractor to provide setting out for the approved construction. The contractor to provide all necessary approvals, including Building Regulations, Structural Engineers details and Local Authority and BCO approvals. The contractor to comply with current Building and Fire Regulations.

Contractor to check if the drawings in the latest revision prior to construction and to remove all previous revisions from circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albion Housing Ltd



1 Existing North Elevation  
 Scale 1:100

Status  
 DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title  
 EXISTING NORTH ELEVATION

Scale 1:100@A3  
 Date 29.07.2020  
 Drawn MI

Drawing No. 001\_0020  
 Rev 01

All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work and contractor is to advise on all details and ensure stability and strength of construction. The contractor to provide setting out for the approved construction, including the location of all structural elements, and to ensure that all work complies with the current Building and Fire Regulations. All structures to be designed and constructed in accordance with the current Building and Fire Regulations.

Contractor to check if the drawings in the latest revision refer to construction and to remove all previous revisions from circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



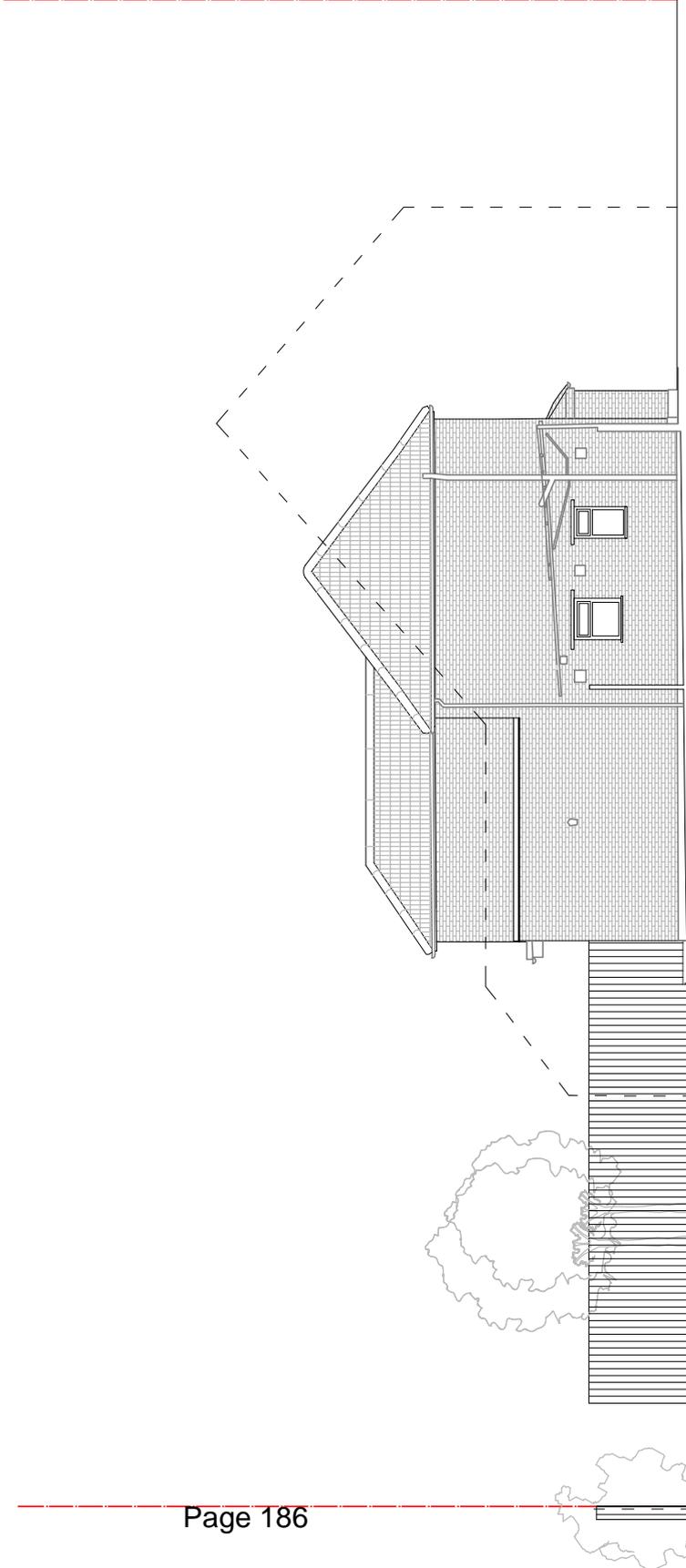
Status  
 DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title  
 EXISTING EAST ELEVATION

Scale	Date	Drawn
1:100@A3	28.07.2020	MI

Drawing No.	Rev
001_0023	01



1 Existing East Elevation  
 Scale 1:100

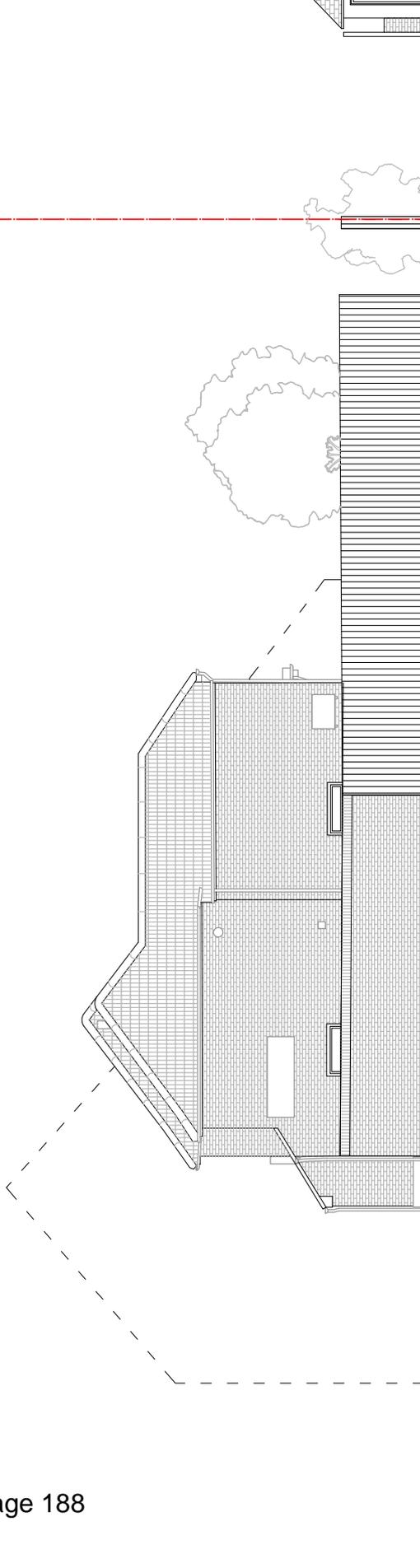


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Contractor to check if the drawings in the latest revision refer to construction and to remove all previous revisions from circulation.

To be read in conjunction with consultant drawings.  
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Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



Status  
 DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

1 Existing West Elevation  
 Scale 1:100

Title  
 EXISTING WEST ELEVATION

Scale	Date	Drawn
1:100@A3	28.07.2020	MI
Drawing No.	Rev	
001_0021	01	

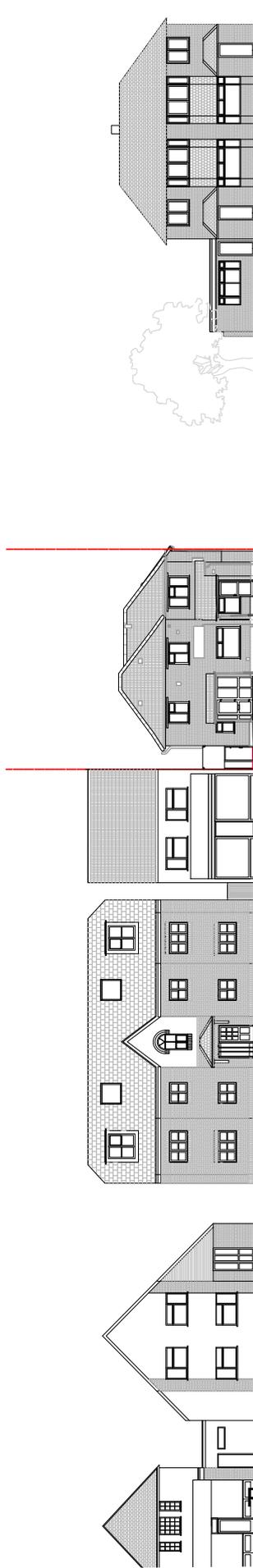
All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work and construction is to adhere to all details and ensure stability and strength of construction. The contractor to provide setting out and ensure that all construction is in accordance with the approved drawings. The contractor to ensure that all construction is in accordance with the current Building and Fire Regulations. All structures to be designed and constructed in accordance with the current Building and Fire Regulations.

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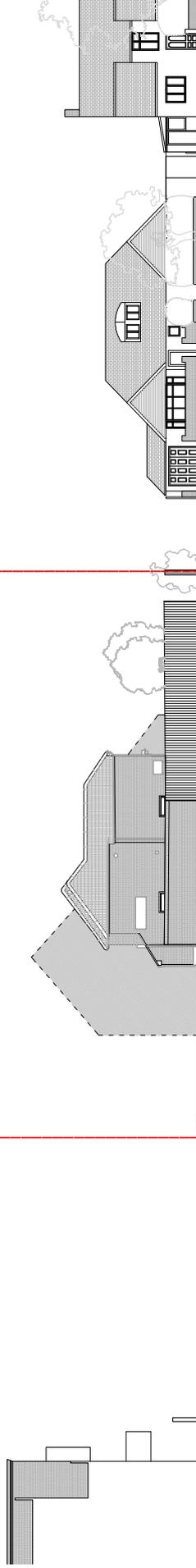
To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



1 Existing Street Elevation - Station Approach North Facing  
 Scale 1:250



2 Existing Street Elevation - Northolt Avenue West Facing  
 Scale 1:250



Status

DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title

EXISTING STREET SCENES

Scale	Date	Drawn
1:250@A3	28.07.2020	MI

Drawing No.	Rev
001_0030	01

42 Enyrdon Road, London, N4 1EQ  
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Contractor to check if the drawings in the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
Job No. 001  
Client Alpon Housing Ltd

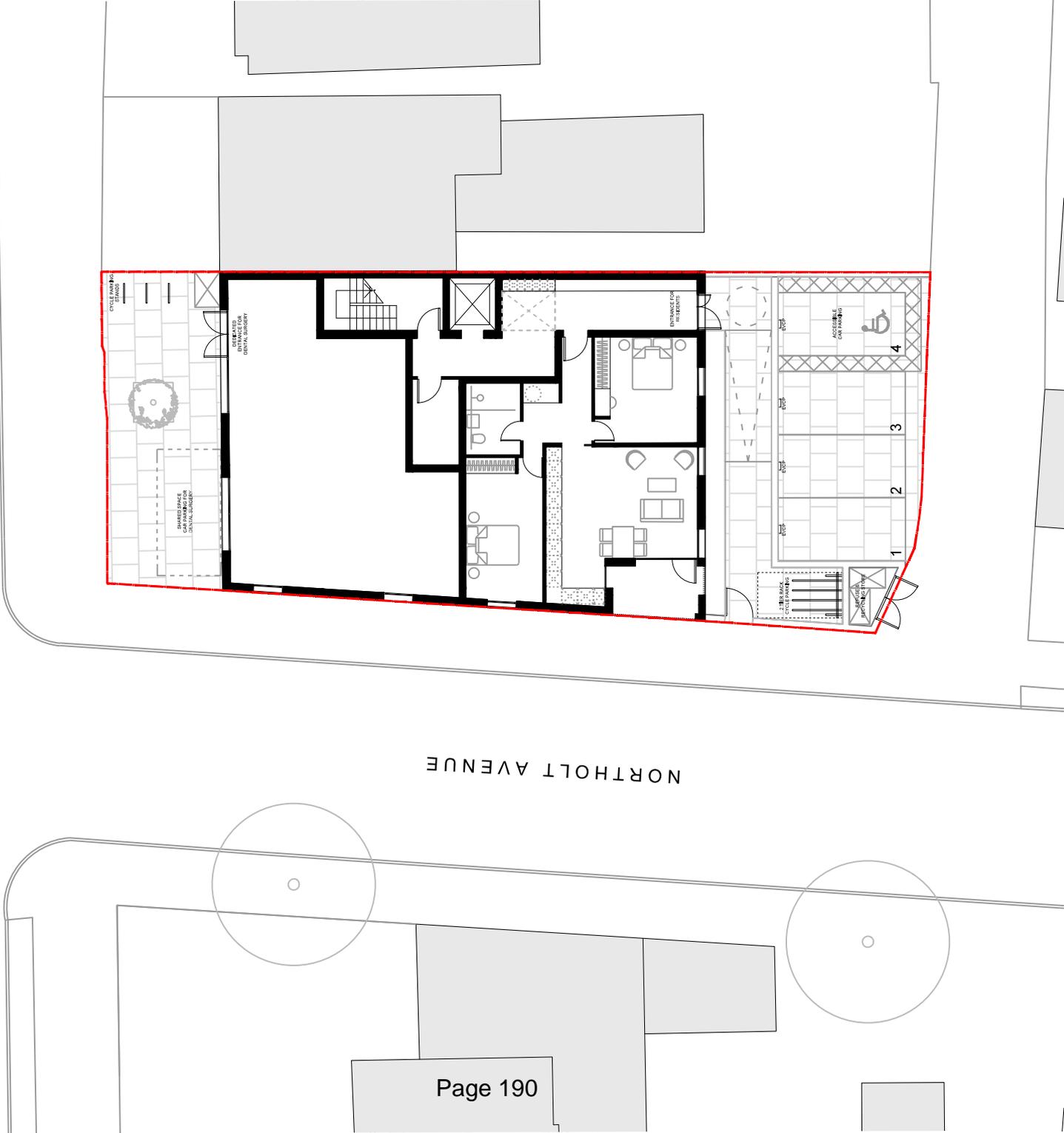


Status DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title PROPOSED SITE PLAN

Scale	Date	Drawn
1:150@A3	28.07.2020	MI
Drawing No.	Rev	
001_2005	01	



NORTHOLT AVENUE



# STATION APPROACH

# STUDIO HALLETT IKE

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Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albion Housing Ltd



REV	DATE	DESC
00	11.02.2021	Planning

Step Free Route

Notes:

- Step free access will be provided from all entrances/exits to each individual dwelling and roof terrace.
- All surfaces to be level and hard wearing.
- All private dwellings to meet at least Part M4 (1).

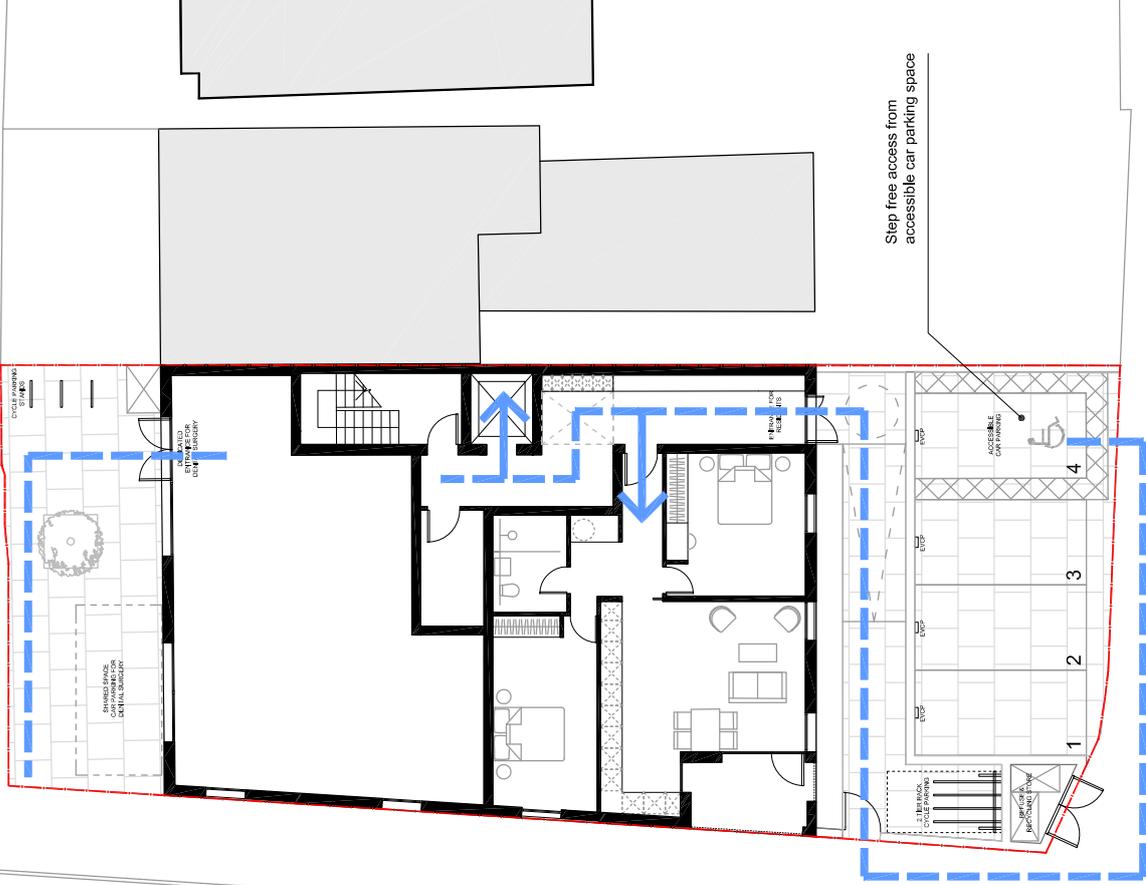
Title  
 PRPSED SITE - ACCESSIBILITY

Scale 1:150@A3  
 Date Drawn 29.07.2020  
 M1

Drawing No. 001\_2005A  
 Rev 00

NORTHOLT AVENUE

Step free access from accessible car parking space



# STUDIO HALLETT IKE

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Contractor to check in this drawing is the latest revision prior to and to remove all previous revisions from circulation.

To be read in conjunction with consultant drawings.

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Project Name 40 Station Approach  
 Job No. 001  
 Client Albion Housing Ltd

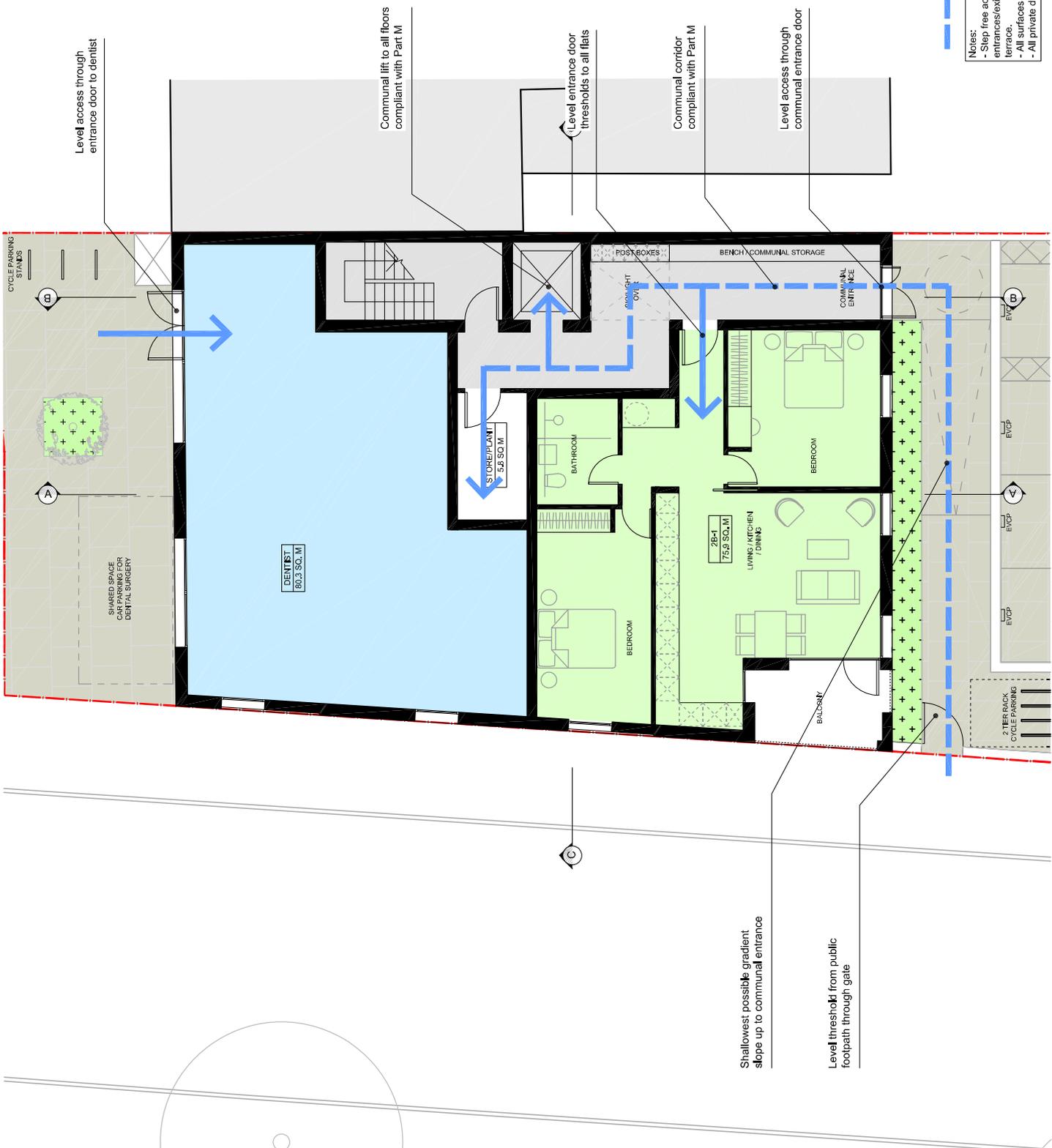
LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E(e)	Dental Surgery	80.3	864
		Apr 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apr 1 Bedroom-1	51.0	549
		Apr 1 Bedroom-2	51.1	550
		Apr 1 Bedroom-3	54.8	590
L02	C3	Apr 1 Bedroom-4	51.0	549
		Apr 1 Bedroom-5	51.1	550
		Apr 1 Bedroom-6	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
		<b>Total</b>	<b>470</b>	<b>5059</b>



Status DESIGN

REV	DATE	DESC
00	11.02.2021	Planning

Title PRPSED PLAN L00- ACCESSIBILITY  
 Scale 1:100@A3  
 Date 29.07.2020  
 Drawn MI  
 Rev 00  
 Drawing No. 001\_2000A



- Notes:
- Step free access will be provided from all entrances/exits to each individual dwelling and roof terrace.
  - All surfaces to be level and hard wearing.
  - All private dwellings to meet at least Part M4 (1).

Shallowest possible gradient slope up to communal entrance

Level threshold from public footpath through gate

All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work. Contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out for all work. All work to be approved by the relevant Local Authority (BCC) and Environmental Health Officers. All work to be in accordance with current Building Regulations and to Engineers' details. All structure to be designed and constructed in accordance with current Building and Fire Regulations.

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To be read in conjunction with consultant drawings.

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Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd

Schedule of Accommodation

LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E1e)	Dental Surgery	80.3	864
		Apt 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apt 1 Bedroom-1	51.0	549
		Apt 1 Bedroom-2	51.1	550
		Apt 1 Bedroom-3	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
L02	C3	Apt 1 Bedroom-4	51.0	549
		Apt 1 Bedroom-5	51.1	550
		Apt 1 Bedroom-6	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
		<b>Total</b>	<b>470</b>	<b>5059</b>



Status  
 DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title  
 PROPOSED PLAN L00

Scale 1:100@A3  
 Date Drawn 29.07.2020  
 MI

Drawing No. 001\_2000  
 Rev 01



# STUDIO HALLETT IKE

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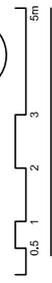
Contractor to check this drawing is the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.

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Project Name 40 Station Approach  
 Job No. 001  
 Client Albion Housing Ltd

LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E(e)	Dental Surgery	80.3	864
		Apt 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apt 1 Bedroom-1	51.0	549
		Apt 1 Bedroom-2	51.1	550
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
L02	C3	Apt 1 Bedroom-4	51.0	549
		Apt 1 Bedroom-5	51.1	550
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
		<b>Total</b>	<b>470</b>	<b>5059</b>



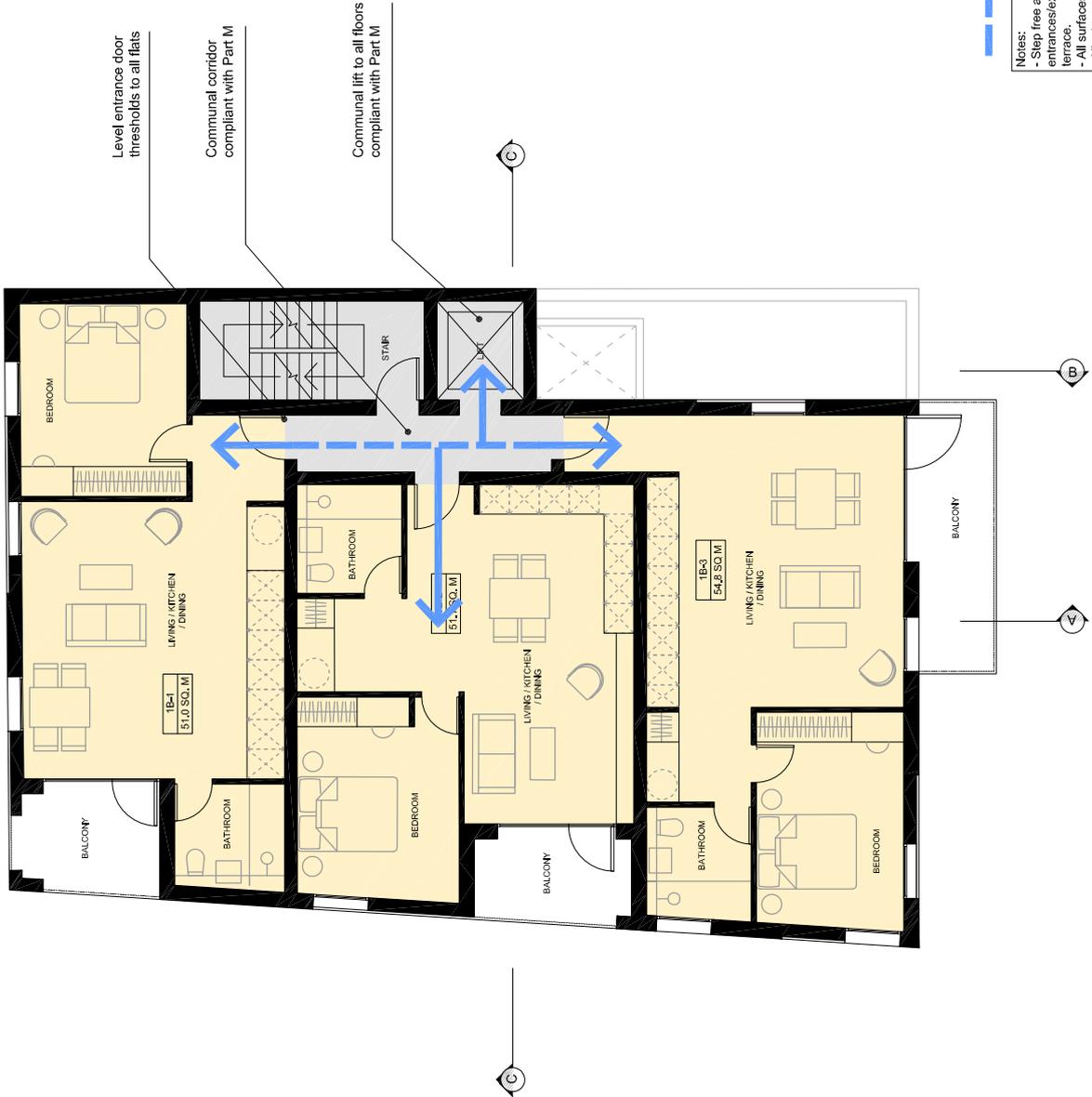
Status  
**DESIGN**

REV	DATE	DESC
00	11.02.2021	Planning

Title  
**PRPSED PLAN L01 - ACCESSIBILITY**

Scale 1:100@A3  
 Date 29.07.2020  
 Drawn MI

Drawing No. 001\_2001A  
 Rev 00



Step Free Route

- Notes:
- Step free access will be provided from all entrances/exits to each individual dwelling and roof terrace.
  - All surfaces to be level and hard wearing.
  - All private dwellings to meet at least Part M4 (1).

All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work. Contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out for all construction. All construction to be in accordance with Local Authority (L.A.) and Environmental Health Regulations and to Engineers' detail. All structure to be designed in accordance with current Building and Fire Regulations.

Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd

### Schedule of Accommodation

LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E1e)	Dental Surgery	80.3	864
	C3	Apt 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apt 1 Bedroom-1	51.0	549
	C3	Apt 1 Bedroom-2	51.1	550
	C3	Apt 1 Bedroom-3	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
L02	C3	Apt 1 Bedroom-4	51.0	549
	C3	Apt 1 Bedroom-5	51.1	550
	C3	Apt 1 Bedroom-6	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
		<b>Total</b>	<b>470</b>	<b>5059</b>



Status  
**DESIGN**

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title  
**PROPOSED PLAN L01**

Scale 1:100@A3  
 Date Drawn 29.07.2020  
 MI

Drawing No. 001\_2001  
 Rev 01



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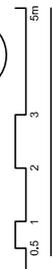
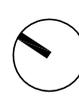
Contractor to check this drawing is the latest revision prior to and to remove all previous revisions from site/calculation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albion Housing Ltd

LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E(e)	Dental Surgery	80.3	864
		Apt 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apt 1 Bedroom-1	51.0	549
		Apt 1 Bedroom-2	51.1	550
		Apt 1 Bedroom-3	54.8	590
	<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>	
L02	C3	Apt 1 Bedroom-4	51.0	549
		Apt 1 Bedroom-5	51.1	550
		Apt 1 Bedroom-6	54.8	590
	<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>	
	<b>Total</b>	<b>470</b>	<b>5059</b>	



Status  
**DESIGN**

REV	DATE	DESC
00	11.02.2021	Planning

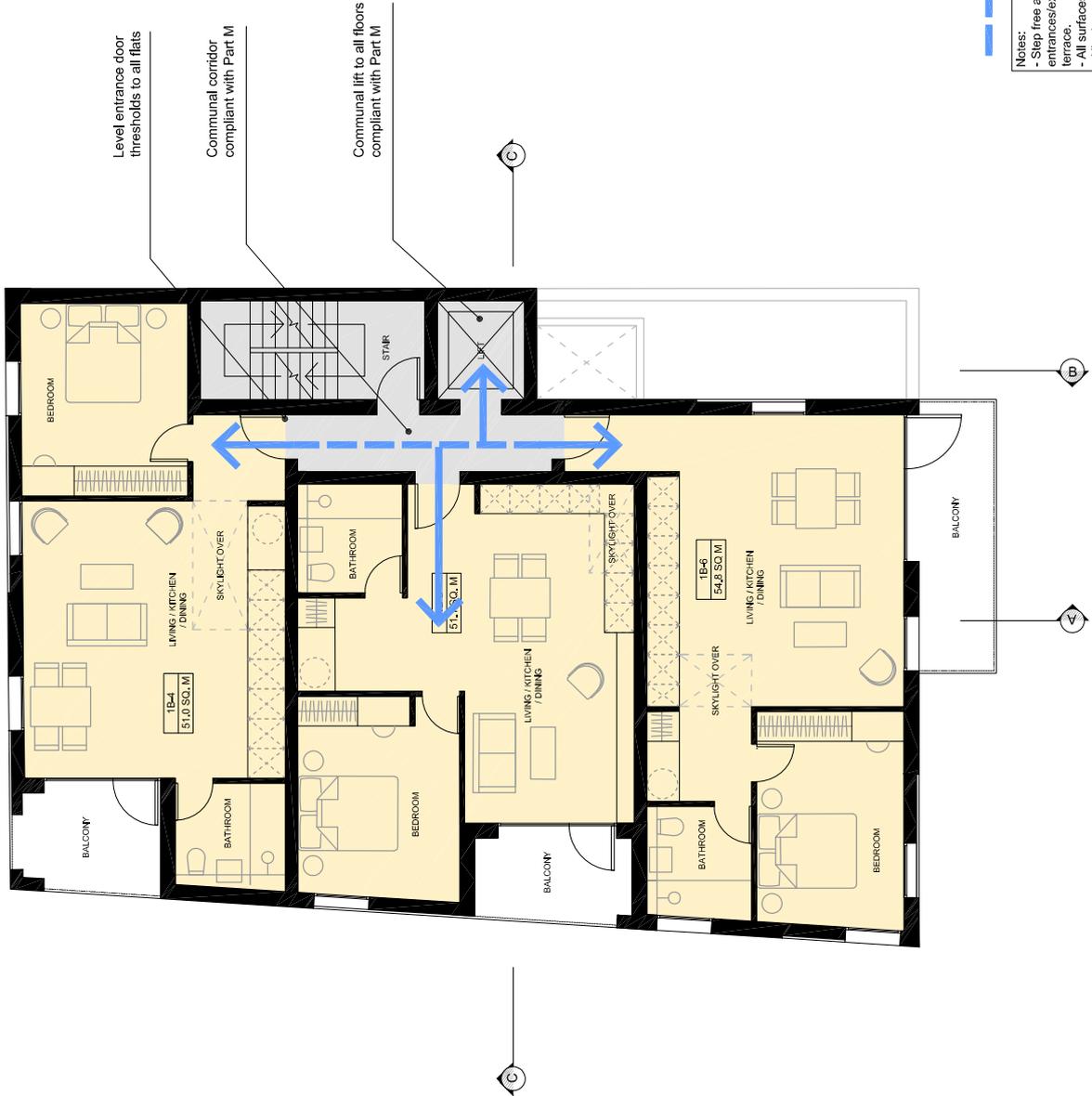
Title  
**PRPSED PLAN L03 - ACCESSIBILITY**

Scale  
 1:100@A3

Date Drawn  
 29.07.2020 MI

Drawing No.  
**001\_2002A**

Rev  
**00**



Level entrance door thresholds to all flats

Communal corridor compliant with Part M

Communal lift to all floors compliant with Part M

Step Free Route

- Notes:
- Step free access will be provided from all entrances/exits to each individual dwelling and roof terrace.
  - All surfaces to be level and hard wearing.
  - All private dwellings to meet at least Part M4 (1).

All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work. Contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out for all construction. All construction to be in accordance with Local Authority (L.A.) and Environmental Health Regulations and to Engineers' detail. All structure to be designed in accordance with current Building and Fire Regulations.

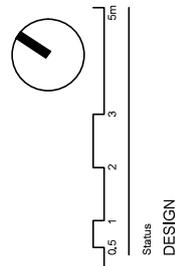
Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site/circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd

LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E1e)	Dental Surgery	80.3	864
	C3	Apt 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apt 1 Bedroom-1	51.0	549
	C3	Apt 1 Bedroom-2	51.1	550
	C3	Apt 1 Bedroom-3	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
L02	C3	Apt 1 Bedroom-4	51.0	549
	C3	Apt 1 Bedroom-5	51.1	550
	C3	Apt 1 Bedroom-6	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
		<b>Total</b>	<b>470</b>	<b>5059</b>



REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title PROPOSED PLAN L03  
 Scale 1:100@A3 Date Drawn 29.07.2020 MI  
 Drawing No. 001\_2002 Rev 01



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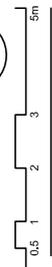
Contractor to check this drawing is the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albion Housing Ltd

LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E(e)	Dental Surgery	80.3	864
		Apt 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apt 1 Bedroom-1	51.0	549
		Apt 1 Bedroom-2	51.1	550
		Apt 1 Bedroom-3	54.8	590
	<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>	
L02	C3	Apt 1 Bedroom-4	51.0	549
		Apt 1 Bedroom-5	51.1	550
		Apt 1 Bedroom-6	54.8	590
	<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>	
	<b>Total</b>	<b>470</b>	<b>5059</b>	



Status  
**DESIGN**

REV	DATE	DESC
00	11.02.2021	Planning

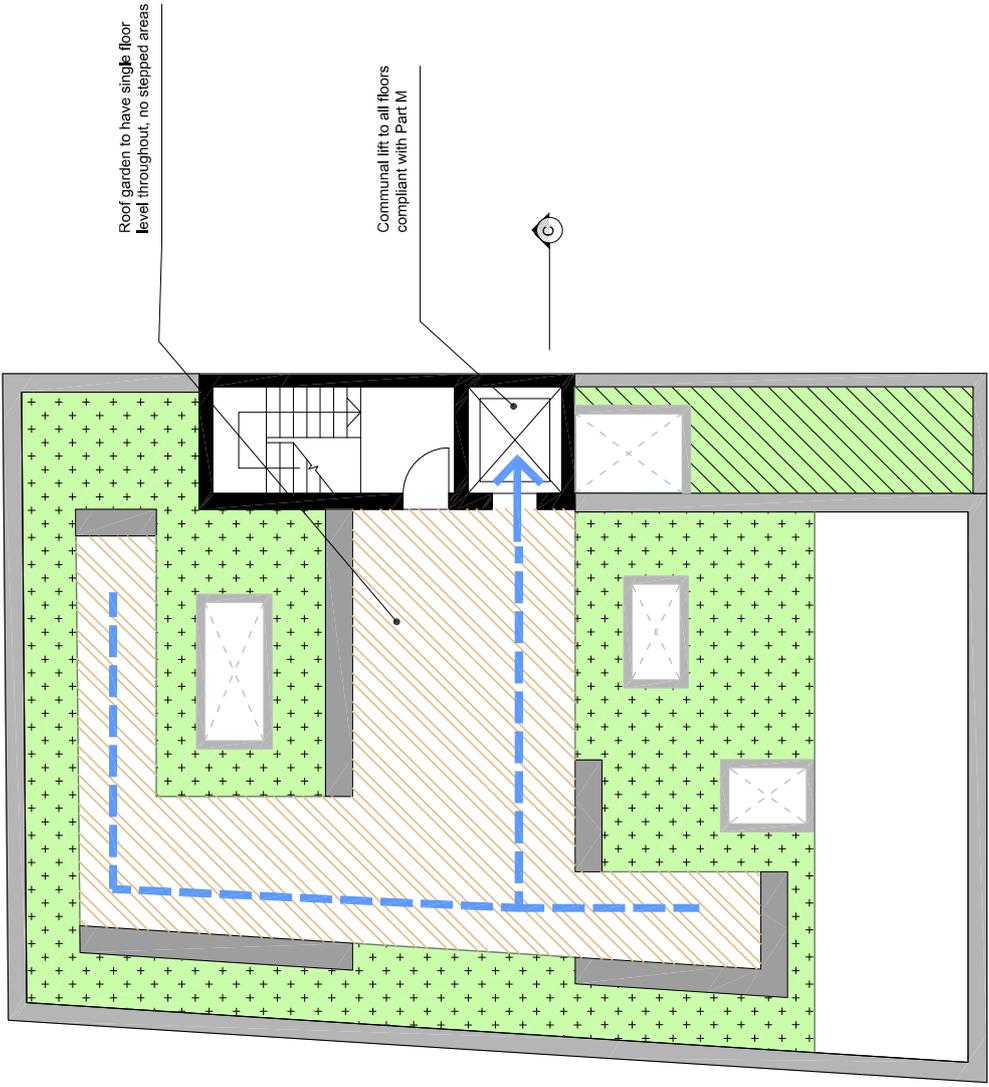
Title  
**PRPSED RF PLAN - ACCESSIBILITY**

Scale  
 1:100@A3

Date Drawn  
 29.07.2020 MI

Drawing No.  
**001\_2003A**

Rev  
**00**



Roof garden to have single floor level throughout, no stepped areas

Communal lift to all floors compliant with Part M

Step Free Route

- Notes:
- Step free access will be provided from all entrances/exits to each individual dwelling and roof terrace.
  - All surfaces to be level and hard wearing.
  - All private dwellings to meet at least Part M4 (1).

All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work and contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out and approval for construction. All structures to be designed in accordance with the Building Regulations, Local Authority (BCO) and Environmental Health Regulations and to Engineers' detail. All structures to be designed in accordance with the Building Regulations, Structural Engineers detail and Local Authority and BCO requirements. All structures to comply with current Building and Fire Regulations.

Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with consultant drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd

LEVEL	USE	TYPE	GIA (sqm)	GIA (sqft)
L00	E1e)	Dental Surgery	80.3	864
		Apt 2 Bedroom-1	75.9	817
		<b>Sub Total</b>	<b>156.4</b>	<b>1683</b>
L01	C3	Apt 1 Bedroom-1	51.0	549
		Apt 1 Bedroom-2	51.1	550
		Apt 1 Bedroom-3	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
L02	C3	Apt 1 Bedroom-4	51.0	549
		Apt 1 Bedroom-5	51.1	550
		Apt 1 Bedroom-6	54.8	590
		<b>Sub Total</b>	<b>156.9</b>	<b>1689</b>
		<b>Total</b>	<b>470</b>	<b>5059</b>



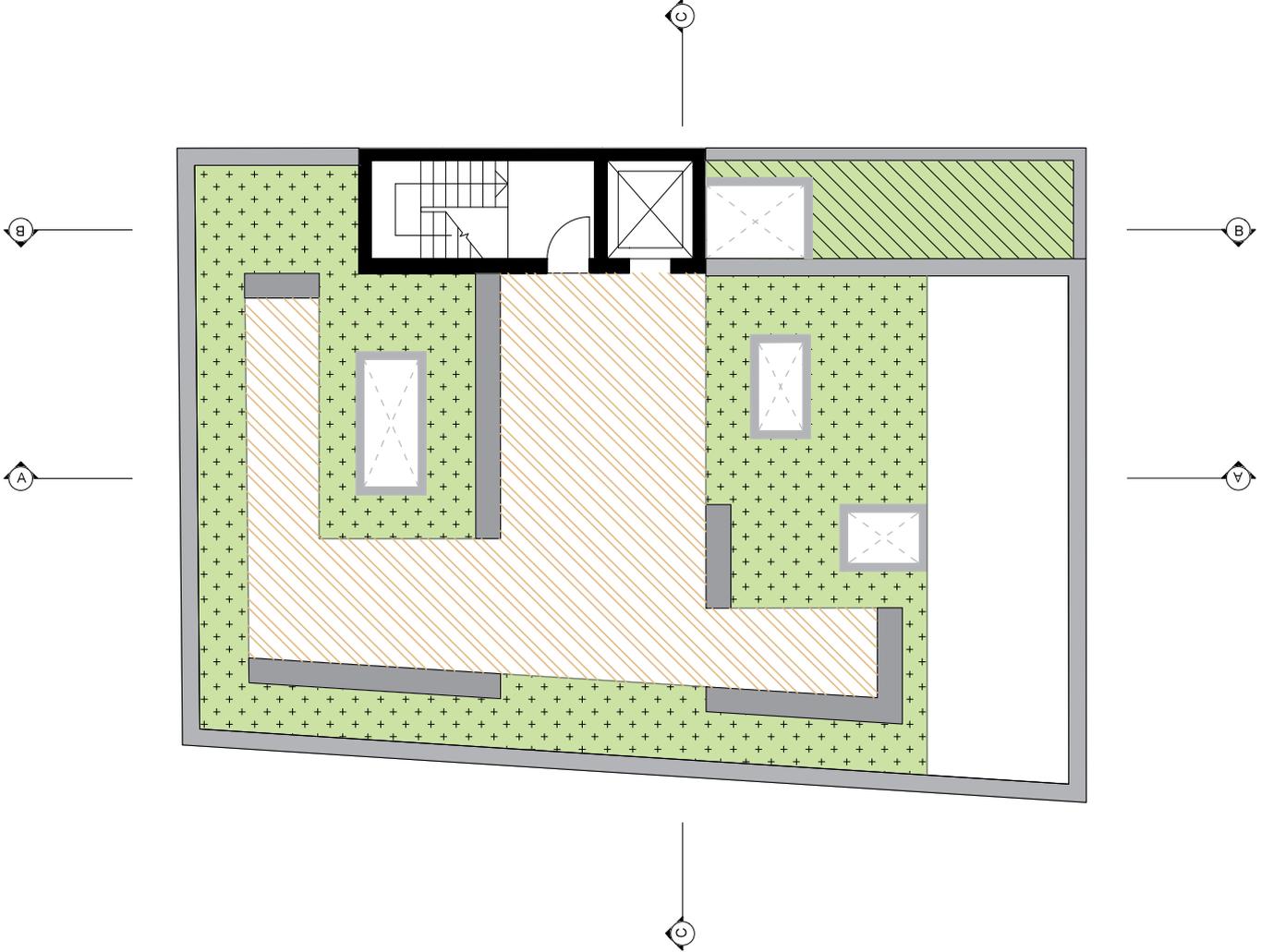
Status  
**DESIGN**

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title  
**PROPOSED ROOF PLAN**

Scale 1:100@A3  
 Date 29.07.2020  
 Drawn MI

Drawing No. 001\_2003  
 Rev 01



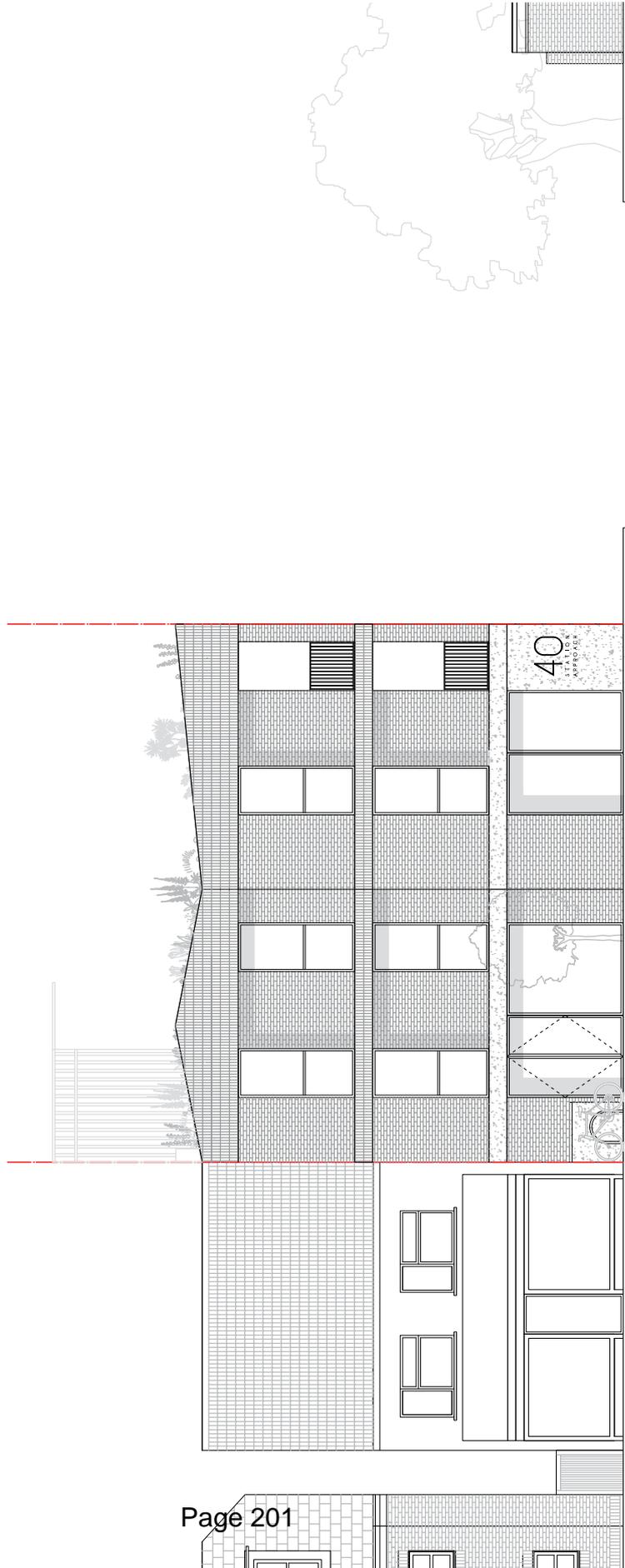
# STUDIO HALLETT IKE

42 Erdymyon Road, London, N4 1EQ  
 info@studiohallettike.co.uk  
 www.studiohallettike.co.uk

All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are in millimetres. The contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out for the building. All drawings are subject to approval by Local Authority, BCO and Environmental Health regulations and to Engineers' details. All structure to be designed and constructed in accordance with current Structural Engineers details and Local Authority and BCO regulations and Fire Regulations. Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site/circulation.

To be read in conjunction with **contract drawings**.  
 Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

1 Proposed North Elevation  
 Scale 1:100

Title		
PROPOSED NORTH ELEVATION		
Scale	Date	Drawn
1:100@A3	29.07.2020	MI
Drawing No.	Rev	
001_2020		01

# STUDIO HALLETT IKE

42 Erdymyon Road, London, N4 1EQ  
 info@studiohallettike.co.uk  
 www.studiohallettike.co.uk

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Contractor to check if this drawing is the latest revision prior to construction and to renew all previous revisions from site circulation.

To be read in conjunction with contract drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



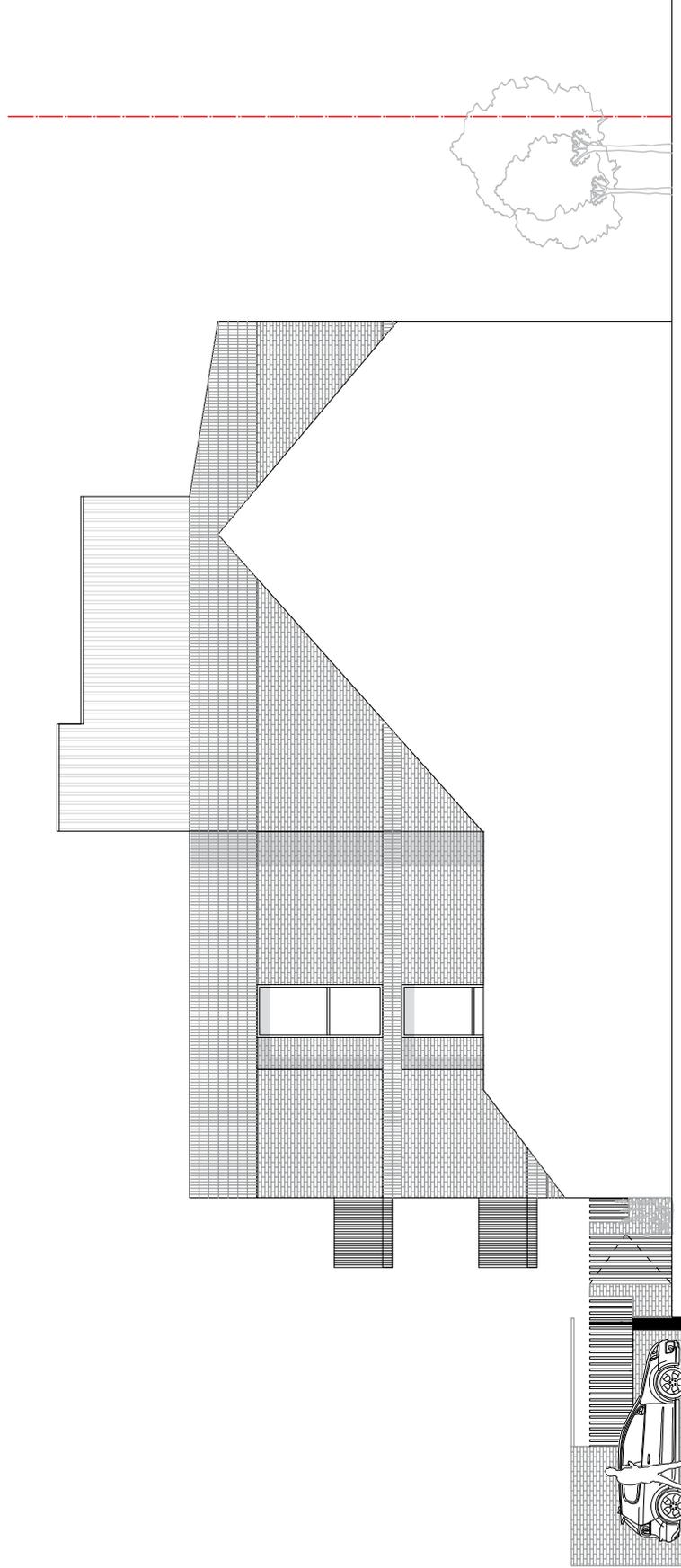
Status  
**DESIGN**

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

Title  
**PROPOSED EAST ELEVATION**

Scale 1:100@A3  
 Date 29.07.2020  
 Drawn MI

Drawing No. 001\_2023  
 Rev 01





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Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site circulation.

To be read in conjunction with contract drawings.

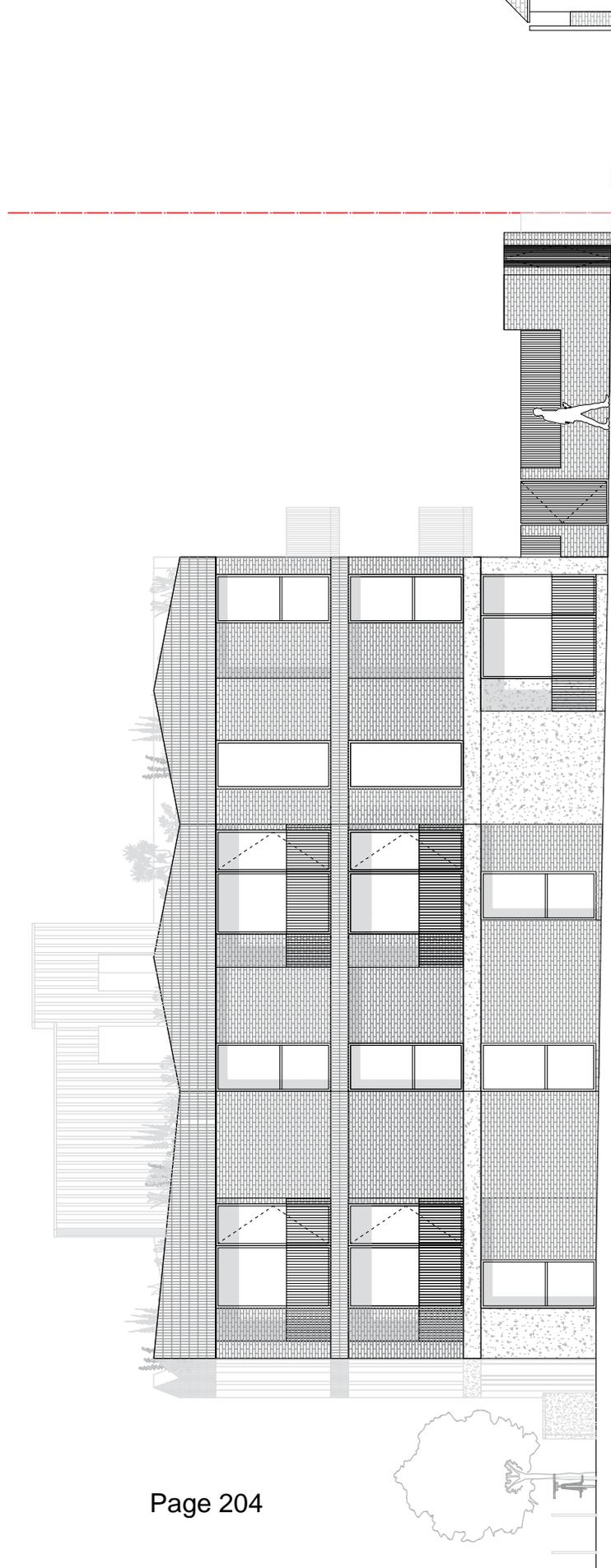
Do not scale from this drawing. Use figured dimensions only. All dimensions in millimetres otherwise stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Alben Housing Ltd



Status DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning



1 Proposed West Elevation  
 Scale 1:100

Title PROPOSED WEST ELEVATION

Scale	Date	Drawn
1:100@A3	29.07.2020	MI

Drawing No.	Rev
001_2021	01



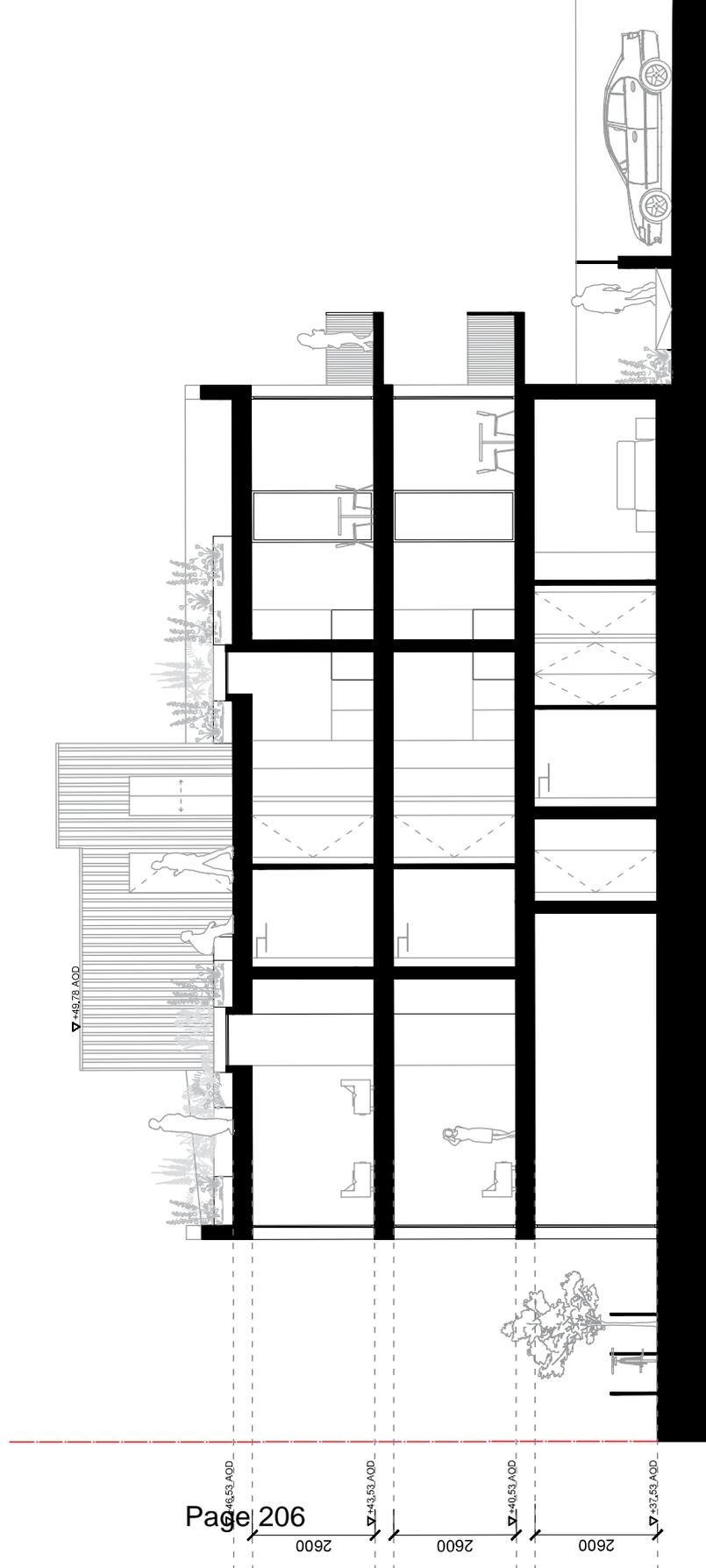
All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are in millimetres. The contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out for the proposed construction. All structures to be designed in accordance with the Building Regulations, Local Authority, BCO and Environmental Health Regulations and to Engineers' detail. All structures to be designed in accordance with the Building Regulations, Structural Engineers detail and Local Authority and BCO Regulations. All structures to comply with current Building and Fire Regulations.

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To be read in conjunction with contract drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in millimetres unless stated.

Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



Status  
 DESIGN

REV	DATE	DESC
00	14.12.2020	Planning

Title  
 PROPOSED SECTION A-A

Scale	Date	Drawn
1:100@A3	29.07.2020	MI

Drawing No.	Rev
001_2040	00



All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are in millimetres. The contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out drawings for approval by the contractor, subject to Local Authority approval. All structures to be designed in accordance with current Building Regulations and Local Authority and BCO Structural Engineers details. All structures to be designed in accordance with current Building and Fire Regulations.

Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site/circulation.

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Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



Status  
 DESIGN

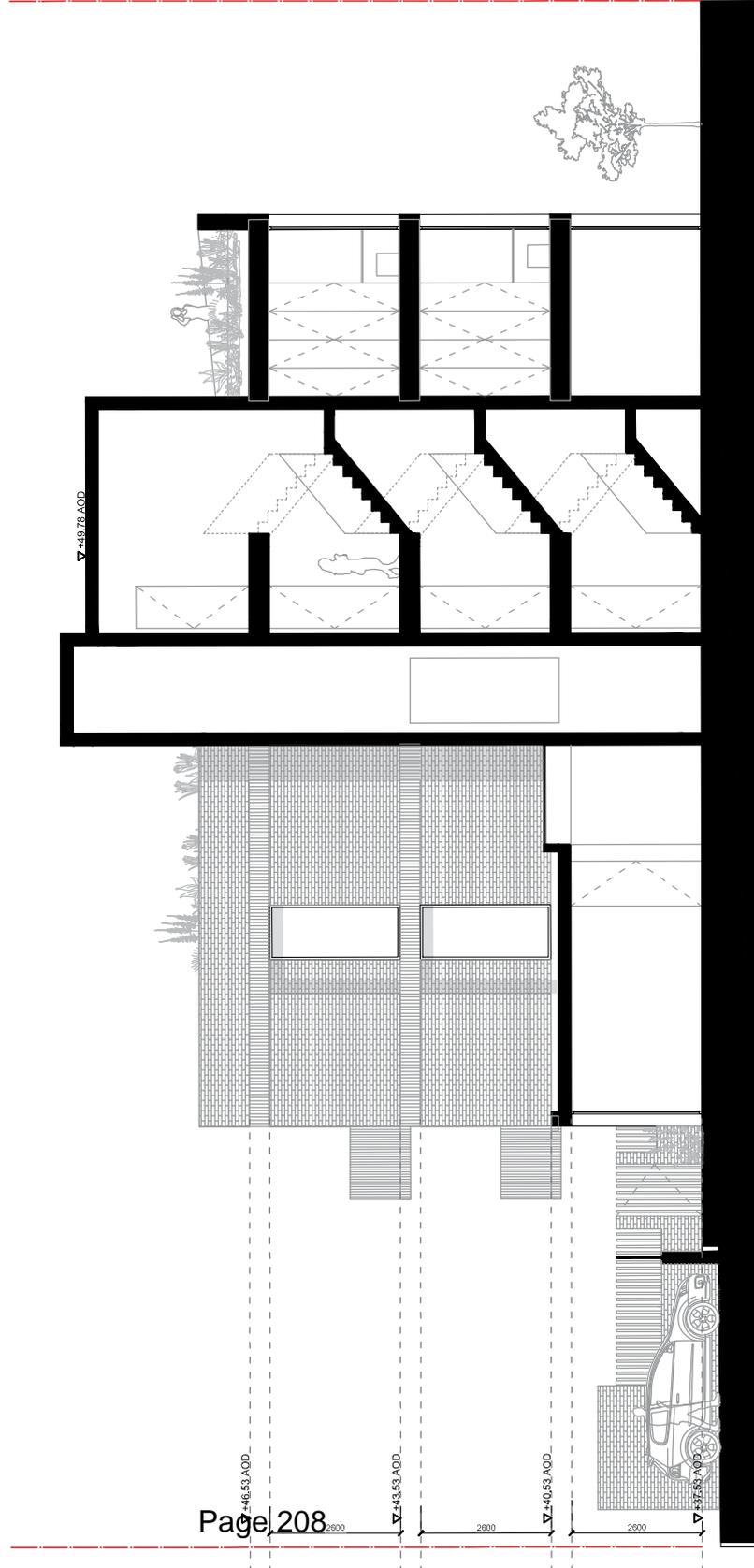
REV	DATE	DESC
00	14.12.2020	Planning

Title

PROPOSED SECTION B-B

Scale	Date	Drawn
1:100@A3	29.07.2020	MI

Drawing No.	Rev
001_2041	00



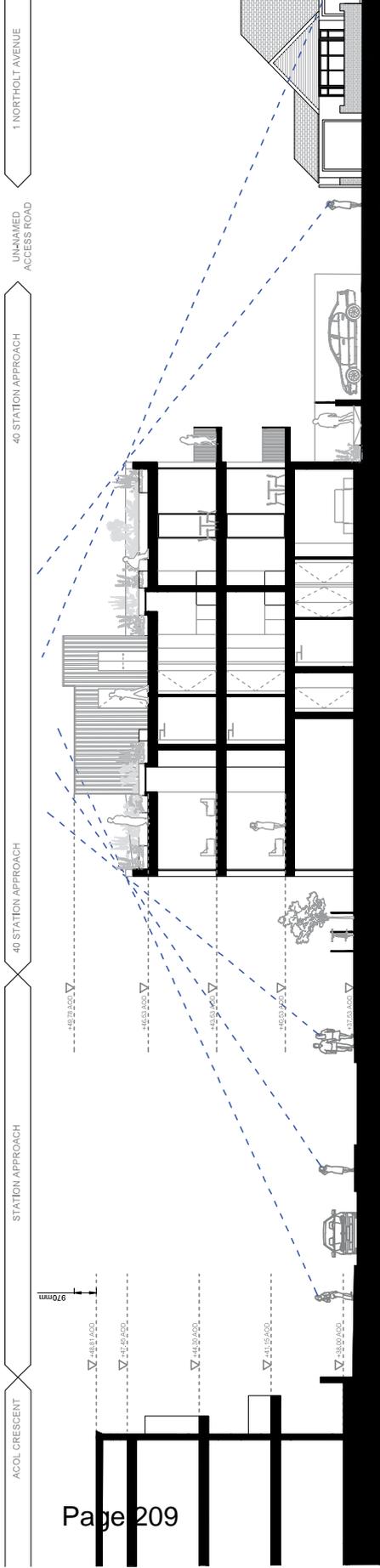
All dimensions and levels to be verified on site by contractor. Any errors to be reported immediately to the architect prior to construction or fabrication. Unless otherwise stated, all dimensions are to the face of work. Contractor is to address all details and ensure stability and strength of construction. The contractor to provide setting out for the proposed construction. All structures to be approved by the Local Planning Authority and Environmental Health. All structures to be designed in accordance with Structural Engineers details and Local Authority and BCO current Building and Fire Regulations.

Contractor to check if this drawing is the latest revision prior to construction and to remove all previous revisions from site/circulation.

To be read in conjunction with contract drawings.

Do not scale from this drawing. Use figured dimensions only. All dimensions in mm unless otherwise stated.

Project Name: 40 Station Approach  
 Job No.: 001  
 Client: Albon Housing Ltd



Status: DESIGN

REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

# STUDIO HALLETT IKE

42 Erdymyon Road, London, N4 1EQ  
 info@studiohallettike.co.uk  
 www.studiohallettike.co.uk

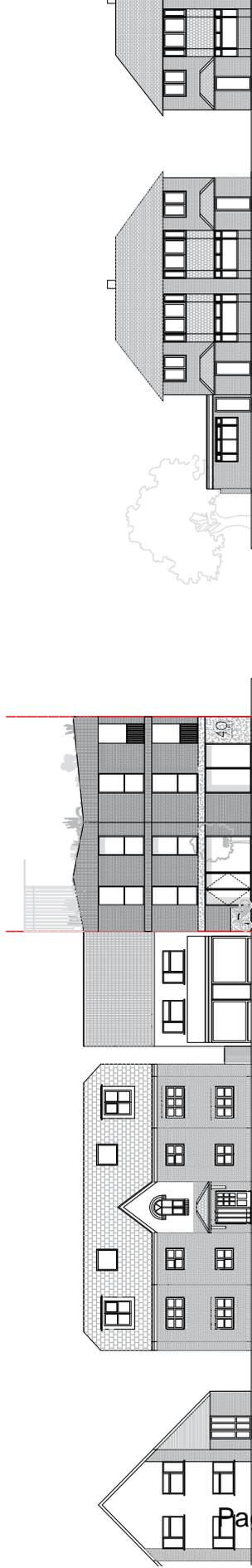
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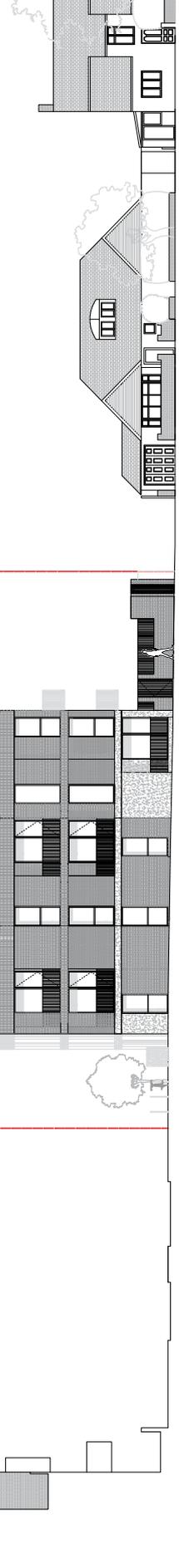
To be read in conjunction with consultant drawings.

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Project Name 40 Station Approach  
 Job No. 001  
 Client Albon Housing Ltd



1 Proposed Street Elevation - Station Approach North Facing  
 Scale 1:250



2 Proposed Street Elevation - Northolt Avenue West Facing  
 Scale 1:250



REV	DATE	DESC
00	18.08.2020	Pre Planning
01	14.12.2020	Planning

PROPOSED STREET SCENES		
Scale	Date	Drawn
1:250@A3	29.07.2020	MI
Drawing No.	Rev	
001_2030		01







**Notes:**

 Site boundary

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Site Address:

**40 STATION APPROACH  
 SOUTH RUISLIP**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**27354/APP/2020/4256**

Scale:

**1:1,250**

Planning Committee:

**North Page 213**

Date:

**March 2021**



**HILLINGDON**  
 LONDON

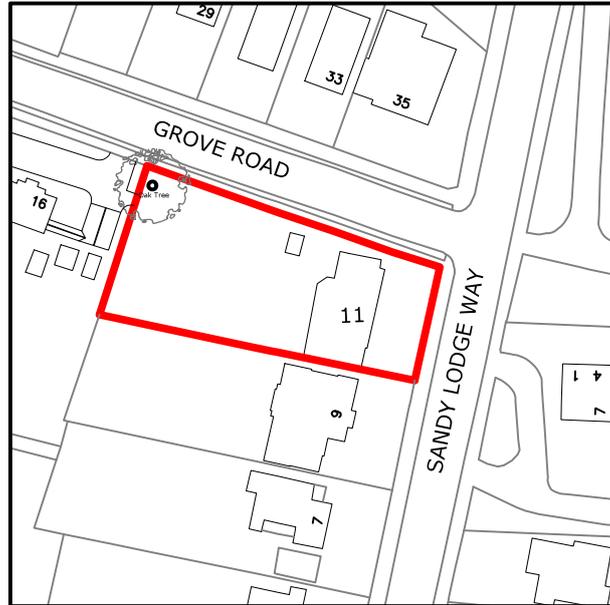
## **Report of the Head of Planning, Transportation and Regeneration**

**Address** 11 SANDY LODGE WAY NORTHWOOD

**Development:** Erection of a two storey building with habitable roofspace to create 3 x 2 -bed self-contained flats and 1 x 3-bed self-contained flat with forecourt car parking cycle parking, new vehicular access and amenity space following demolition of existing dwelling.

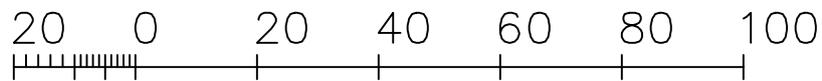
**LBH Ref Nos:** 16948/APP/2020/3623

**Drawing Nos:**

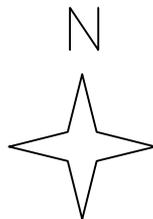


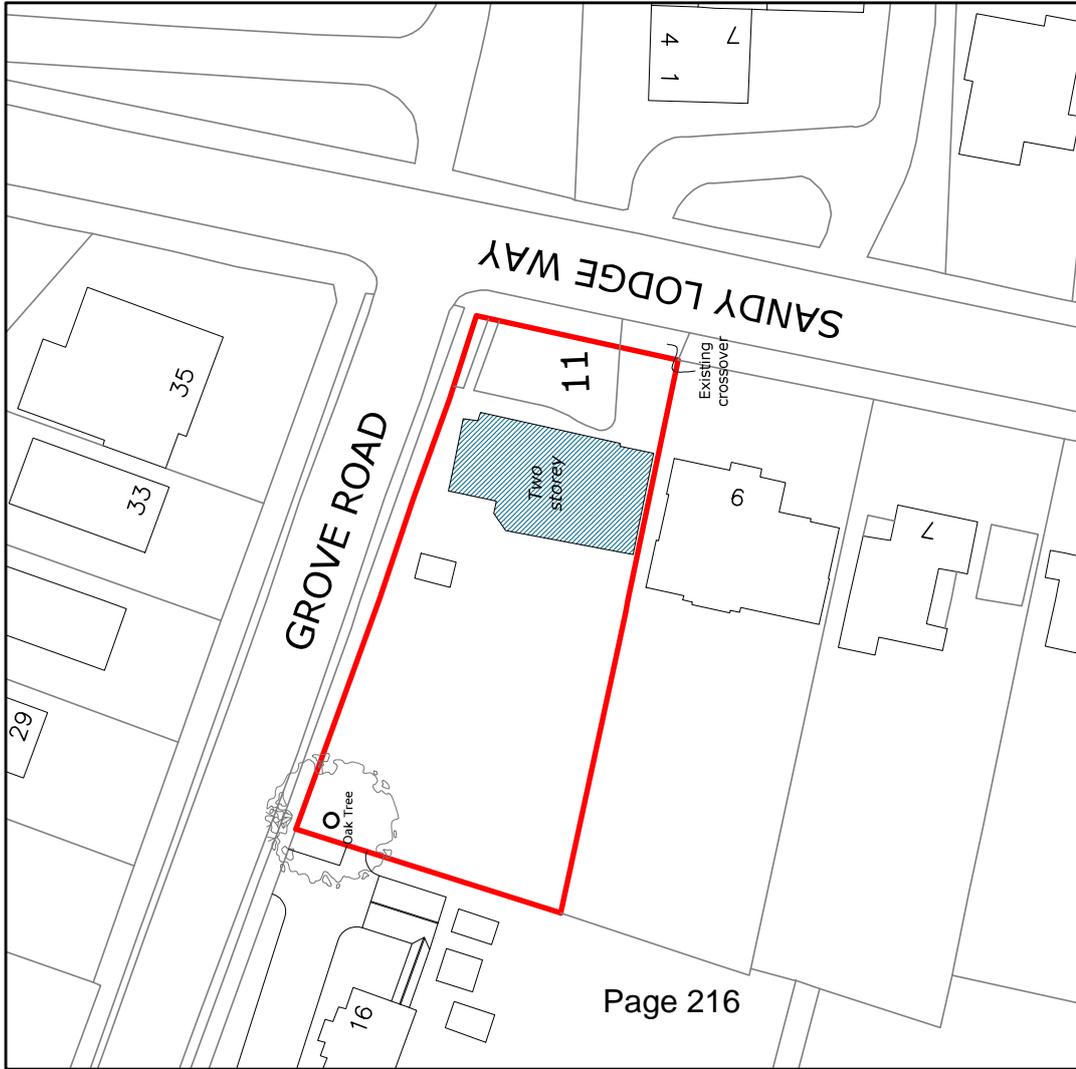
Scale: 1/1250

**11 SANDY LODGE WAY, MIDDLESEX.  
HA6 2AH**

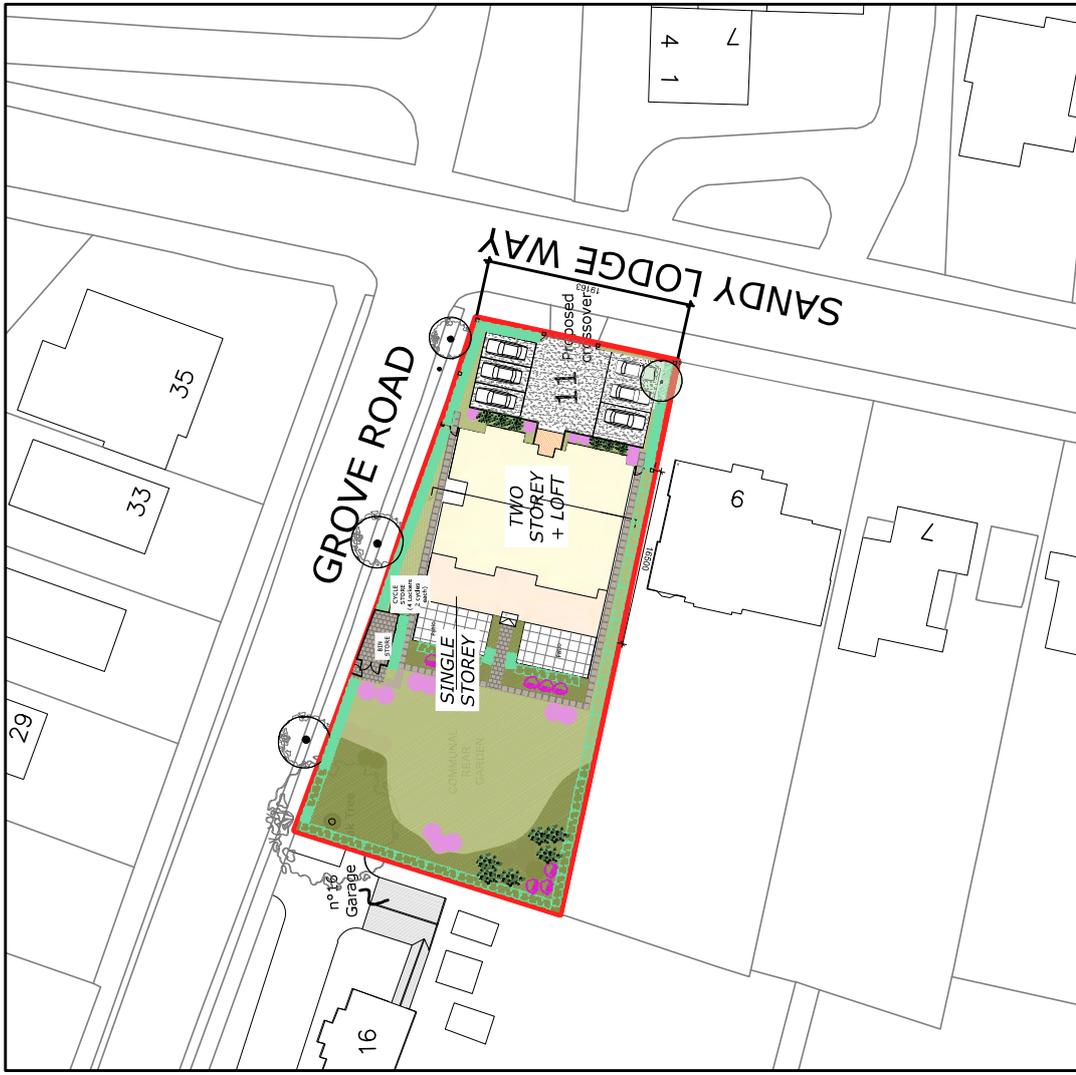


Metres





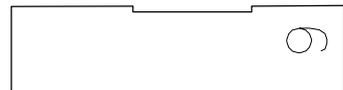
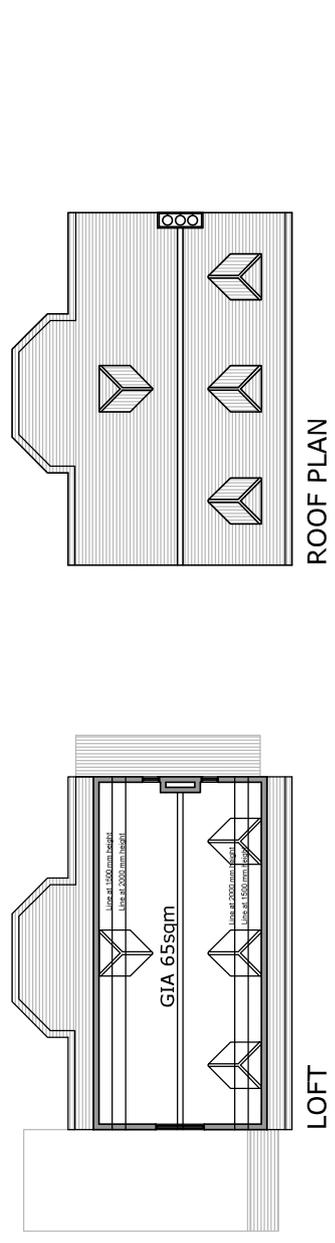
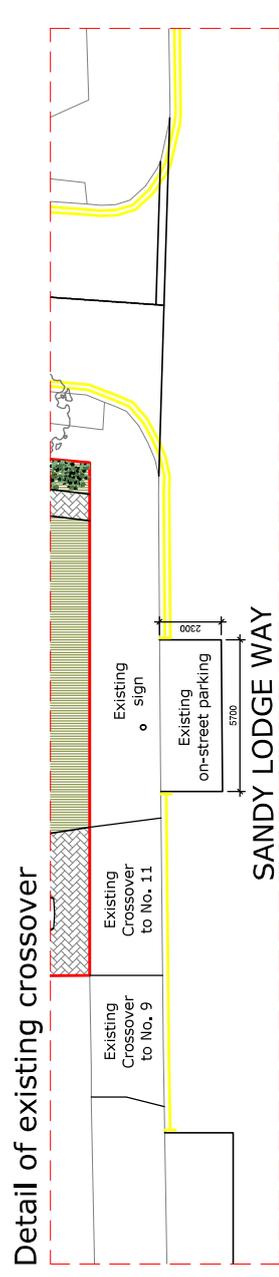
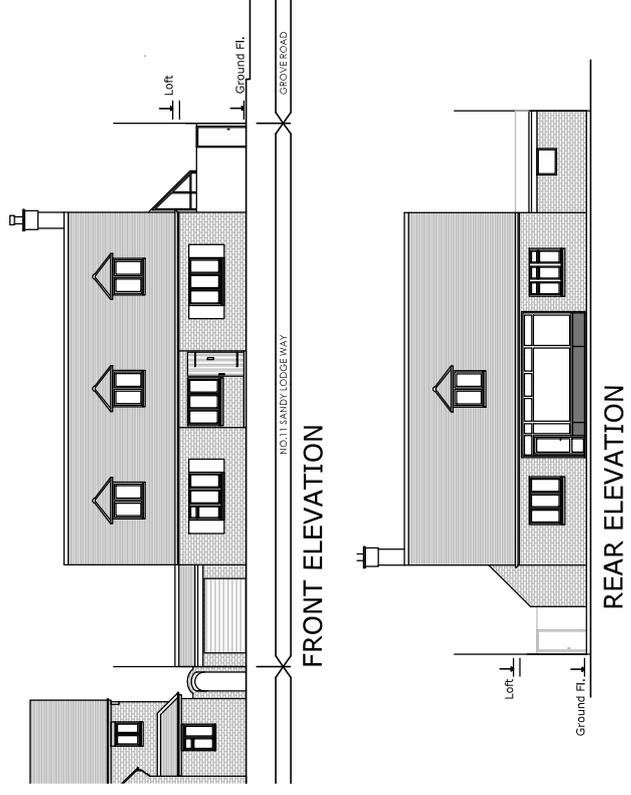
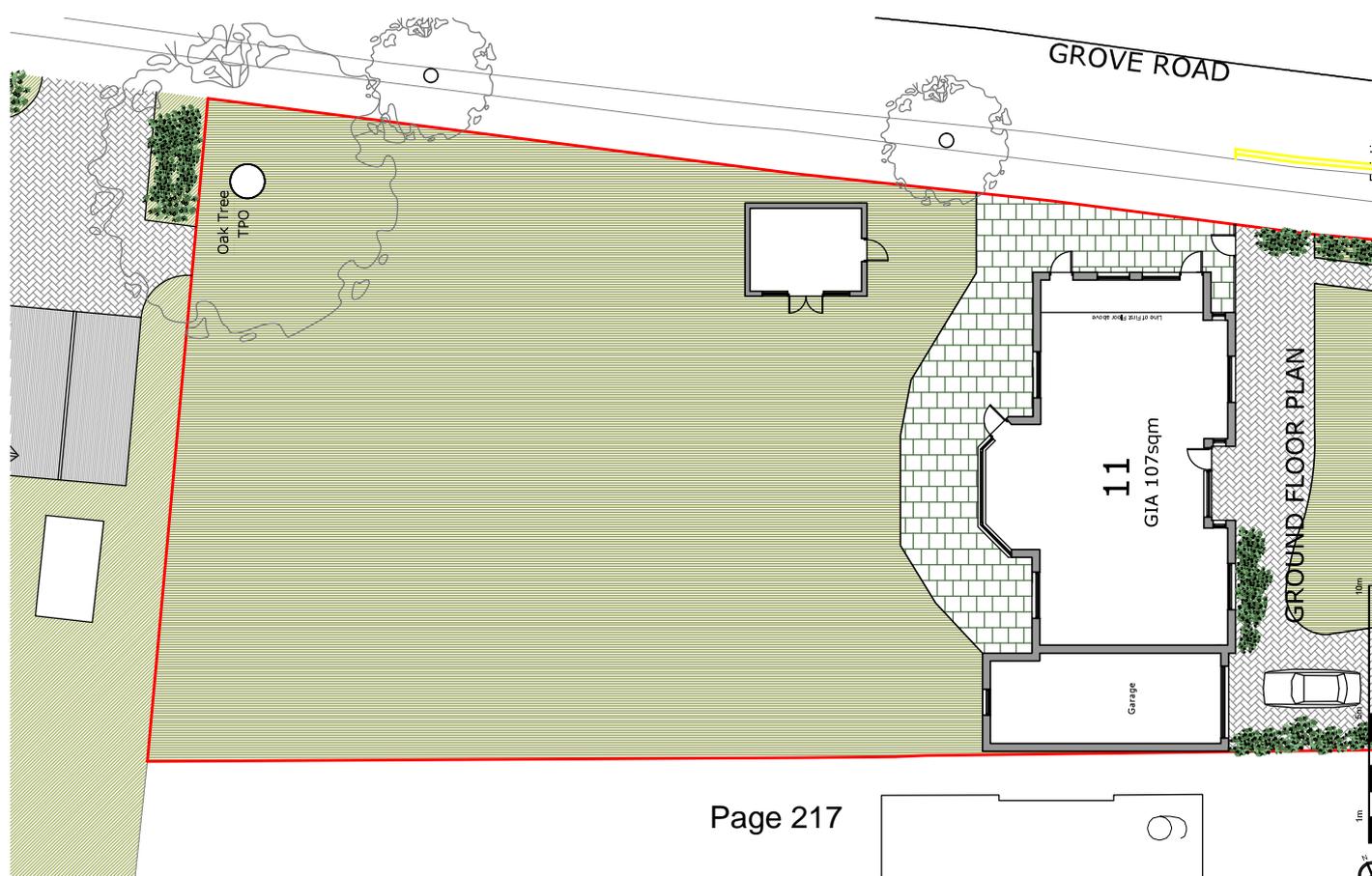
Existing Site Plan



Proposed Site Plan



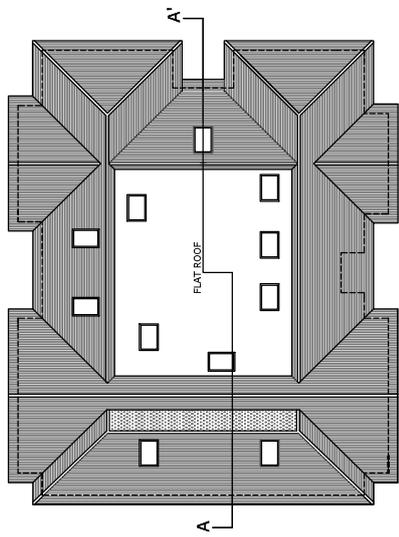
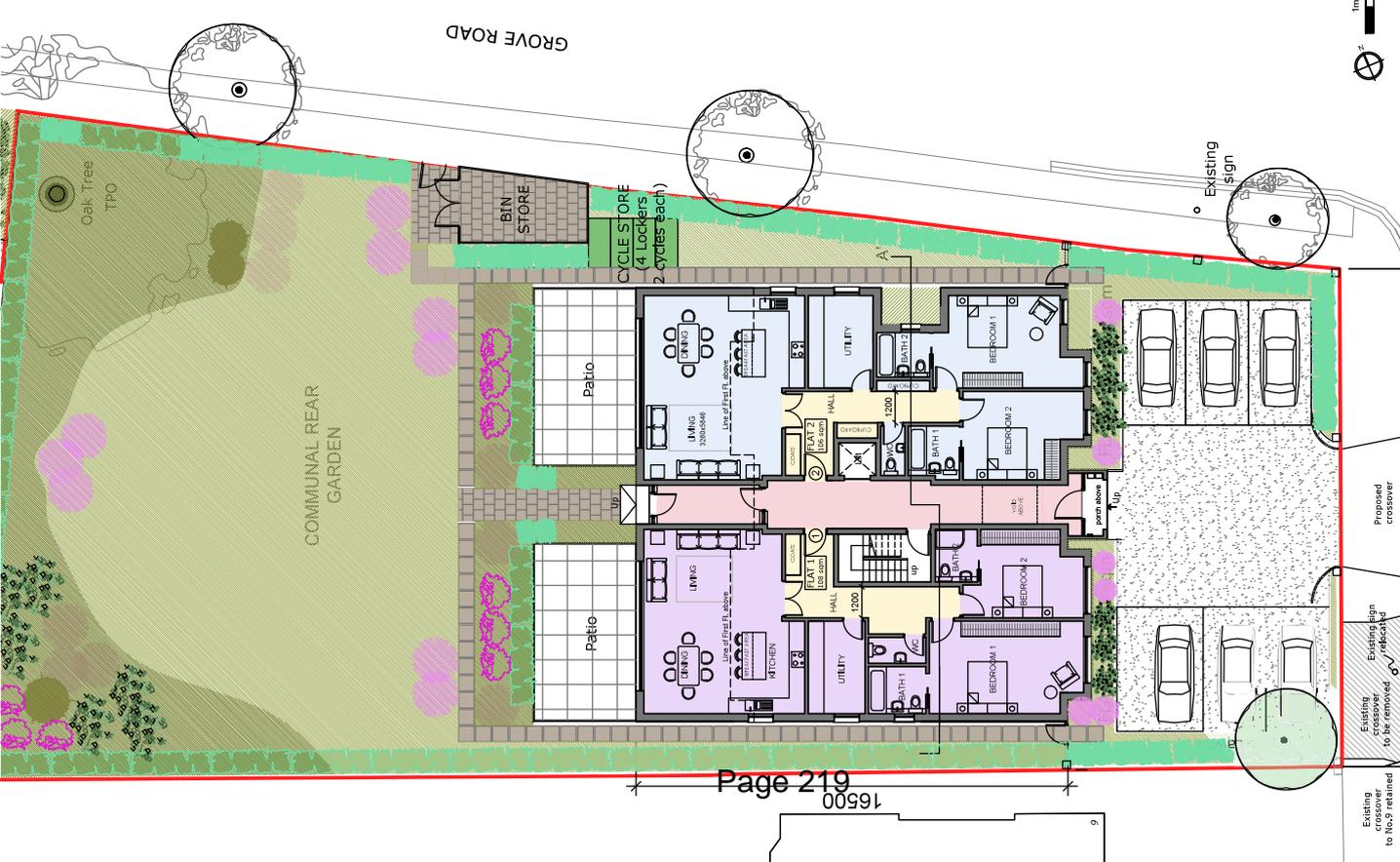
PROJECT	DWG NO.	SCALE	DETAIL	03.11.2020	PROJECT NO.
11 SANDY LODGE WAY, NORTHWOOD, HA6 2AH	200	1:500 @ A3	EXISTING AND PROPOSED SITE PLANS	SUBMITTED TO PLANNING DEPT. HILLINGDON	18.53
EXISTING AND PROPOSED SITE PLANS					



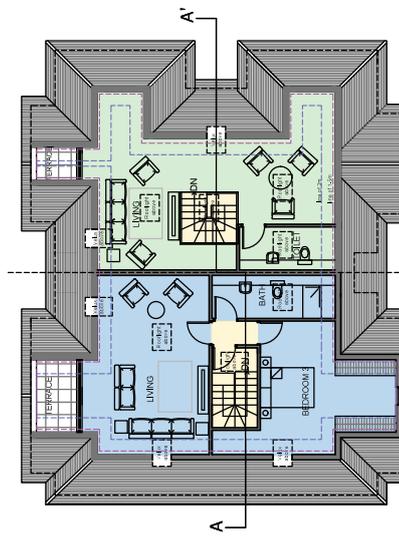
PROJECT	11 SANDY LODGE WAY, NORTHWOOD, HA6 2AH	DWG NO.	201	SCALE	1:100 @ A1	DETAIL	EXISTING PLANS AND ELEVATIONS	03.11.2020	03.11.2020	PROJECT NO.	16.48	PR Architecture Ltd.
EXISTING HOUSE								SUBMITTED TO PLANNING DEPT. HILLINGDON				Chartered Architect 120, Pinner Road, Harrow, HA1 4JD. Tel: 0208 857 2304



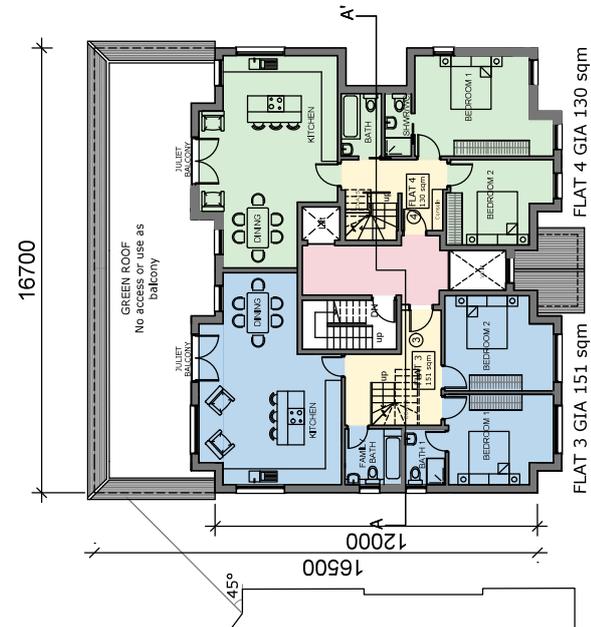
PROJECT	DWG NO.	SCALE	DETAIL	03.11.2020 SUBMITTED	PROJECT NO.	<b>PR Architecture Ltd.</b> Chartered Architect 120, Pinner Road, Harrow, HA1 4JD. Tel: 0208 357 2304
11 SANDY LODGE WAY, NORTHWOOD. HA6 2AH	202	1:100 @ A1	PROPOSED GROUND FLOOR PLAN. SITE PLANNING PLAN	Page 218 PLANNING DEPT. HILLINGDON	18.53	
PROPOSED 4 FLATS BUILDING						



Roof plan



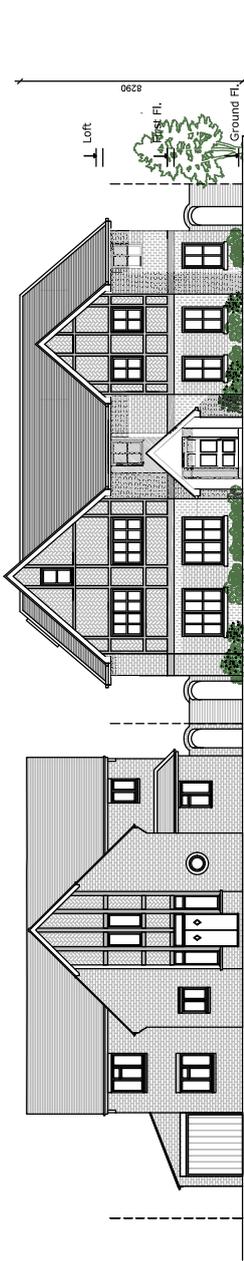
Loft floor plan



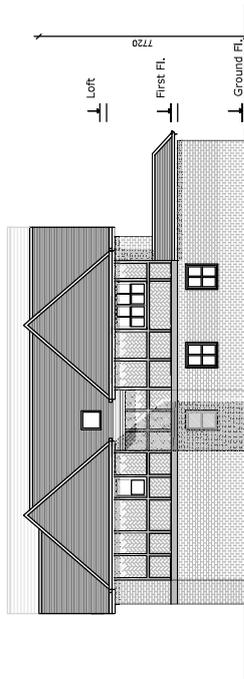
First floor



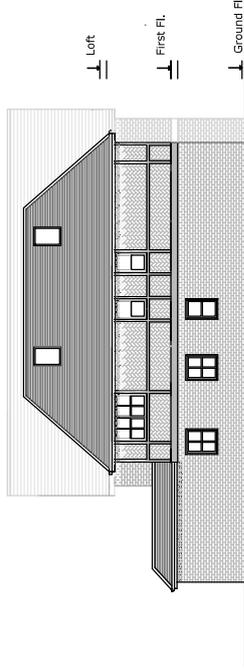
PROJECT <b>11 SANDY LODGE WAY, NORTHWOOD, HA6 2AH</b> PROPOSED 4 FLATS BUILDING - LOFT ACCOMMODATION	DWG NO.	SCALE	DETAIL	03.11.2020	SUBMITTED TO PLANNING DEPT. HILLINGDON	PROJECT NO.	18.53
	<b>203</b>	1:100 @ A1	PROPOSED FLOOR PLANS				
<b>PR Architecture Ltd.</b> Chartered Architect 120, Pinner Road, Harrow, HA1 4JD. Tel: 0208 857 2304							



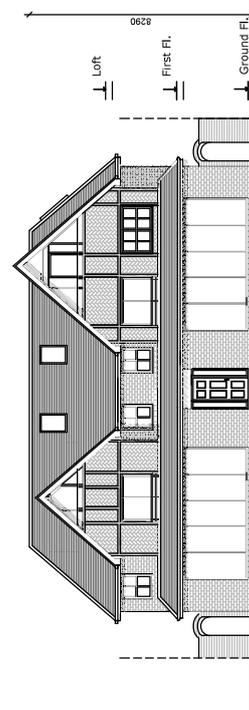
FRONT ELEVATION - 11 SANDY LODGE WAY



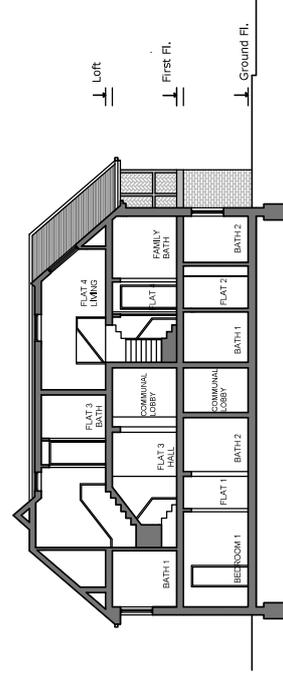
SIDE ELEVATION 1



SIDE ELEVATION 2



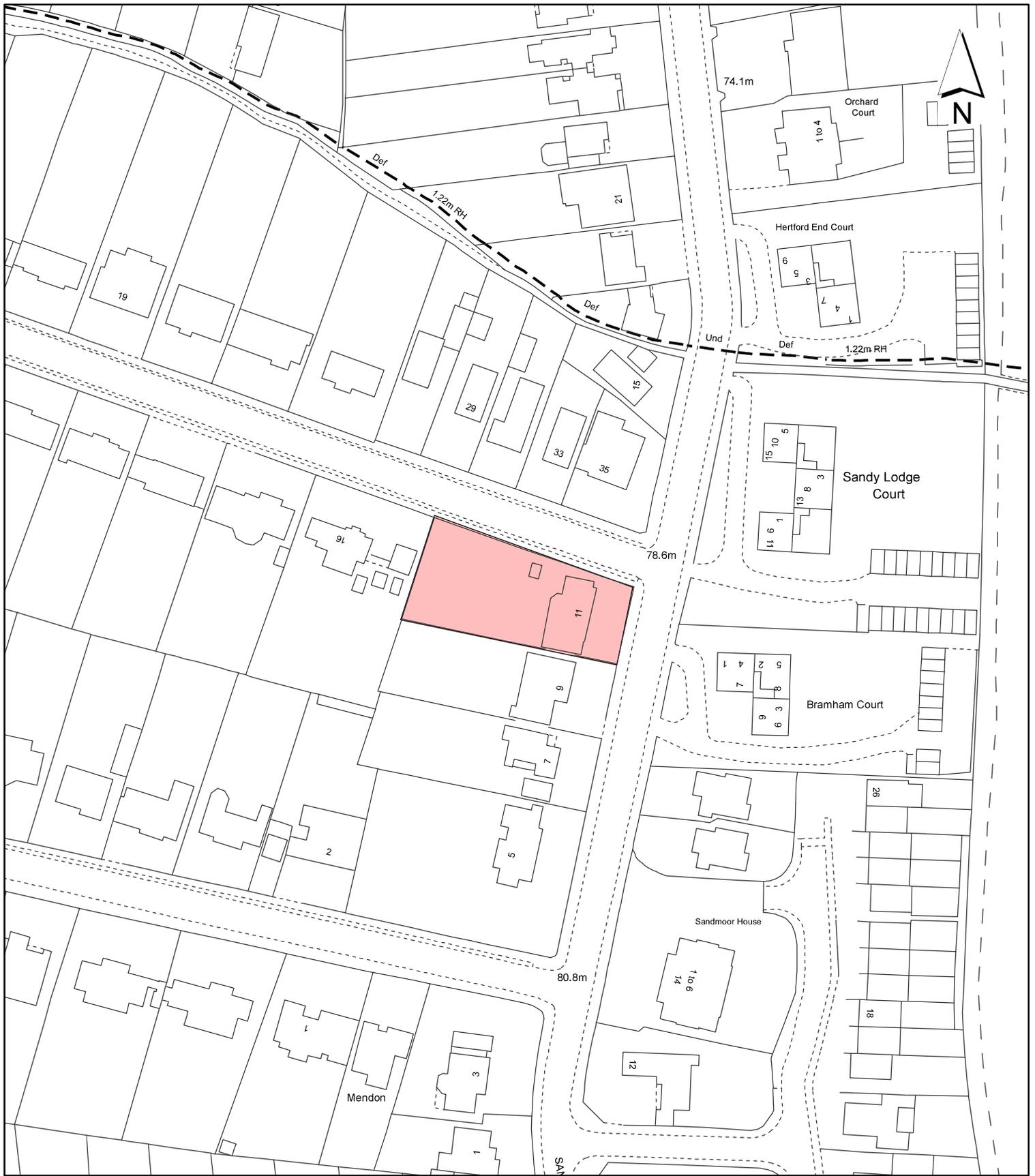
REAR ELEVATION



SECTION A-A'



PROJECT	DWG NO.	SCALE	DETAIL	03.11.2020	PROJECT NO.
11 SANDY LODGE WAY, NORTHWOOD, HA6 2AH	204	1:100 @ A1	PROPOSED ELEVATIONS AND SECTION	SUBMITTED TO PLANNING DEPT. HILLINGDON	18.53
PROPOSED 4 FLATS BUILDING - LOFT ACCOMMODATION					



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Site Address:

**11 SANDY LODGE WAY  
NORTHWOOD**

**LONDON BOROUGH  
OF HILLINGDON**  
Residents Services  
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**16948/APP/2020/3623**

Scale:

**1:1,250**

Planning Committee:

**North Page 221**

Date:

**March 2021**



**HILLINGDON**  
LONDON

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